

NOTICE OF MEETING



EXTRAORDINARY LOCAL TRAFFIC COMMITTEE MEETING

An Extraordinary Local Traffic Committee Meeting of Byron Shire Council will be held as follows:

Venue	Conference Room, Station Street, Mullumbimby
Date	Tuesday, 16 March 2021
Time	10:30am
Committee Members	Ian Shanahan – Transport for NSW Detective Chief Inspector Matt Kehoe – Police Cr Basil Cameron Tamara Smith MP

BYRON SHIRE COUNCIL
EXTRAORDINARY LOCAL TRAFFIC COMMITTEE MEETING

BUSINESS OF MEETING

1. APOLOGIES

2. DECLARATIONS OF INTEREST – PECUNIARY AND NON-PECUNIARY

3. ADOPTION OF MINUTES FROM PREVIOUS MEETINGS

3.1 Local Traffic Committee Meeting held on 16 February 2021

4. MATTERS ARISING

5. OUTSTANDING ISSUES/RESOLUTIONS

6. REGULATORY MATTERS

6.1	Brunswick Heads - No Parking Signage	3
6.2	Event - Byron Writers Festival 2021	5
6.3	Event - Mullum2Bruns Paddle	17
6.4	Bay Lane Modifications.....	25

REGULATORY MATTERS

Report No. 6.1 Brunswick Heads - No Parking Signage

File No: I2021/204

5

Council recently introduced No Parking 1am - 6am along South Beach Road Brunswick Heads. This has reduced the illegal camping problem along the beach front but has pushed campers to a couple other locations within Brunswick Heads, namely:

- i) Along the River front. See the yellow highlighted area in figure 1.
- 10 ii) In the Stan Thompson Oval area on the southern approach into town, see yellow area in figure 2.

To help Council's compliance team better manage illegal camping in these areas Council requests the installation of:

- 15 i) No Parking 1am-6am signs along the river front streets highlighted yellow in Figure 1. Blue dots show the typical location of proposed signs.
- ii) No Parking 1am-6am area signs at the entrance to Stan Thompson Oval area. Blue line at the entrance shown the typical location proposed for signage.



Figure 1 – Brunswick Heads river frontage signage plan



Figure 2 – Stan Thompson Oval area.

5

RECOMMENDATION:

That Council endorse the installation of:

10

1. No Parking 1am-6am signage along the eastern side of Mona Lane, Fawcett St (between Mona Lane and Park Street) and Park Street (between Fawcett Street and Mullumbimbi Street).
2. No Parking Area 1am-6am signage at the entrance to the Stan Thompson Oval area

Report No. 6.2 Event - Byron Writers Festival 2021

File No: I2021/300

Summary

- 5 Council has received an event application with Traffic Management Plan (TMP) for the 2021 Byron Writers Festival to be held between Friday 6th August and Sunday 8th August 2021.

The event site is located at the northern end of Bayshore Drive, adjacent to Elements Resort. See attached 2021 TMP for location.

- 10 Based on previous events in 2018 and 2019 patron numbers are expected to be in the order of 12 000 tickets sold, with 55% of visitors staying for four days or more, and 12% for over seven days. The festival will therefore contribute to additional traffic in the lead up to, during and after the festival.

CONSIDERATIONS

- 15 Ewingsdale Road is susceptible to queuing and congestion.

Byron Bypass will be operational and its impact on traffic flow is currently unknown. It is expected to have minimal impact initially.

Refer to attachment 1 for the TMP for the event. In summary it proposes:

- 20
- Wayfinding traffic control signage on Ewingsdale Rd on the approach to Bayshore Drive.
 - 40km/h speed zone north of the Bayshore Drive rail crossing to event car park
 - 10km/h from the car park entry to the end of Bayshore Drive.
- 25
- Retain pedestrian traffic within the parking area as much as possible and provide a pedestrian crossing linking the car park to the event within the 10k/h zone, with traffic controllers
 - Mobility parking to be provided adjacent to the event on Bayshore Drive and a valet service for PWD patrons to allow their vehicles to be parked amongst the regular cars, thus effectively creating more PWD parking spaces.
- 30
- Taxi and Uber drop off point is proposed within the Car Park on the south western side of Bayshore Drive.
 - Bus drop off area is proposed at the end of Bayshore Dr within the cul-de-sac.
 - Traffic exiting the event and heading to Byron Bay is directed down Bayshore Drive.

BYRON SHIRE COUNCIL

LOCAL TRAFFIC COMMITTEE MEETING

6.2

- Traffic heading west along Ewingsdale Rd from Byron Town Centre is directed to turn right at the Bayshore Dr Roundabout.
 - As a contingency, staff recommends if significant traffic congestion occurs at the Bayshore Drive Roundabout with east bound traffic backing up towards The Farm then west bound traffic arriving from Byron Town Centre is to be directed towards the event via Sunrise Blvd.

RECOMMENDATION:

10 That Council support the Byron Bay Writers Festival 6-8th August 2021, subject to:

- 15 1) Development and implementation of a Traffic Management Plan (TMP) and Traffic Control Plan(s) (TCP) by those with appropriate TfNSW accreditation and the holding of current and appropriate levels of insurance and liability cover. The TMP and TCP to be generally in accordance with Attachment 1 (E2021/38636);
- 2) West bound event traffic arriving from Byron Town Centre to be directed down Sunrise Blvd, as a contingency, if east bound traffic on Ewingsdale Rd is queued back to the Ewingsdale Road interchange;
- 3) Separate approvals by NSW Police and RMS being obtained;
- 20 4) The event organiser to:
 - 25 i) Advertise the impact of the event, via a Variable Message Signage (VMS) and notice in the local weekly paper, a minimum of one week prior to the operational impacts taking effect, noting it must include the event name, specifics of any traffic impacts or road closures and times, alternative route arrangements, event organiser, a personal contact name and a telephone number for all event related enquiries or complaints;
 - 30 ii) Arrange the event being notified on Council's webpage a minimum one week prior to the TCP being implemented;
 - iii) undertake consultation with community and affected businesses including adequate response/action to any raised concerns during and after the event;
 - iv) undertake consultation with emergency services and any identified issues addressed;
 - v) hold \$20m public liability insurance cover which is valid for the event.

35 Attachments:

- 1 Byron Writers Fest - TMP, E2021/38636 , page 8 [↓](#)

BYRON SHIRE COUNCIL

LOCAL TRAFFIC COMMITTEE MEETING

6.2

Ingen Consulting
Alstonville NSW 2477
0417 264 987
michiel@ingenconsulting.com.au
www.ingenconsulting.com.au



Our Reference: J1130_TMP2

General Manager
Byron Shire Council
PO Box 219
Mullumbimby NSW 2482

8th February 2021

Attn: Andrew Pearce

Re: Byron Writers Festival 2021 — Traffic Management Plan

Dear Andrew,

The Byron Writers Festival have engaged our services to prepare a Traffic Management Plan (TMP) for the 2021 Byron Writers Festival, at Bayshore Drive in Byron Bay.

Pacific Motorway

Given that congestion in the southbound lane of the Pacific Motorway leading up to the Byron Bay exit is a daily occurrence, the Byron Writers Festival should not be considered the cause of this congestion on festival days. It is recognised however that the festival is a contributor of traffic on the Pacific Motorway. In recognition of this and the delays caused for festival patrons travelling to and from the festival site, festival management is in the process of implementing management techniques that improve the mode share characteristics of the festival traffic.

Speed zones

Similar to previous years, we have included a 40km/h speed zone from the rail crossing to the car park entrance on Bayshore Drive, and 10km/h from the car park entry to the end of Bayshore Drive. The latter would improve the safety of any pedestrian-vehicle interactions that may occur on Bayshore Drive. These speed zones are depicted on the attached Traffic Control Plans.

Civil | Traffic | Noise | Project Management | Contract Management

Byron Writers Festival 2021
Traffic Management Plan



Pedestrian crossing

We propose to retain the pedestrian crossing (from the car park to the footpath) at the same location as last year. We have also included the 10 km/h speed zone north of the car parking entry and pedestrian fencing along the western side of the swale. We conclude that this will result in a significant increase in pedestrian safety, as the likelihood of crossing is reduced due to the fencing, and the consequence of an unwanted pedestrian-vehicle interaction is reduced due to the low vehicle speed.

Bus and taxi movement plan

Similarly to last year, a bus and taxi movement plan is included in the set of drawings. With the increased use of drop offs by services such as Uber, a suitable and efficient drop off zone is allocated within the car parking area close to the entrance. The design and placement of internal signage to facilitate the vehicle flows as indicated in the bus and taxi movement plan is not included in this TMP (which is intended to describe signage in the public road only) and will be included in the festival's internal management documentation.

Traffic management strategies

A number of traffic management strategies, outside the scope of the traffic management plan for the festival, are being assessed for suitability to include in the management of traffic at this year's festival.

These include but may not be limited to:

- A valet service for PWD patrons to allow their vehicles to be parked amongst the regular cars, thus effectively creating more PWD parking spaces
- Strategies to improve the use of buses
- Strategies to reduce the use of cars

Council will be informed of progress on these items after each strategy has been assessed for suitability and economy.

Traffic Guidance Schemes

The attached traffic guidance schemes reflect the above descriptions. If you have any questions, please do not hesitate to contact our office.

Byron Writers Festival 2021
Traffic Management Plan



Yours sincerely,

A handwritten signature in blue ink, appearing to read "MK", written over a light blue horizontal line.

Michiel Kamphorst

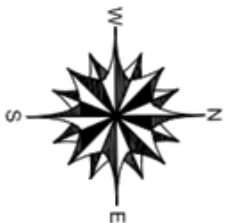
MSc, BSc, RPEng, RPEQ, NER (Civil)

Prepare a Work Zone Traffic Management Plan 0052298287

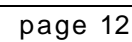
Attachments: Traffic Guidance Schemes

TRAFFIC GUIDANCE SCHEMES
for
2021 BYRON WRITERS FESTIVAL
at
Bayshore Drive, Byron Bay

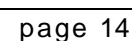
DRAWING TABLE	
DRAWING NUMBER	TITLE
J1130_TGS_01	Title page
J1130_TGS_02	Ewingsdale Road directional signage
J1130_TGS_03	Departure directional signage
J1130_TGS_04	Bayshore Drive
J1130_TGS_05	Details
J1130_TGS_06	Bus and taxi movement plan

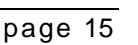


	
REV: 0	DESCRIPTION: S138 application
DATE: 8/2/21	DRAWN: MK
REVIEWED: MK	APPROVED: MK
APPROVED BY:  Michael Kamphorst, MSC, BSC, RPEng, RPEQ, NER Prepare a Work Zone Traffic Management Plan 0052298287	
Date: 8th February 2021	
	
Ingen Consulting Pty Ltd ACN: 623 948 112 a: Alstonville, NSW p: +614 1726 4987 e: michael@ingenconsulting.com.au w: www.ingenconsulting.com.au	
CLIENT: Byron Writers Festival PO Box 1846 Byron Bay NSW 2481	
PROJECT: 2021 Byron Writers Festival Traffic Guidance Schemes	
DRAWING TITLE: Title Page	
PROJECT NUMBER: J1130	ORIGINAL SIZE: A3
DRAWING NUMBER: J1130_TGS_01	REVISION: 0



page 13

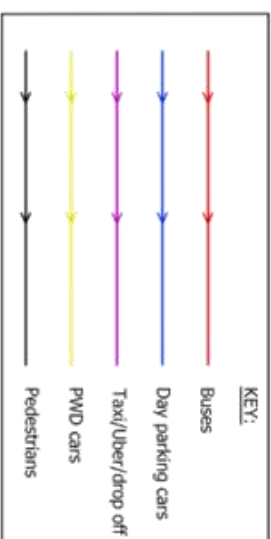




An aerial photograph of a golf course with several colored lines and arrows overlaid. A blue line starts from the left, loops around a small building, and then extends straight towards the right. A yellow line follows a similar path but is slightly offset. A red line follows the yellow line closely. A purple line forms a loop around a small building on the left. Black arrows indicate the direction of travel along these lines. The background shows a large green field, a forested area at the top, and a building with a pool at the bottom.

TGS NOTES:

1. This Traffic Guidance Schemes (TGS) is to be read in conjunction with the associated Traffic Management Plan.
2. This TGS is only valid for the duration of the 2021 Byron Writers Festival
3. This TGS is only to be implemented by adequately experienced and certified personnel.
4. Installation of signs, barriers and other traffic control devices shall be in accordance with the AS1742 series and the TNSW Traffic Control at Work Sites version 6.0.
5. Signage is based on TNSW standard TGS's, but sign spacing has been amended to suit local conditions and reduce complexity for users.
6. Traffic controllers to be TNSW-accredited and wearing suitable high-visibility clothing.
7. Pedestrian marshals to be suitably experienced and to wear high-visibility vest.
8. Where dimensions are not shown, sign locations to correspond to locations shown on aerial imagery.



Report No. 6.3 Event - Mullum2Bruns Paddle

File No: I2021/302

SUMMARY

- 5 Council has received an application from the Mullum2Bruns Paddle Committee for the Mullum2Bruns Paddle 2021 to be held Sunday 23rd May 2021.

BACKGROUND

- 10 The event will be the same as the 2020 Traffic Management Plan and Traffic Control Plan. The 2020 TCP has been attached for reference but has not yet been updated to reflect 2021 dates. Below is a general summary of what traffic control is proposed as part of the 2021 event.

- 15 To ensure greater safety around the registration and marshalling area the event proposes temporary one-way traffic off Brunswick Terrace, between Tincogan Street and Tyagarah Street, Mullumbimby, between 6:30am and 10:00am. The direction of the one way is shown in the image below.



Additional traffic control measures proposed include:

- The right turn off Tincogan Street into Brunswick Terrace to have a “no right turn” sign
 - The left turn off Tincogan St into Brunswick Terrace to have a “no left turn”
- 20

BYRON SHIRE COUNCIL

LOCAL TRAFFIC COMMITTEE MEETING

6.3

- Relevant detour signs will be installed at the approaches to the impacted area
 - No parking signs along the one way part of Brunswick Terrace
 - 5 • Participants will be diverted along the southern part of Brunswick Terrace to the intersection of Brunswick Terrace & Tincogan Street to enable them to cross to the starting area. The traffic controllers will hold traffic crossing to the registration zone until there are spaces for them to unload their craft.
 - Uniformed Marine Rescue personnel will be patrolling the homes opposite the registration area to ensure that participants are not parking in front of or across driveways, thereby minimising inconvenience to the residents.
 - 10 • A letter-box drop would be done 2 weeks prior to the event, informing residents in the surrounding streets of the temporary one-way traffic closures.
-

15 **RECOMMENDATION:**

That Council endorse the Mullum2Bruns Paddle 2020 to be held Sunday 23 May 2021, subject to:

1. The development and implementation of a Traffic Management Plan (TMP) and Traffic Control Plan(s) (TCP) by those with appropriate NSW accreditation.
- 20 2. The TMP and TCP is to include, but not limited to, the following:
 - a) One way traffic along Brunswick Terrace, Mullumbimby, between 06:30am and 10:00am on Sunday 23rd May 2021. The one way is to be from Tyagarah Street to Tincogan Street;
 - 25 b) The right turn off Tincogan Street into Brunswick Terrace to have a “no right turn” sign;
 - c) The left turn off Tincogan St into Brunswick Terrace to have a “no left turn”;
 - d) Relevant detour signs at the approaches to the impacted area;
 - e) No parking signs along the one way part of Brunswick Terrace.
- 3) The event organiser to:
 - 30 a) Advertise the impact of the event, via a Variable Message Signage on site and notice in the local weekly paper, a minimum of one week prior to the operational impacts taking effect, noting it must include the event name, specifics of any traffic impacts or road closures and times, alternative route arrangements, event organiser, a personal contact name and a telephone
 - 35 number for all event related enquiries or complaints;

BYRON SHIRE COUNCIL

LOCAL TRAFFIC COMMITTEE MEETING

6.3

b) Arrange for the event to be notified on Council's webpage a minimum one week prior to the TCP being implemented;

c) undertake consultation with relevant community and affected businesses including adequate response/action to any raised concerns during and after the event;

d) undertake consultation with emergency services and any identified issues addressed;

e) hold \$20m public liability insurance cover which is valid for the event.

Attachments:

- 1 42.2020.6.1 Event info - Mullum2Bruns Paddle - 24 May 2020 - traffic control plans, E2020/2760 , page 20 [↓](#)

www.invarion.com

<p>1300 800 301</p>	<p>Date: 5/12/2016 Author: Scott Pieniacki Project: Mullum to Brunswick Heads Paddle</p>
	<p>Comments:</p> <div style="display: flex; justify-content: space-between; align-items: flex-end; margin-top: 20px;"> <div style="width: 60%;"> <p>Designed & Inspected By Scott Pieniacki RMS Design & Inspect Traffic Control Plans</p> </div> <div style="width: 35%; text-align: right;"> <p>#7333016695</p> </div> </div>

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<p style="font-size: 0.8em; margin-top: 5px;">1300 800 301</p>	<p>Date: 5/12/2016 Author: Scott Pieniacki Project: Mullum to Brunswick Heads Paddle</p> <p>Comments:</p> <div style="display: flex; justify-content: space-between; align-items: flex-end; margin-top: 20px;"> <div style="flex-grow: 1;"> <p>Designed & Inspected By Scott Pieniacki</p> <p>RMS Design & Inspect Traffic Control Plans</p> </div> <div style="text-align: right;"> <p>#7333016695</p> </div> </div>
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<p style="font-size: x-small; margin-top: 5px;">1300 800 301</p>	<p>Date: 5/12/2016 Author: Scott Pieniacki Project: Mullum to Brunswick Heads Paddle</p> <p>Comments:</p> <div style="display: flex; justify-content: space-between; align-items: flex-end; margin-top: 20px;"> <div style="font-size: x-small;"> Designed & Inspected By Scott Pieniacki RMS Design & Inspect Traffic Control Plans </div> <div style="text-align: center; flex-grow: 1;"> </div> <div style="font-size: x-small; white-space: nowrap;"> #7333016695 </div> </div>
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<p>1300 800 301</p>	<p>Date: 5/12/2016 Author: Scott Pieniacki Project: Mullum to Brunswick Heads Paddle</p> <p>Comments:</p> <div style="display: flex; justify-content: space-between; align-items: flex-end; margin-top: 20px;"> <div style="flex-grow: 1;"> <p>Designed & Inspected By Scott Pieniacki</p> <p>RMS Design & Inspect Traffic Control Plans</p> </div> <div style="text-align: right;"> <p>#7333016695</p> </div> </div>
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Schedule of signs	
	X 3
	X 1
	X 2
	X 1
	X 2
	X 1
	X 4
	X 2
	(R) X 1 (L) X 1 (R & L) X 2

5

Report No. 6.4 Bay Lane Modifications

File No: I2021/363

5 Staff propose to modify the signage and regulatory plan for Bay Lane to improve waste collection.

In July 2020 LTC approved making Bay Lane One Way from Jonson Street to Fletcher Street. However, it is now known that waste collection vehicles are not able to access Bay Lane via Jonson Street due to the inability to manoeuvre into Bay Lane off Jonson Street.

10 Instead, waste collection trucks have been servicing Bay Lane against the legal direction of travel, travelling from Fletcher St and exiting onto Jonson St. This is the only way Bay Lane can be adequately serviced.

Therefore, to allow this to happen legally staff propose the following modifications to Bay Lane (refer to attachment 1):

- i. No longer make Bay Lane One Way
- 15 ii. Remove all One Way signs and lines
- iii. Widen bollards to provide waste trucks with increased room to manoeuvre
- iv. Modify the No Entry signage at the eastern end to say No Entry, Residents and Service Vehicles Excepted.
- 20 v. Install No Stopping 1am -9am signage across 3 car spaces on the north western side Jonson Street.

RECOMMENDATION:

25 **That Council supports removing the One Way from Bay Lane and installing the regulatory signs and lines outlined in Attachment 1 (E2021/40751).**

Attachments:

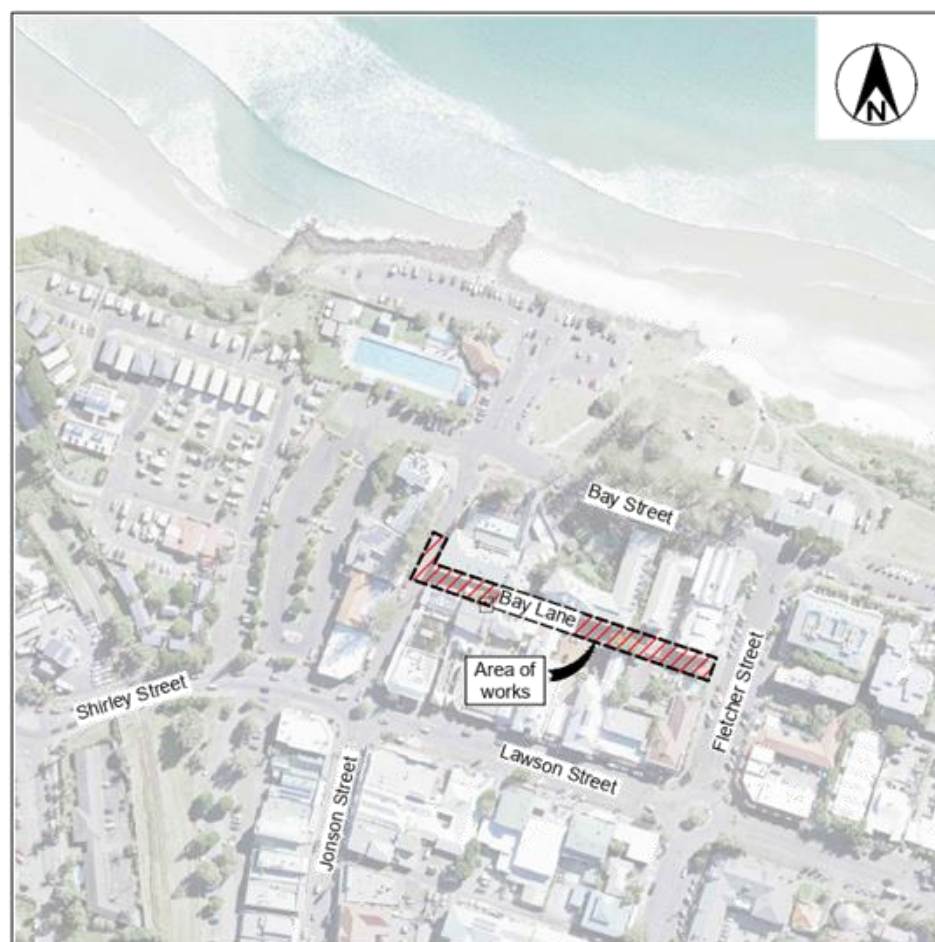
- 30 1 Bay Lane - 2758_CONSTRUCTION ISSUE_rev 2, E2021/40751 , page 26 [↓](#)

Bay Lane Activation Evening Closure Byron Bay



**Byron
Shire
Council**

Approval
on behalf of the General Manager
Director Infrastructure Services
Date:



Locality sketch

Index		
Description	DWG No.	Issue
Index and Locality Sketch	2796-01	2
General Notes	2796-02	1
Site Plan	2796-03	2
Site Plan	2796-04	2
Quantities	2796-05	2

Legend

Issue A, B, C, etc. = Preliminary approvals / tender drawings (**NOT FOR CONSTRUCTION**)
Issue 1, 2, 3, etc. = Construction issue drawings

**Project No
2758**

ISSUED FOR CONSTRUCTION
DATE 02.03.21

Project number:	
2758	
Drawing number	Issue
2758-01	2

ACAD FILE NO: G:\Engineer\CAD\2700-2799\2758 Bay Lane Site Activation Closure for Pop Up Cafe Design\WORKING\CONSTRUCTION\2758_BAY_LANE_REV 2.dwg

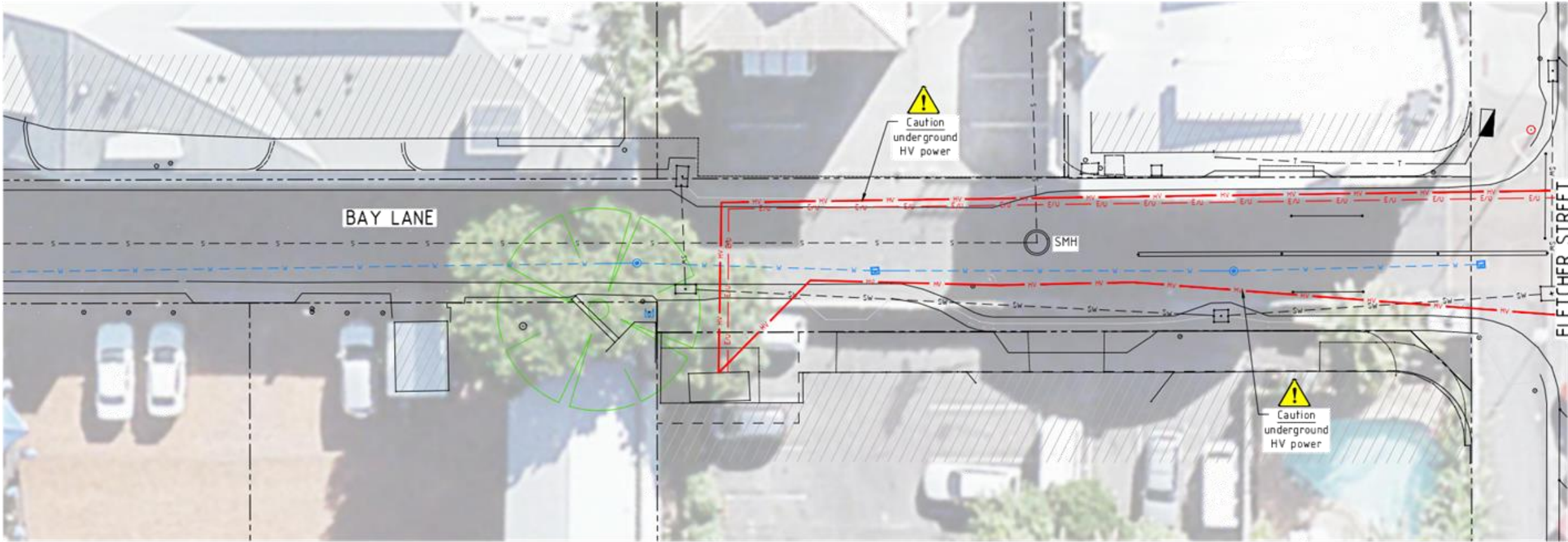
General 1. These drawings shall be read in conjunction with the relevant Northern Rivers Local Government development design and construction manuals and standard drawings. 2. This note and the following notes form an integral part of this drawing set. 3. All dimensions are in metres unless shown otherwise. 4. Dimensions shall not be scaled from the drawings. 5. Materials and workmanship shall be in accordance with the specifications, together with the requirements of all applicable codes of practice, Australian standards and statutory authorities. 6. Survey data has been compiled from field pick-ups and office records. The project manager should ensure that sufficient data is shown to enable construction without disturbance to features that are not shown on the drawings. 7. Services shown hereon have been located where visible on the site, from information received from relevant authorities and from historical records held by Byron shire council. 8. Prior to any demolition, excavation or construction on site, the relevant authorities should be contacted for possible location of further underground services and detailed location of all services (Dial before you dig 1100). 9. The title boundaries shown hereon were not field investigated or marked at the time of survey and have been determined by plan dimensions only. 10. The origin of co-ordinates is MGA. 11. The datum for levels is AHD.				Restoration of surfaces 1. The constructor shall clean pavements, lawns and other improved areas and leave them in the same order as they were at the commencement of the works. The constructor shall restore any fencing removed during construction and shall restore lawns with turf cut and set aside from the original surface and with imported turf from a source approved by the construction engineer. (WSA 02 2002 Part 3, Section 25). 2. Immediately after backfilling of a trench excavated through a pavement has been completed, the constructor shall temporarily restore the pavement. Where the trench crosses bitumen or concrete pavement, the surface is to be protected from deterioration. A pre-mixed asphaltic material may be used for such temporary restoration. The constructor shall maintain the temporary restoration until final restoration is carried out. Final restoration of the pavement shall be carried out to restore the pavement and its sub-base to no less than the original condition. Final restoration may include, if required by the construction engineer, the removal of temporary restoration. 3. In other than roadways, the constructor shall place the backfill sufficiently high to compensate for expected settlement and further backfilling shall be carried out or the original backfill trimmed at the end of the defects liability period in order that the surface of the completed trench may then conform with the adjacent surface. Surplus material shall be removed and disposed of to areas arranged by the constructor. Where dry weather conditions have persisted after the original backfilling, including during the defects liability period, the constructor shall take all necessary steps to consolidate the trench before removing surplus materials from the site. 4. In locations where, in the opinion of the construction engineer, surplus material left in the vicinity of the trench would not be objectionable, the surplus material may be disposed by spreading neatly in the vicinity of the trench to the satisfaction of the construction engineer in such a way as to avoid future erosion of the backfill and adjacent ground surfaces. The constructor shall maintain the backfill and adjacent ground until the expiry of the defects liability period. 5. Where, within public or private property, the reasonable convenience of persons will require such, the construction engineer may order the constructor to level trenches at the time of backfilling. The constructor shall make good any subsequent settlement, as required by placing additional fill. 6. The constructor shall immediately restore any damaged or disturbed private property and services. 7. Should the constructor elect to tunnel under paving, kerb and gutter or other improved surfaces in lieu of trenching, backfilling shall be so carried out as to restore full support to those surfaces. The constructor shall remain responsible for the repair of the improved surfaces, if subsequently damaged due to subsidence of the backfill, until the end of the defects liability period. 8. The constructor shall provide notice to affected property owners of any pending works.				Existing services 1. The constructor shall be responsible for the location of existing services prior to commencing with the works. 2. The constructor shall be responsible for the replacement of any existing services damaged during construction with new services of equivalent type and specifications. 3. The constructor shall be responsible for liaising with telecommunications and electrical supply authorities with supply and fitment of replacement telecommunications and electricity pits and/or lids to suit his works program 4. When constructing or working near existing pressure mains it should be expected that there are concrete thrust blocks located at bends or other fittings on the existing main. It is very important not to disturb the bearing soil behind the thrust block to avoid failure of the existing pressure main. If excavation around existing thrust blocks can not be avoided then the existing pressure main shall be taken off line during the excavation works.				Proposed services 1. After laying and jointing of a pipeline has been completed the constructor shall present the laid and jointed pipes for inspection by the construction engineer prior to commencement of trench backfilling. (WSA 02 2002, section 21). 2. Backfill shall not be placed until the construction engineer has given approval. 3. Material for the side support and overlay of the pipe shall be as for pipe bedding specified in clause C402.23. The material shall be compacted in layers of not more than 150mm to 95 per cent of the standard maximum dry density of the material used when determined in accordance with AS 1289.5.7.1. 4. 4. The constructor shall backfill the remainder of the excavation and compact the backfill in layers of not more than 150mm thick in accordance with WSA 02-2002 Part 3, Section 21.1. 5. Where the trench is within a roadway, proposed roadway, or footpath area, the remainder of the trench shall be: backfilled with a non-cohesive granular material, with a grading falling generally within the limits shown in Table C402.3, and compacted to density index of 70 when determined in accordance with AS 1289.5.4.1 for cohesionless materials A. Below 0.5m of the road surface B. In the road reserve, but excluding the road pavement - backfilled with excavated material, and compacted to 100 per cent of the standard maximum dry density of the material when determined in accordance with AS 1289.5.7.1, to within 0.5m of the road surface, but excluding the pavement layers. - Backfilled with road base and sub-base material as per existing or proposed pavement layers and compacted to 100 per cent of the standard maximum dry density of the material when determined in accordance with AS 1289.5.7.1 - Elsewhere, unless stated otherwise, the remainder of the trench shall be backfilled with ordinary excavated backfill material. Where suitable material is not available, granular material may be used for the full depth of backfilling. The material shall be compacted to a density index of 70 when determined in accordance with AS 1289.5.4.1 for cohesionless materials or 98 per cent of the standard maximum dry density of the material when determined in accordance with AS 1289.5.7.1 for cohesive materials. 6. The constructor shall carry out backfilling and compaction without damaging the pipe or its external coating or wrapping or producing any movement of the pipe. 7. The constructor shall carry out compaction tests 75mm to 100mm below the level being tested (WSA 02-2002 Part 3, Section 22.3). 8. The constructor may compact backfill by trench flooding only where: (A) The ground and backfill material is cohesionless and (B) Water for flooding has been sourced at the site. (C) The process will not create mud which would be moved off site by vehicles or construction plant. (D) Additives are not used.																																				
Site works 1. All soils containing organic matter (e.g. roots, grass etc.) must be stripped from the construction site prior to filling / building works and must not be used as fill material. 2. All exposed surfaces shall be grassed or paved to prevent scour and erosion damage. 3. The constructor is responsible for implementing all necessary sedimentation and erosion control measures specified or deemed necessary to protect the works and adjacent areas. 4. The constructor is responsible for the maintenance and management of a temporary and / or permanent erosion and sedimentation controls during the construction and maintenance period. 5. All oversized material, which may impede compaction, must be removed from the fill platform. 6. Fill is to be uniformly compacted in up to 200-300mm horizontal layers and must achieve a minimum standard of compaction of greater than 95% standard compaction to AS 1289 for cohesive soils, or a density index of greater than 65% for cohesionless soils. Benching of the natural ground will be required on sloping ground prior to commencement of fill operations. 7. Clays of high plasticity or high in-situ moisture content are not to be used as fill. 8. An imported granular fill with a plasticity index preferably less than 15%, with no excessive oversized material, may be used. 9. Field density tests, or equivalent, should be carried out to verify that the standard of compaction is achieved. Field density tests are to be taken over the full depth of the layer or from the bottom of the layer.				Concrete All workmanship and materials shall be in accordance with A.S.3600, current edition with amendments 1. Concrete quality (unless otherwise shown) shall be as follows • course aggregate - maximum size 20mm • cement - type "A" Portland cement. • concrete shall have the following slump during placement - beams ,slabs and footings 80mm - columns and walls 80mm 3. Slab joints shall be placed as follows • footpaths - as per Northern Rivers Local Government standard drawing R-07 • Slabs and walls - refer to slab jointing plan within this drawing set • Slab sawn joints shall be cut within 24 hours of slab pouring in a neat and straight cut. 4. All splatter to surrounding surfaces shall be cleaned up immediately 5. Cover to reinforcement shall be obtained by the use of plastic bar chairs with maximum spacing of 800mm in any direction 6. All concrete shall be compacted using high frequency vibrators. 7. Curing of concrete surfaces shall commence immediately after surfaces are finished and shall continue to cure for a minimum of 7 days 8. Slabs with specific rough finishes shall be kept free of bleed water and floated to prevent the formation of plastic shrinkage cracks.				Driveways 1. All existing driveways affected by new works are to be cut back, removed & reconstructed using material to match existing. 2. The constructor shall liaise with the property owners regarding any variation to the above. 3. Reconstruction of existing concrete driveway or pathway is to be in accordance with Northern Rivers Local Government D1.37 AND D1.38 "Handbook for driveway access to property" and relevant standard drawings . 4. Reconstruction of existing bitumen sealed driveway shall be of similar construction to that of the existing with a compacted gravel base course																																								
<table border="1"> <tr> <td>1</td> <td>Construction Issue</td> <td>J.B.</td> <td>K.H.</td> <td>18.06.20</td> </tr> <tr> <td>Issue</td> <td>Amendment details</td> <td>Drawn</td> <td>Check</td> <td>Date</td> </tr> </table>				1	Construction Issue	J.B.	K.H.	18.06.20	Issue	Amendment details	Drawn	Check	Date	Infrastructure Services Council offices 70-90 Station Street, Mullumbimby NSW 2482. Phone 02 66267000 Fax 02 66843018 Website www.byron.nsw.gov.au								Approved on behalf of the General Manager Date: _____ <table border="1"> <tr> <td>Designed</td> <td>J.B.</td> <td>18.06.20</td> </tr> <tr> <td>Drawn</td> <td>J.B.</td> <td>18.06.20</td> </tr> <tr> <td>Checked</td> <td>K.H.</td> <td>18.06.20</td> </tr> <tr> <td>Horizontal datum</td> <td colspan="2">MGA</td> </tr> <tr> <td>Vertical datum</td> <td colspan="2">AHD</td> </tr> </table>				Designed	J.B.	18.06.20	Drawn	J.B.	18.06.20	Checked	K.H.	18.06.20	Horizontal datum	MGA		Vertical datum	AHD		Project: Bay Lane Activation Evening Closure Byron Bay Plan title: General Notes				Project number: 2758 Drawing number: 2758-02 Issue: 1			
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ACAS FILE NO: G:\Engineering\2700-2799\2758 Bay Lane\Site plans\ Closure for Pop Up\CH Design\WORKING\STRUCTURE\2758_BAY LANE_REV 2.dwg



Drawing number	Issue
2758-03	2

Continued on Drawing 2758-03



Site Plan
Scale 1:200 (A3)

- Legend**
- Property boundary
 - Existing tree
 - Existing telecommunications pit
 - Existing sewer gravity main
 - Existing storm water pipe
 - Existing water main
 - Existing underground high voltage power
 - Existing sewer maintenance hole
 - Existing underground power
 - Existing overhead power
 - Existing power / telecommunications pole
 - Existing fence line
 - Proposed sign
 - Proposed fixed bollard
 - Proposed automated bollard

Note: Existing underground services shown are from BSC records and Dial Before You Dig documentation only. Constructor to accurately locate existing services prior to commencement of works.

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Diagram illustrating the required signage for a right-turn lane:

- NO STOPPING** (10m x 10m) X 1
- RIGHT TURN ONLY** (10m x 10m) X 1
- Custom 'RIGHT TURN ONLY'

[illegible]