

# NOTICE OF MEETING



## EXTRAORDINARY LOCAL TRAFFIC COMMITTEE MEETING

An Extraordinary Local Traffic Committee Meeting of Byron Shire Council will be held as follows:

<b>Venue</b>	<b>Conference Room, Station Street, Mullumbimby</b>
<b>Date</b>	<b>Tuesday, 16 March 2021</b>
<b>Time</b>	<b>10:30am</b>
<b>Committee Members</b>	<b>Ian Shanahan – Transport for NSW Detective Chief Inspector Matt Kehoe – Police Cr Basil Cameron Tamara Smith MP</b>

**BYRON SHIRE COUNCIL**  
EXTRAORDINARY LOCAL TRAFFIC COMMITTEE MEETING

**BUSINESS OF MEETING**

**1. APOLOGIES**

**2. DECLARATIONS OF INTEREST – PECUNIARY AND NON-PECUNIARY**

**3. ADOPTION OF MINUTES FROM PREVIOUS MEETINGS**

3.1 Local Traffic Committee Meeting held on 16 February 2021

**4. MATTERS ARISING**

**5. OUTSTANDING ISSUES/RESOLUTIONS**

**6. REGULATORY MATTERS**

6.1	Brunswick Heads - No Parking Signage .....	3
6.2	Event - Byron Writers Festival 2021 .....	5
6.3	Event - Mullum2Bruns Paddle .....	17
6.4	Bay Lane Modifications.....	25

REGULATORY MATTERS

Report No. 6.1 Brunswick Heads - No Parking Signage

File No: I2021/204

5

Council recently introduced No Parking 1am - 6am along South Beach Road Brunswick Heads. This has reduced the illegal camping problem along the beach front but has pushed campers to a couple other locations within Brunswick Heads, namely:

- i) Along the River front. See the yellow highlighted area in figure 1.
- 10 ii) In the Stan Thompson Oval area on the southern approach into town, see yellow area in figure 2.

To help Council's compliance team better manage illegal camping in these areas Council requests the installation of:

- 15 i) No Parking 1am-6am signs along the river front streets highlighted yellow in Figure 1. Blue dots show the typical location of proposed signs.
- ii) No Parking 1am-6am area signs at the entrance to Stan Thompson Oval area. Blue line at the entrance shown the typical location proposed for signage.



Figure 1 – Brunswick Heads river frontage signage plan



Figure 2 – Stan Thompson Oval area.

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5

**RECOMMENDATION:**

That Council endorse the installation of:

- 10
1. No Parking 1am-6am signage along the eastern side of Mona Lane, Fawcett St (between Mona Lane and Park Street) and Park Street (between Fawcett Street and Mullumbimbi Street).
  2. No Parking Area 1am-6am signage at the entrance to the Stan Thompson Oval area

## Report No. 6.2 Event - Byron Writers Festival 2021

File No: I2021/300

### Summary

- 5 Council has received an event application with Traffic Management Plan (TMP) for the 2021 Byron Writers Festival to be held between Friday 6<sup>th</sup> August and Sunday 8<sup>th</sup> August 2021.

The event site is located at the northern end of Bayshore Drive, adjacent to Elements Resort. See attached 2021 TMP for location.

- 10 Based on previous events in 2018 and 2019 patron numbers are expected to be in the order of 12 000 tickets sold, with 55% of visitors staying for four days or more, and 12% for over seven days. The festival will therefore contribute to additional traffic in the lead up to, during and after the festival.

### CONSIDERATIONS

- 15 Ewingsdale Road is susceptible to queuing and congestion.

Byron Bypass will be operational and its impact on traffic flow is currently unknown. It is expected to have minimal impact initially.

Refer to attachment 1 for the TMP for the event. In summary it proposes:

- 20
- Wayfinding traffic control signage on Ewingsdale Rd on the approach to Bayshore Drive.
  - 40km/h speed zone north of the Bayshore Drive rail crossing to event car park
  - 10km/h from the car park entry to the end of Bayshore Drive.
- 25
- Retain pedestrian traffic within the parking area as much as possible and provide a pedestrian crossing linking the car park to the event within the 10k/h zone, with traffic controllers
  - Mobility parking to be provided adjacent to the event on Bayshore Drive and a valet service for PWD patrons to allow their vehicles to be parked amongst the regular cars, thus effectively creating more PWD parking spaces.
- 30
- Taxi and Uber drop off point is proposed within the Car Park on the south western side of Bayshore Drive.
  - Bus drop off area is proposed at the end of Bayshore Dr within the cul-de-sac.
  - Traffic exiting the event and heading to Byron Bay is directed down Bayshore Drive.

# BYRON SHIRE COUNCIL

## LOCAL TRAFFIC COMMITTEE MEETING

6.2

- Traffic heading west along Ewingsdale Rd from Byron Town Centre is directed to turn right at the Bayshore Dr Roundabout.
  - As a contingency, staff recommends if significant traffic congestion occurs at the Bayshore Drive Roundabout with east bound traffic backing up towards The Farm then west bound traffic arriving from Byron Town Centre is to be directed towards the event via Sunrise Blvd.

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### RECOMMENDATION:

10 That Council support the Byron Bay Writers Festival 6-8<sup>th</sup> August 2021, subject to:

- 15 1) Development and implementation of a Traffic Management Plan (TMP) and Traffic Control Plan(s) (TCP) by those with appropriate TfNSW accreditation and the holding of current and appropriate levels of insurance and liability cover. The TMP and TCP to be generally in accordance with Attachment 1 (E2021/38636);
- 2) West bound event traffic arriving from Byron Town Centre to be directed down Sunrise Blvd, as a contingency, if east bound traffic on Ewingsdale Rd is queued back to the Ewingsdale Road interchange;
- 3) Separate approvals by NSW Police and RMS being obtained;
- 20 4) The event organiser to:
  - 25 i) Advertise the impact of the event, via a Variable Message Signage (VMS) and notice in the local weekly paper, a minimum of one week prior to the operational impacts taking effect, noting it must include the event name, specifics of any traffic impacts or road closures and times, alternative route arrangements, event organiser, a personal contact name and a telephone number for all event related enquiries or complaints;
  - 30 ii) Arrange the event being notified on Council's webpage a minimum one week prior to the TCP being implemented;
  - iii) undertake consultation with community and affected businesses including adequate response/action to any raised concerns during and after the event;
  - iv) undertake consultation with emergency services and any identified issues addressed;
  - v) hold \$20m public liability insurance cover which is valid for the event.

### 35 Attachments:

- 1 Byron Writers Fest - TMP, E2021/38636 , page 8 [↓](#)

**BYRON SHIRE COUNCIL**

LOCAL TRAFFIC COMMITTEE MEETING

6.2

Ingen Consulting  
Alstonville NSW 2477  
0417 264 987  
michiel@ingenconsulting.com.au  
www.ingenconsulting.com.au



Our Reference: J1130\_TMP2

General Manager  
Byron Shire Council  
PO Box 219  
Mullumbimby NSW 2482

8<sup>th</sup> February 2021

Attn: Andrew Pearce

**Re: Byron Writers Festival 2021 – Traffic Management Plan**

Dear Andrew,

The Byron Writers Festival have engaged our services to prepare a Traffic Management Plan (TMP) for the 2021 Byron Writers Festival, at Bayshore Drive in Byron Bay.

**Pacific Motorway**

Given that congestion in the southbound lane of the Pacific Motorway leading up to the Byron Bay exit is a daily occurrence, the Byron Writers Festival should not be considered the cause of this congestion on festival days. It is recognised however that the festival is a contributor of traffic on the Pacific Motorway. In recognition of this and the delays caused for festival patrons travelling to and from the festival site, festival management is in the process of implementing management techniques that improve the mode share characteristics of the festival traffic.

**Speed zones**

Similar to previous years, we have included a 40km/h speed zone from the rail crossing to the car park entrance on Bayshore Drive, and 10km/h from the car park entry to the end of Bayshore Drive. The latter would improve the safety of any pedestrian-vehicle interactions that may occur on Bayshore Drive. These speed zones are depicted on the attached Traffic Control Plans.

Civil | Traffic | Noise | Project Management | Contract Management

Byron Writers Festival 2021  
Traffic Management Plan



### **Pedestrian crossing**

We propose to retain the pedestrian crossing (from the car park to the footpath) at the same location as last year. We have also included the 10 km/h speed zone north of the car parking entry and pedestrian fencing along the western side of the swale. We conclude that this will result in a significant increase in pedestrian safety, as the likelihood of crossing is reduced due to the fencing, and the consequence of an unwanted pedestrian-vehicle interaction is reduced due to the low vehicle speed.

### **Bus and taxi movement plan**

Similarly to last year, a bus and taxi movement plan is included in the set of drawings. With the increased use of drop offs by services such as Uber, a suitable and efficient drop off zone is allocated within the car parking area close to the entrance. The design and placement of internal signage to facilitate the vehicle flows as indicated in the bus and taxi movement plan is not included in this TMP (which is intended to describe signage in the public road only) and will be included in the festival's internal management documentation.

### **Traffic management strategies**

A number of traffic management strategies, outside the scope of the traffic management plan for the festival, are being assessed for suitability to include in the management of traffic at this year's festival.

These include but may not be limited to:

- A valet service for PWD patrons to allow their vehicles to be parked amongst the regular cars, thus effectively creating more PWD parking spaces
- Strategies to improve the use of buses
- Strategies to reduce the use of cars

Council will be informed of progress on these items after each strategy has been assessed for suitability and economy.

### **Traffic Guidance Schemes**

The attached traffic guidance schemes reflect the above descriptions. If you have any questions, please do not hesitate to contact our office.

Byron Writers Festival 2021  
Traffic Management Plan



Yours sincerely,

A handwritten signature in blue ink, appearing to read "MK", written over a light blue circular stamp.

**Michiel Kamphorst**

*MSc. BSc. RPEng. RPEQ. NER (Civil)*

*Prepare a Work Zone Traffic Management Plan 0052298287*

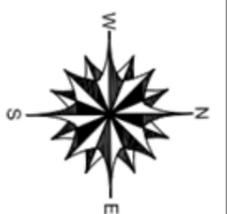
**Attachments:** Traffic Guidance Schemes

# TRAFFIC GUIDANCE SCHEMES for 2021 BYRON WRITERS FESTIVAL at Bayshore Drive, Byron Bay

DRAWING TABLE	
DRAWING NUMBER	TITLE
J1130_TGS_01	Title page
J1130_TGS_02	Ewingsdale Road directional signage
J1130_TGS_03	Departure directional signage
J1130_TGS_04	Bayshore Drive
J1130_TGS_05	Details
J1130_TGS_06	Bus and taxi movement plan
REVISION	
0	
0	
0	
0	
0	
0	



Subject site



<b>0</b>	DESCRIPTION: S138 application	DATE: 8/2/21	DRAWN: MK	REVIEWED: MK	APPROVED: MK
<p style="font-size: small;">APPROVED BY: <i>Michel Kamphorst</i>                  Michel Kamphorst,                  MSc, BSc, RPEng, RPEQ, NER                  Prepare a Work Zone Traffic                  Management Plan 0052298287</p> <p style="font-size: small;">Date: 8th February 2021</p>					



INGEN

CONSULTING

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CLIENT:  
 Byron Writers Festival  
 PO Box 1846  
 Byron Bay NSW 2481

PROJECT:  
 2021 Byron Writers Festival  
 Traffic Guidance Schemes

DRAWING TITLE:  
 Title Page

PROJECT NUMBER:  
 J1130

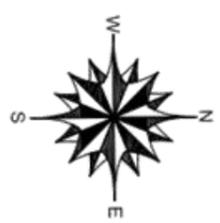
DRAWING NUMBER:  
 J1130\_TGS\_01

ORIGINAL SIZE:  
 A3

REVISION:  
 0

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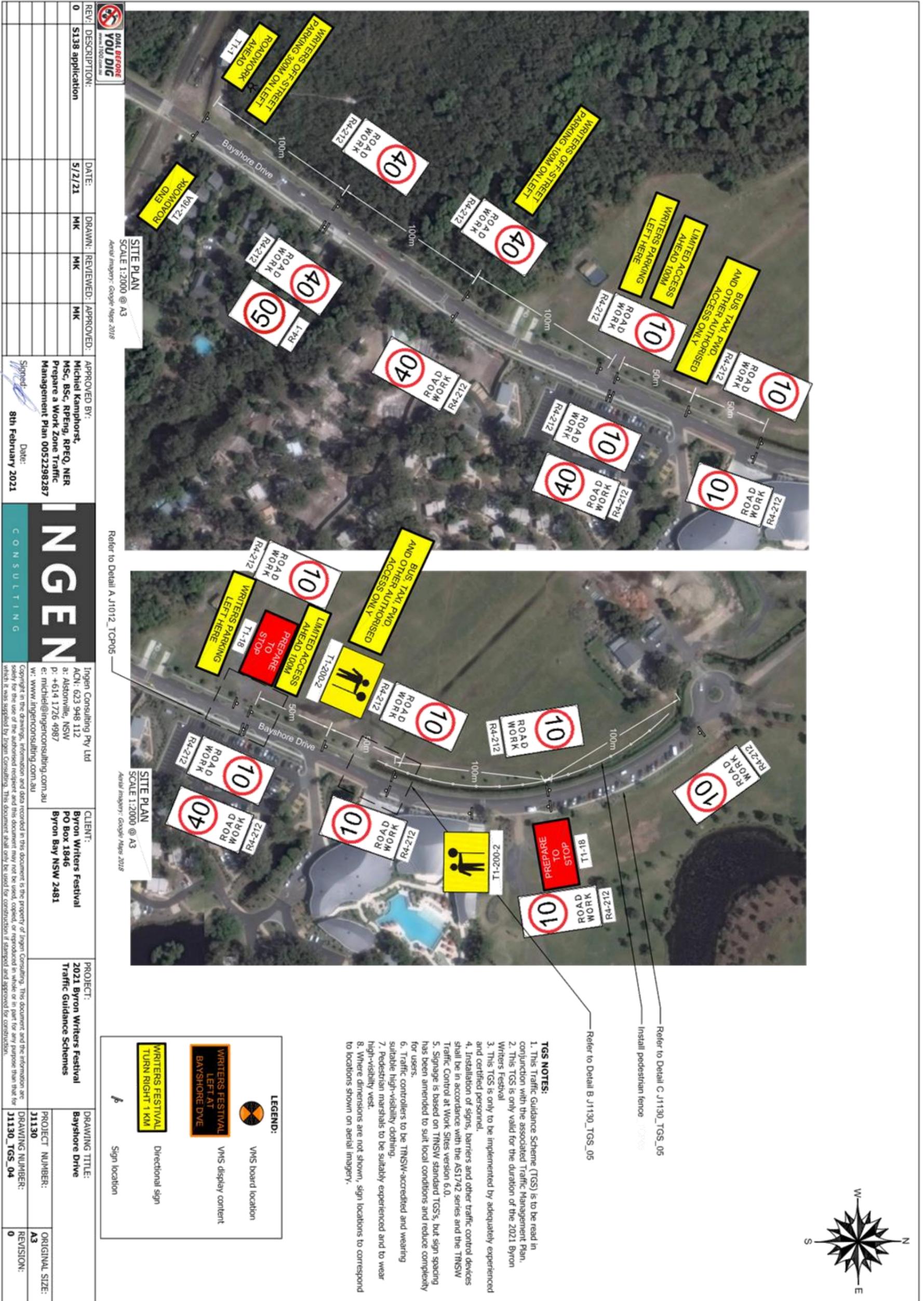


- TGS NOTES:**
1. This Traffic Guidance Scheme (TGS) is to be read in conjunction with the associated Traffic Management Plan.
  2. This TGS is only valid for the duration of the 2021 Byron Writers Festival
  3. This TGS is only to be implemented by adequately experienced and certified personnel.
  4. Installation of signs, barriers and other traffic control devices shall be in accordance with the AS1742 series and the TNSW Traffic Control at Work Sites version 6.0.
  5. Signage is based on TNSW standard TGS's, but sign spacing has been amended to suit local conditions and reduce complexity for users.
  6. Traffic controllers to be TNSW-accredited and wearing suitable high-visibility clothing.
  7. Pedestrian marshals to be suitably experienced and to wear high-visibility vest.
  8. Where dimensions are not shown, sign locations to correspond to locations shown on aerial imagery.

**LEGEND:**

- VMS board location
- WRITERS FESTIVAL LEFT AT BAYSHORE DVE
- VMS display content

REV: 0	DESCRIPTION: S138 application	DATE: 8/2/21	DRAWN: MK	REVIEWED: MK	APPROVED: MK	APPROVED BY: Michel Kamphorst, MSC, BSC, REPEQ, REEQ, NER	CLIENT: Byron Writers Festival PO Box 1846 Byron Bay NSW 2481	PROJECT: 2021 Byron Writers Festival Traffic Guidance Schemes	DRAWING TITLE: Departure directional signage	DRAWING NUMBER: 11130_TGS_03	ORIGINAL SIZE: A3	REVISION: 0
<p><b>DIAL BEFORE YOU DIG</b> www.1100.com.au</p>						<p>SCALE 1:1000 @ A3 Aerial Imagery: Google Maps 2018</p>						
<p>SCALE 1:5000 @ A3 Aerial Imagery: NSW Planning Portal</p>						<p>APPROVED: 8th February 2021</p>						
<p>INGEN CONSULTING</p>						<p>Ingen Consulting Pty Ltd ACN: 623 948 112 a: Alstonville, NSW p: +614 1726 4987 e: michiel@ingenconsulting.com.au w: www.ingenconsulting.com.au</p>						
<p>Copyright in the drawings, information and data recorded in this document is the property of Ingen Consulting. This document, and the information are solely for the use of the authorized recipient and this document may not be used, copied, or reproduced in whole or in part for any purpose other than that for which it was supplied by Ingen Consulting. This document shall only be used for construction if stamped and approved for construction.</p>												



REV: 0	DESCRIPTION: S138 application	DATE: 5/2/21	DRAWN: MK	REVIEWED: MK	APPROVED: MK
<p><b>APPROVED BY:</b> Michel Kamphorst, MSC, BSC, RPEng, RPEQ, NER                  Prepare a Work Zone Traffic Management Plan 0052298287</p> <p><b>Signed:</b> [Signature] <b>Date:</b> 8th February 2021</p>					

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Refer to Detail A J1012\_TGPOS

Refer to Detail B J1130\_TGS\_05

Refer to Detail C J1130\_TGS\_05

Install pedestrian fence

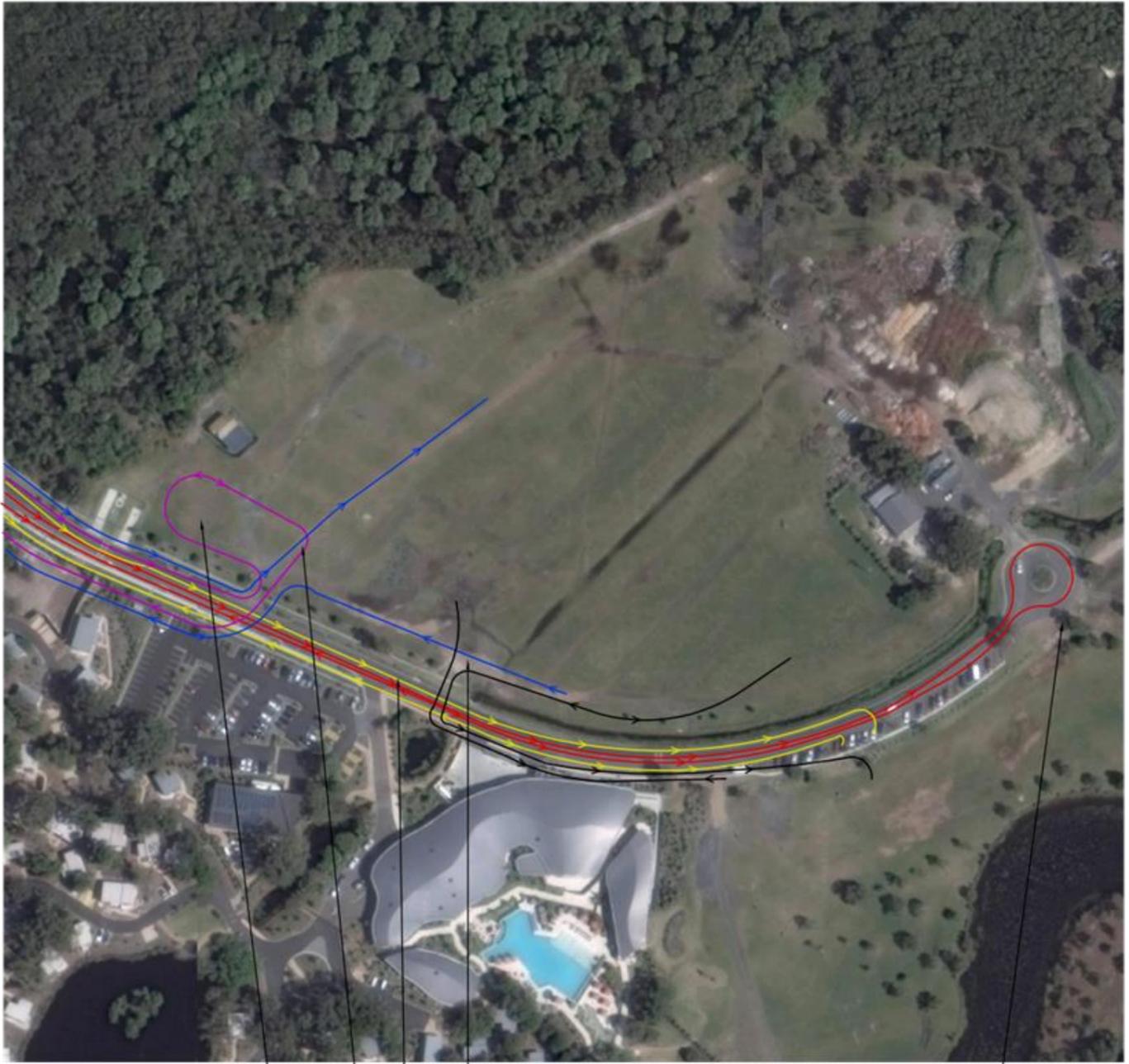
Refer to Detail A J1012\_TGPOS

Refer to Detail B J1130\_TGS\_05

Refer to Detail C J1130\_TGS\_05

Install pedestrian fence





Bus and mini-bus set down area

Internal conflict points to be managed by suitably experienced staff

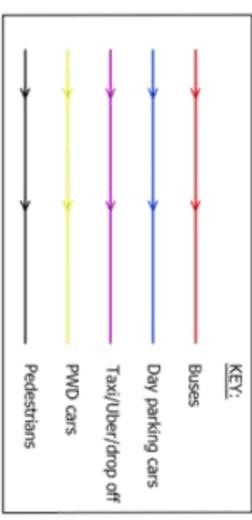
No cars other than PWD past this point

Internal conflict points to be managed by suitably experienced staff

Create dedicated drop-off area



- TGS NOTES:**
1. This Traffic Guidance Schemes (TGS) is to be read in conjunction with the associated Traffic Management Plan.
  2. This TGS is only valid for the duration of the 2021 Byron Writers Festival
  3. This TGS is only to be implemented by adequately experienced and certified personnel.
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  6. Traffic controllers to be TNSW-accredited and wearing suitable high-visibility clothing.
  7. Pedestrian marshals to be suitably experienced and to wear high-visibility vest.
  8. Where dimensions are not shown, sign locations to correspond to locations shown on aerial imagery.



REV: 0	DESCRIPTION: S138 application	DATE: 8/2/21	DRAWN: MK	REVIEWED: MK	APPROVED: MK
APPROVED BY: <b>Michel Kamphorst, MSC, BSC, RPEng, RPEQ, NER</b> Prepare a Work Zone Traffic Management Plan 0052298287 Date: <b>8th February 2021</b>					

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**CLIENT:**  
 Byron Writers Festival  
 PO Box 1846  
 Byron Bay NSW 2481

**PROJECT:**  
 2021 Byron Writers Festival  
 Traffic Guidance Scheme

<b>DRAWING TITLE:</b>	Bus and taxi movement plan
<b>PROJECT NUMBER:</b>	11130
<b>DRAWING NUMBER:</b>	11130_TGS_06
<b>ORIGINAL SIZE:</b>	A3
<b>REVISION:</b>	0

## Report No. 6.3 Event - Mullum2Bruns Paddle

File No: I2021/302

### SUMMARY

- 5 Council has received an application from the Mullum2Bruns Paddle Committee for the Mullum2Bruns Paddle 2021 to be held Sunday 23rd May 2021.

### BACKGROUND

10 The event will be the same as the 2020 Traffic Management Plan and Traffic Control Plan. The 2020 TCP has been attached for reference but has not yet been updated to reflect 2021 dates. Below is a general summary of what traffic control is proposed as part of the 2021 event.

15 To ensure greater safety around the registration and marshalling area the event proposes temporary one-way traffic off Brunswick Terrace, between Tincogan Street and Tyagarah Street, Mullumbimby, between 6:30am and 10:00am. The direction of the one way is shown in the image below.



Additional traffic control measures proposed include:

- The right turn off Tincogan Street into Brunswick Terrace to have a “no right turn” sign
  - The left turn off Tincogan St into Brunswick Terrace to have a “no left turn”
- 20

# BYRON SHIRE COUNCIL

## LOCAL TRAFFIC COMMITTEE MEETING

6.3

- Relevant detour signs will be installed at the approaches to the impacted area
  - No parking signs along the one way part of Brunswick Terrace
  - 5 • Participants will be diverted along the southern part of Brunswick Terrace to the intersection of Brunswick Terrace & Tincogan Street to enable them to cross to the starting area. The traffic controllers will hold traffic crossing to the registration zone until there are spaces for them to unload their craft.
  - Uniformed Marine Rescue personnel will be patrolling the homes opposite the registration area to ensure that participants are not parking in front of or across driveways, thereby minimising inconvenience to the residents.
  - 10 • A letter-box drop would be done 2 weeks prior to the event, informing residents in the surrounding streets of the temporary one-way traffic closures.
- 

### 15 **RECOMMENDATION:**

**That Council endorse the Mullum2Brunns Paddle 2020 to be held Sunday 23 May 2021, subject to:**

1. **The development and implementation of a Traffic Management Plan (TMP) and Traffic Control Plan(s) (TCP) by those with appropriate NSW accreditation.**
- 20 2. **The TMP and TCP is to include, but not limited to, the following:**
  - a) **One way traffic along Brunswick Terrace, Mullumbimby, between 06:30am and 10:00am on Sunday 23<sup>rd</sup> May 2021. The one way is to be from Tyagarah Street to Tincogan Street;**
  - 25 b) **The right turn off Tincogan Street into Brunswick Terrace to have a “no right turn” sign;**
  - c) **The left turn off Tincogan St into Brunswick Terrace to have a “no left turn”;**
  - d) **Relevant detour signs at the approaches to the impacted area;**
  - e) **No parking signs along the one way part of Brunswick Terrace.**
- 3) **The event organiser to:**
  - 30 a) **Advertise the impact of the event, via a Variable Message Signage on site and notice in the local weekly paper, a minimum of one week prior to the operational impacts taking effect, noting it must include the event name, specifics of any traffic impacts or road closures and times, alternative route arrangements, event organiser, a personal contact name and a telephone**
  - 35 **number for all event related enquiries or complaints;**

# BYRON SHIRE COUNCIL

## LOCAL TRAFFIC COMMITTEE MEETING

6.3

**b) Arrange for the event to be notified on Council's webpage a minimum one week prior to the TCP being implemented;**

5

**c) undertake consultation with relevant community and affected businesses including adequate response/action to any raised concerns during and after the event;**

**d) undertake consultation with emergency services and any identified issues addressed;**

**e) hold \$20m public liability insurance cover which is valid for the event.**

### **Attachments:**

10

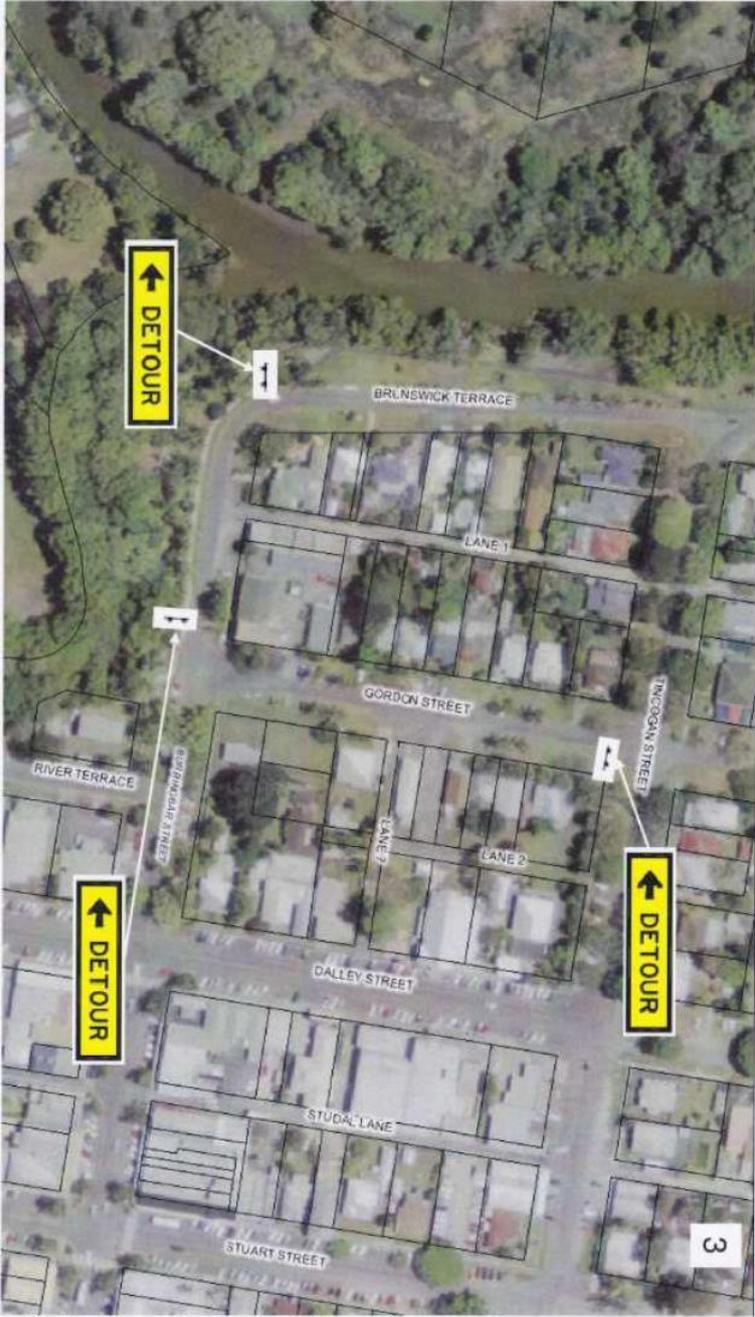
- 1 42.2020.6.1 Event info - Mullum2Bruns Paddle - 24 May 2020 - traffic control plans, E2020/2760 , page 20 [↓](#)



 1300 800 301	<b>Date:</b> 5/12/2016 <b>Author:</b> Scott Pieniacki <b>Project:</b> Mullum to Brunswick Heads Paddle
	<b>Comments:</b>  Designed & Inspected By Scott Pieniacki .....#7333016695 RMS Design & Inspect Traffic Control Plans

[www.invarion.com](http://www.invarion.com)

<p style="font-size: small; margin-top: 5px;">1300 800 301</p>	<p><b>Date:</b> 5/12/2016 <b>Author:</b> Scott Pieniacki <b>Project:</b> Mullum to Brunswick Heads Paddle</p> <p><b>Comments:</b></p> <div style="text-align: right; margin-top: 10px;"> </div> <p style="font-size: small; margin-top: 5px;">Designed &amp; Inspected By Scott Pieniacki .....#7333016695 RMS Design &amp; Inspect Traffic Control Plans</p>
----------------------------------------------------------------	-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------



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 1300 800 301	<b>Date:</b> 5/12/2016 <b>Author:</b> Scott Pieniacki <b>Project:</b> Mullum to Brunswick Heads Paddle
	<b>Comments:</b>  <i>Matthew</i> Designed & Inspected By Scott Pieniacki .....#7333016695 RMS Design & Inspect Traffic Control Plans

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<p>1300 800 301</p>	<p><b>Date:</b> 5/12/2016 <b>Author:</b> Scott Pieniacki <b>Project:</b> Mullum to Brunswick Heads Paddle</p> <p><b>Comments:</b></p>
	<p>Designed &amp; Inspected By Scott Pieniacki .....#7333016695                  RMS Design &amp; Inspect Traffic Control Plans</p>

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Schedule of signs	
	X 3
	X 2
	X 1
	X 1
	X 2
	X 1
	X 4
	X 2
	(R) X 1 (L) X 1 (R & L) X 2

5

## Report No. 6.4 Bay Lane Modifications

File No: I2021/363

5 Staff propose to modify the signage and regulatory plan for Bay Lane to improve waste collection.

In July 2020 LTC approved making Bay Lane One Way from Jonson Street to Fletcher Street. However, it is now known that waste collection vehicles are not able to access Bay Lane via Jonson Street due to the inability to manoeuvre into Bay Lane off Jonson Street.

10 Instead, waste collection trucks have been servicing Bay Lane against the legal direction of travel, travelling from Fletcher St and exiting onto Jonson St. This is the only way Bay Lane can be adequately serviced.

Therefore, to allow this to happen legally staff propose the following modifications to Bay Lane (refer to attachment 1):

- i. No longer make Bay Lane One Way
- 15 ii. Remove all One Way signs and lines
- iii. Widen bollards to provide waste trucks with increased room to manoeuvre
- iv. Modify the No Entry signage at the eastern end to say No Entry, Residents and Service Vehicles Excepted.
- 20 v. Install No Stopping 1am -9am signage across 3 car spaces on the north western side Jonson Street.

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### RECOMMENDATION:

25 **That Council supports removing the One Way from Bay Lane and installing the regulatory signs and lines outlined in Attachment 1 (E2021/40751).**

### Attachments:

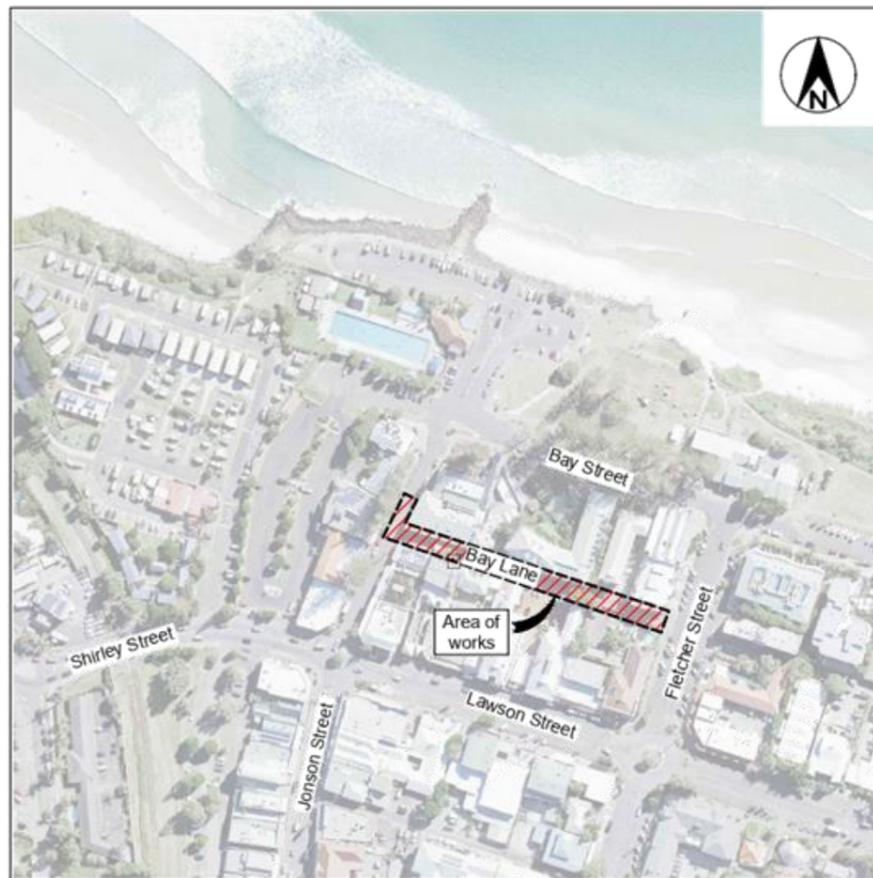
30 1 Bay Lane - 2758\_CONSTRUCTION ISSUE\_rev 2, E2021/40751 , page 26 [↓](#)

# Bay Lane Activation Evening Closure Byron Bay



**Byron  
Shire  
Council**

**Approval**  
on behalf of the General Manager  
.....  
Director Infrastructure Services  
Date: .....



Locality sketch

Index		
Description	DWG No.	Issue
Index and Locality Sketch	2796-01	2
General Notes	2796-02	1
Site Plan	2796-03	2
Site Plan	2796-04	2
Quantities	2796-05	2

**Legend**  
Issue A, B, C, etc. = Preliminary approvals / tender drawings (NOT FOR CONSTRUCTION)  
Issue 1, 2, 3, etc. = Construction issue drawings

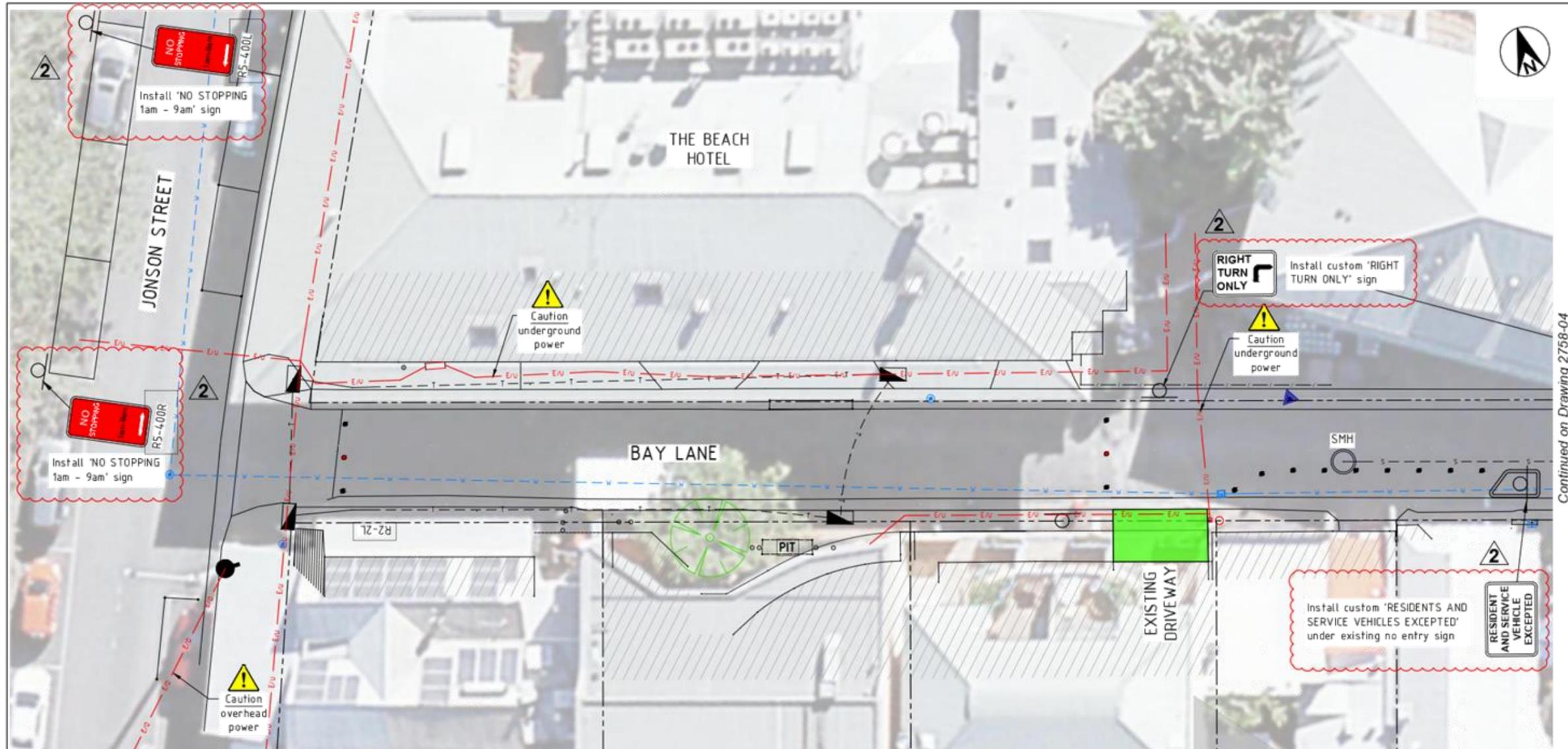
**Project No  
2758**

**ISSUED FOR CONSTRUCTION  
DATE 02.03.21**

Project number:	
<b>2758</b>	
Drawing number	Issue
<b>2758-01</b>	<b>2</b>

ACAD FILE NO: G:\Engineer\CAD\2700-2799\2758 Bay Lane site plans\Q:\Users for Pop Up\Chel Design\WORK\CONSTRUCTION\2758\_BAY\_LANE\_REV 2.dwg





Continued on Drawing 2758-04

Site Plan  
Scale 1:200 (A3)

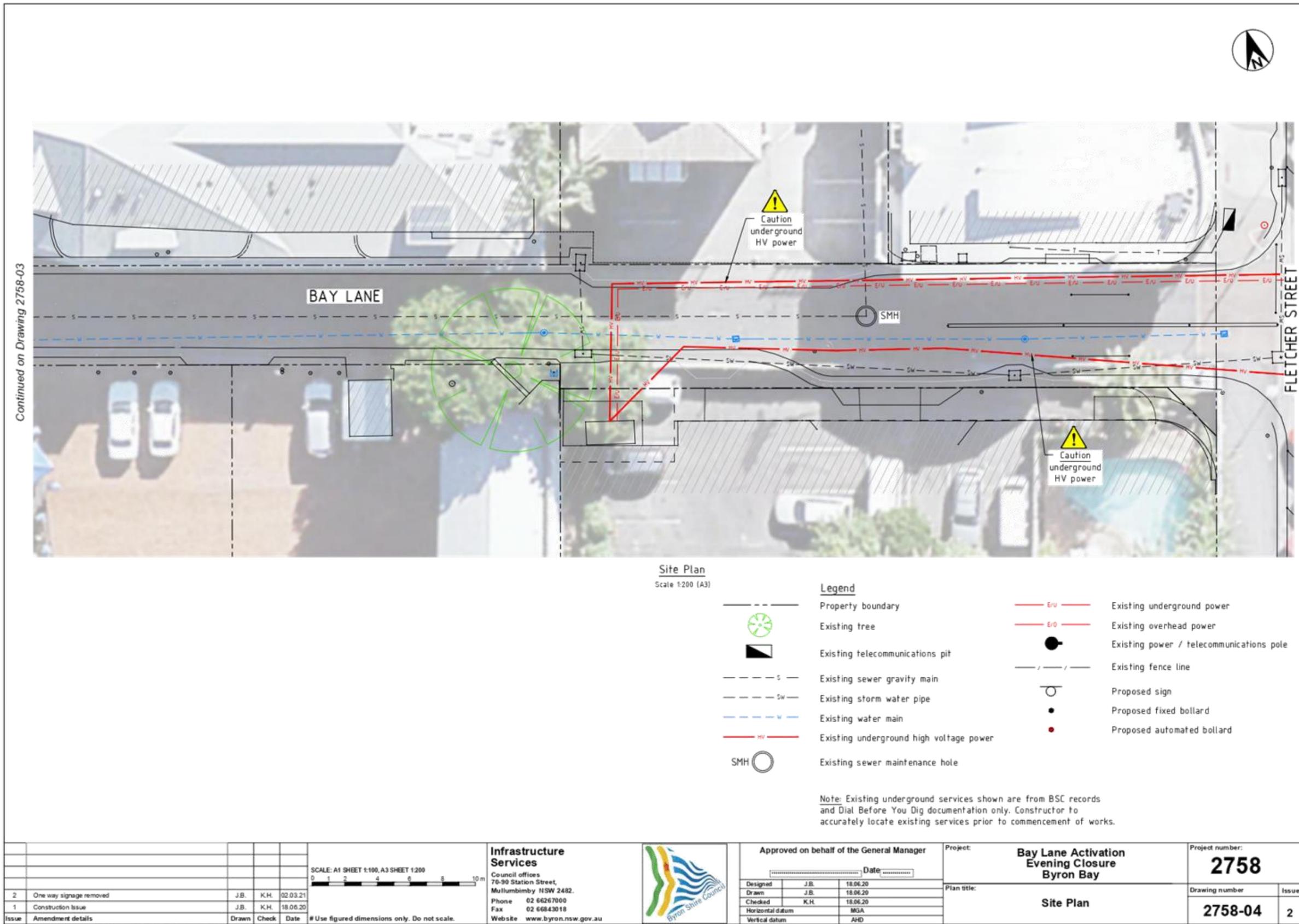
**Notes:**

- Existing underground services shown are from BSC records and Dial Before You Dig documentation only. Constructor to accurately locate existing services prior to commencement of works.

Legend	
	Property boundary
	Existing tree
	Existing telecommunications pit
	Existing sewer gravity main
	Existing storm water pipe
	Existing water main
	Existing underground high voltage power
	Existing sewer maintenance hole
	Existing underground power
	Existing overhead power
	Existing power / telecommunications pole
	Existing fence line
	Proposed sign
	Proposed fixed bollard
	Proposed automated bollard

<p>SCALE: A1 SHEET 1:100, A3 SHEET 1:200</p>		<p><b>Infrastructure Services</b> Council offices 70-90 Station Street, Mullumbimby NSW 2482. Phone 02 66267000 Fax 02 66843018 Website www.byron.nsw.gov.au</p>	<p>Approved on behalf of the General Manager</p> <p>Date: _____</p> <table border="1"> <tr><td>Designed</td><td>J.B.</td><td>18.06.20</td></tr> <tr><td>Drawn</td><td>J.B.</td><td>18.06.20</td></tr> <tr><td>Checked</td><td>K.H.</td><td>18.06.20</td></tr> <tr><td>Horizontal datum</td><td colspan="2">MGA</td></tr> <tr><td>Vertical datum</td><td colspan="2">AHD</td></tr> </table>		Designed	J.B.	18.06.20	Drawn	J.B.	18.06.20	Checked	K.H.	18.06.20	Horizontal datum	MGA		Vertical datum	AHD		<p>Project: <b>Bay Lane Activation Evening Closure Byron Bay</b></p> <p>Plan title: <b>Site Plan</b></p>		<p>Project number: <b>2758</b></p> <p>Drawing number: <b>2758-03</b></p> <p>Issue: <b>2</b></p>	
Designed	J.B.		18.06.20																				
Drawn	J.B.	18.06.20																					
Checked	K.H.	18.06.20																					
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<p>SCALE: A1 SHEET 1:50, A3 SHEET 1:100</p>		<table border="1"> <tr><td>Issue</td><td>Amendment details</td><td>Drawn</td><td>Check</td><td>Date</td></tr> <tr><td>2</td><td>Proposed signage amended</td><td>J.B.</td><td>K.H.</td><td>02.03.21</td></tr> <tr><td>1</td><td>Construction Issue</td><td>J.B.</td><td>K.H.</td><td>18.06.20</td></tr> </table>		Issue	Amendment details	Drawn	Check	Date	2	Proposed signage amended	J.B.	K.H.	02.03.21	1	Construction Issue	J.B.	K.H.	18.06.20	<p># Use figured dimensions only. Do not scale.</p>				
Issue	Amendment details	Drawn	Check	Date																			
2	Proposed signage amended	J.B.	K.H.	02.03.21																			
1	Construction Issue	J.B.	K.H.	18.06.20																			

ACAD FILE NO: G:\Engineer\CAD\2700-2792758 Bay Lane site plans\Drawings for Top Up\CH Design\DWG\CONSTRUCTION\2758\_BAY\_LANE\_REV 2.dwg



Signage Quantities

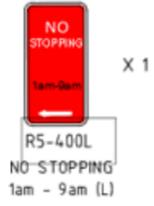


X 1



X 1

Custom 'RIGHT  
TURN ONLY'



X 1



X 1

Custom 'RESIDENT  
AND SERVICE  
VEHICLE EXCEPTED'

Issue	Amendment details	Drawn	Check	Date
2	Signage quantities amended	J.B.	K.H.	02.03.21
1	Construction Issue	J.B.	K.H.	18.06.20

ACAD FILE NO: G:\Engineering\2700-2799\2758 Bay Lane site plans\Drawings for Pop Up\CH Design\WORKBOOK\STRUCTURE\2758\_BAY\_LANE\_REV 2.dwg

**Infrastructure Services**  
Council offices  
70-90 Station Street,  
Mullumbimby NSW 2482.  
Phone 02 66267000  
Fax 02 66843018  
Website www.byron.nsw.gov.au



Approved on behalf of the General Manager

..... Date .....

Designed	J.B.	18.06.20
Drawn	J.B.	18.06.20
Checked	K.H.	18.06.20
Horizontal datum	MGA	
Vertical datum	AHD	

Project:	<b>Bay Lane Activation Evening Closure Byron Bay</b>
Plan title:	<b>Quantities</b>

Project number:	<b>2758</b>
Drawing number	<b>2758-05</b>
Issue	<b>2</b>