

Notice of Meeting

Local Traffic Committee Meeting

A Local Traffic Committee Meeting of Byron Shire Council will be held as follows:

Venue	Conference Room, Station Street, Mullumbimby
Date	Tuesday, 12 March 2024
Time	10.30am

Phil Holloway
Director Infrastructure Services

*I2024/355
Distributed 05/03/24*

BUSINESS OF MEETING

1. APOLOGIES

2. DECLARATIONS OF INTEREST – PECUNIARY AND NON-PECUNIARY

3. ADOPTION OF MINUTES FROM PREVIOUS MEETINGS

3.1 Local Traffic Committee Meeting held on 13 February 2024

4. MATTERS ARISING

5. OUTSTANDING ISSUES/RESOLUTIONS

6. REGULATORY MATTERS

6.1	375 Coorabell Road Intersection Upgrade.....	3
6.2	Bayshore Drive Upgrade	5
6.3	Council Depot Bayshore Drive - Permit Holders Only Parking Signage For Staff Parking	56

7. MATTERS FOR TRAFFIC ENGINEERING ADVICE

7.1	Belongil - Kendall Street to Childe Street Local Traffic Management	60
7.2	Police Station Street Parking- Bangalow	79
7.3	Bangalow Weir Parklands - Illegal Camping.....	81
7.4	Burringbar Street Draft Concept Design	83

8. EVENTS

8.1	Mullum Laneways 2024 - Kaleido Mullumbimby	116
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REGULATORY MATTERS

Report No. 6.1 375 Coorabell Road Intersection Upgrade

File No: I2024/261

5

The purpose of this report is to gain Council support for the proposed line markings and regulatory signs shown in Attachment 1 (E2024/18185).

10 Council has received a Subdivision Works Certificate and Roads Act applications associated with a development approval for a Community title subdivision to create sixteen (16) lots inclusive of the neighbourhood management lot of an approved multiple occupancy comprising fifteen (15) dwelling sites at 375 Coorabell Road, Coorabell

A standard Austroads BAL & BAR intersection is required.

Conditions 7 a) of development consent 10.2021.675.1 state:

Access/Intersection Construction

15 Access/intersection, road shoulder, pavement widening and associated drainage construction, including any necessary relocation of services in accordance with the approved drawings. Refer NDC drawings 17550-01-CC-CI-23 to CI-27 Rev B

Note: Any removal of trees requires separate approval.

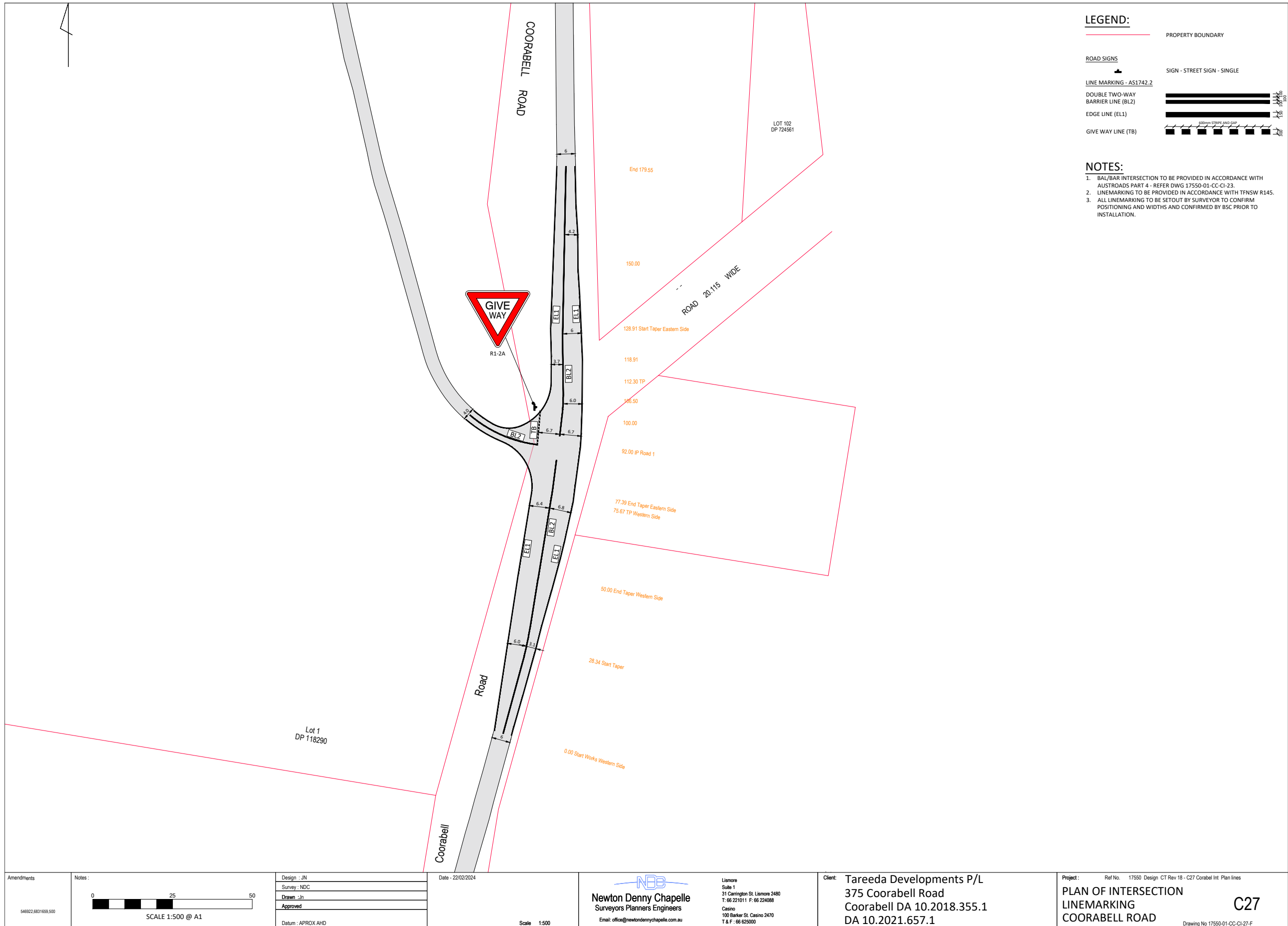
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RECOMMENDATION:

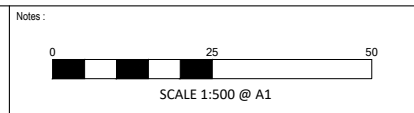
That Council supports the signage and line markings associated with the Coorabell Road intersection works, as shown in Attachment 1 (E2024/18185)

25 **Attachments:**

1 14.2021.657.1 LTC submission, E2024/18185 , page 4  



Amendments



Design : JN
 Survey : NDC
 Drawn : JN
 Approved :
 Datum : APPROX AHD

Date : 22/02/2024

Scale 1:500

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Client: Tareeda Developments P/L
 375 Coorabell Road
 Coorabell DA 10.2018.355.1
 DA 10.2021.657.1

Project: Ref No. 17550 Design CT Rev 18 - C27 Corabell Int Plan lines

**PLAN OF INTERSECTION
 LINEMARKING
 COORABELL ROAD**

C27

Drawing No 17550-01-CC-CI-27-F

Report No. 6.2 Bayshore Drive Upgrade

File No: I2024/262

5 The purpose of this report is to gain Local Traffic Committee and Council support for the proposed changes to Bayshore Drive, Byron Bay.

This work will be from the roundabout at the intersection of Centennial Circuit and Grevillea Street, to just beyond the second intersection of Centennial Circuit near the IGA. This tie into the TAFE roundabout design which was completed by the same consultancy.

10 The proposed works will improve parking, drainage, the bus stop, and the poor condition road surface, creating a safer road environment.

Work will include:

- Renewing the existing road pavement
- Angle and parallel parking
- Stormwater infrastructure
- 15 • Service relocations
- An upgraded bus stop.

The project has undergone consultation with the community at two drop-in sessions in February 2024 and over 2,000 letters and flyers being distributed to the local community. Construction has been advertised on the radio, Council website and Facebook.

20 The project is designed in two stages with the first stage consisting of the road and parking upgrades and the second stage including a cycleway and footpath. Stage 2 also includes crossings and further improvements to the bus stop. Stage 2 is currently shovel ready but unfunded.



Plans for both Stages have been attached to this report (Attachment 1 and Attachment 2).

25 A Safety in Design report and Review of Environmental Factors has been completed for the proposed work.

RECOMMENDATION:

- 5 **That Council supports the signage, line markings, bus stops and traffic control devices associated with the Bayshore Drive works as shown in Attachment 1 (E2023/127977)**

Attachments:

- 10 1 PM20_1201 - Bayshore Drive Upgrade - Stage 1 IFC Drawings, E2023/127977 , page 7 [↓](#) 
- 2 J7553 BAYSHORE STAGE 2 IFC, E2024/18387 , page 33 [↓](#) 

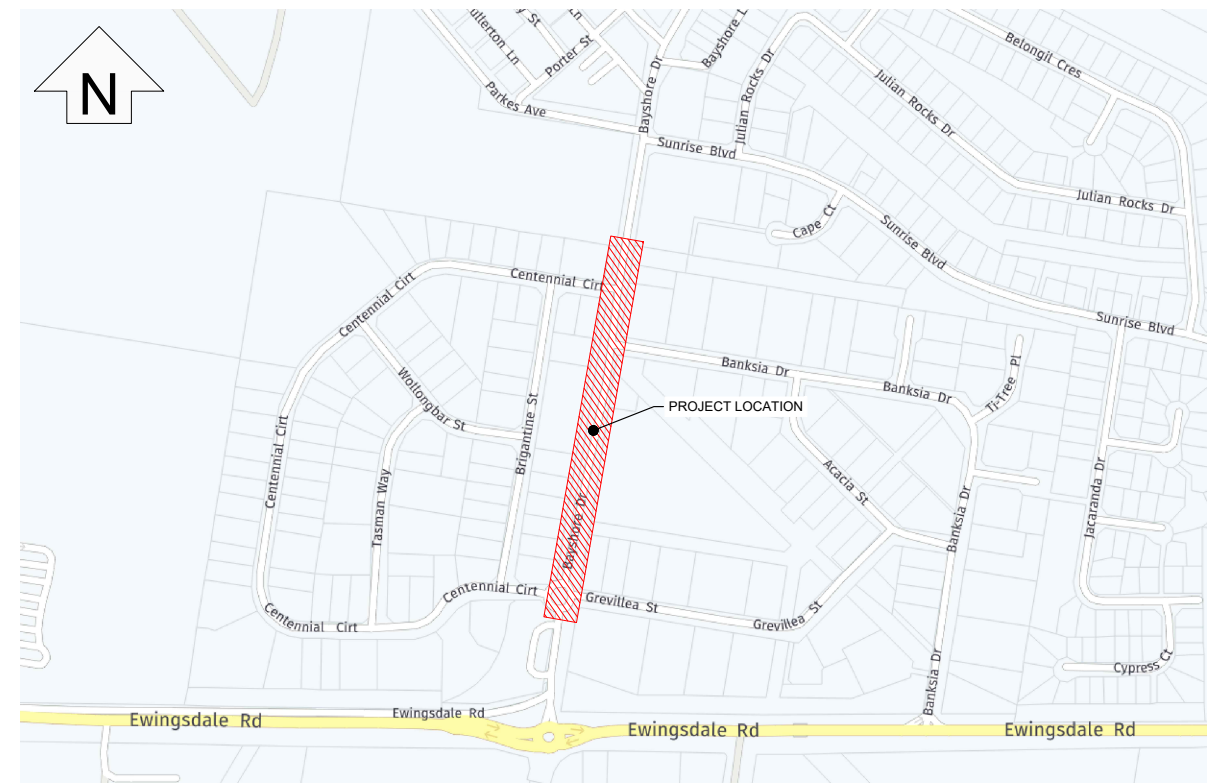
BYRON SHIRE COUNCIL BAYSHORE DRIVE UPGRADE

STAGE 1 - PAVEMENT WORKS DETAILED DESIGN



C O N S U L T I N G

DRAWING REGISTER		
DRAWING NUMBER	TITLE	REVISION
1000	COVER SHEET & DRAWING REGISTER	0
1010	GENERAL NOTES	0
1050	EXISTING CONDITIONS AND DEMOLITION PLAN - SHEET 1 OF 2	0
1051	EXISTING CONDITIONS AND DEMOLITION PLAN - SHEET 2 OF 2	0
1100	EROSION AND SEDIMENT CONTROL PLAN	0
1110	EROSION AND SEDIMENT CONTROL DETAILS	0
1300	OVERALL ROAD WORKS AND KEY PLAN	0
1301	ROAD WORKS PLAN - SHEET 1 OF 5	0
1302	ROAD WORKS PLAN - SHEET 2 OF 5	0
1303	ROAD WORKS PLAN - SHEET 3 OF 5	0
1304	ROAD WORKS PLAN - SHEET 4 OF 5	0
1305	ROAD WORKS PLAN - SHEET 5 OF 5	0
1310	BANKSIA DRIVE INTERSECTION PLAN	0
1320	TYPICAL SECTIONS - SHEET 1 OF 2	0
1321	TYPICAL SECTIONS - SHEET 2 OF 2	0
1330	CONSTRUCTION DETAILS	0
1340	ROAD LONGITUDINAL SECTIONS - SHEET 1 OF 2	0
1341	ROAD LONGITUDINAL SECTIONS - SHEET 2 OF 2	0
1350	CROSS SECTIONS - SHEET 1 OF 4	0
1351	CROSS SECTIONS - SHEET 2 OF 4	0
1352	CROSS SECTIONS - SHEET 3 OF 4	0
1353	CROSS SECTIONS - SHEET 4 OF 4	0
1410	STORMWATER LONGITUDINAL SECTIONS	0
1400	STORMWATER CATCHMENT PLAN	0
1600	WATER MAIN DIVERSION PLAN	A
1620	WATER MAIN DETAILS - SHEET 1 OF 2	A
1621	WATER MAIN DETAILS - SHEET 2 OF 2	A
1700	SIGNAGE AND LINE MARKING PLAN - SHEET 1 OF 2	0
1701	SIGNAGE AND LINE MARKING PLAN - SHEET 2 OF 2	0



LOCALITY PLAN
NOT TO SCALE

IMAGE SOURCE: NEARMAP

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A	80% COMPLETION ISSUE FOR REVIEW	01/08/23	BT	BT	LW	MP	NOT TO SCALE		DATE:		LOCAL GOVERNMENT AUTHORITY:		BYRON SHIRE COUNCIL		ORIGINAL SIZE:
0	CONSTRUCTION ISSUE	17/11/23	BT	BT	MP	MP			THIS DRAWING MUST NOT BE USED FOR CONSTRUCTION UNLESS SIGNED AS APPROVED	administration@planitconsulting.com.au	BYRON SHIRE COUNCIL	A1	J7553	1000	0

GENERAL

- THESE DRAWINGS ARE TO BE READ IN CONJUNCTION WITH THE FOLLOWING DOCUMENTS:
 - OTHER PROVIDED ENGINEERING DRAWINGS;
 - TECHNICAL SPECIFICATIONS;
 - SUPPLEMENTARY SPECIFICATIONS; AND
 - WRITTEN INSTRUCTIONS.
- CURRENT LOCAL AUTHORITY STANDARD DETAILS ARE TO BE ADOPTED UNLESS STATED OTHERWISE.
- ALL CONSTRUCTION MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE RELEVANT SPECIFICATION FOR THE WORKS TOGETHER WITH THE REQUIREMENTS OF ALL THE RELEVANT CODES OF PRACTICE REFERRED TO THEREIN AND THE REQUIREMENTS OF CURRENT LOCAL AUTHORITY STANDARDS AND SPECIFICATIONS.
- THE CONTRACTOR IS RESPONSIBLE FOR THE DESIGN AND PROVISION OF ANY TEMPORARY BRACING, PROPPING ETC. TO DRAINAGE PIPES DURING CONSTRUCTION. STRUCTURES SHALL BE MAINTAINED IN A STABLE POSITION AND NO PART SHALL BE OVERSTRESSED.
- ANY PERMITS AND APPROVALS REQUIRED FOR CONSTRUCTION OF PERMANENT OR TEMPORARY WORKS SHALL BE OBTAINED BY THE CONTRACTOR.
- THE SURVEYS FOR THIS PROJECT THAT THE DESIGN IS FOUNDED ON IS DRAWING NUMBER HM21096-1 DATED 14/10/2022 BY HEATH AND MCPHAIL SURVEYING WITH ADDITIONAL SURVEY BY BYRON BAY SURVEYING DATED 02/05/2023.
- EXISTING SURFACE LEVELS ON THE DRAWINGS ARE INDICATIVE ONLY.
- DO NOT OBTAIN DIMENSIONS FROM SCALING.
- ALL LOCATIONS, ORIENTATION AND LEVELS SHALL BE VERIFIED ON SITE BEFORE COMMENCING ANY WORK. DISCREPANCIES SHALL BE REFERRED TO THE SITE SUPERINTENDENT.

EXISTING SERVICES

- SERVICES SHOWN HAVE BEEN DETERMINED BY ELECTRONIC MEANS AND SOME POT HOLING SAMPLING HAS BEEN UNDERTAKEN. THE LOCATION OF UNDERGROUND SERVICES SHOWN ON THESE DRAWINGS ARE APPROXIMATE ONLY AND THEIR EXACT POSITION SHOULD BE DETERMINED ON SITE BY THE CONTRACTOR. NO GUARANTEE IS GIVEN THAT ALL EXISTING SERVICES ARE SHOWN.
- THE CONTRACTOR SHALL OBTAIN THE LOCATION OF ALL SERVICES AND PROTECT THESE SERVICES PRIOR TO WORKING IN THE VICINITY. ANY DAMAGE WILL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- WORK TO ANY SERVICES SHOULD BE DONE IN CONSULTATION WITH THE APPROPRIATE SERVICE PROVIDER.

SETTING OUT

- DIGITAL DATA WILL BE PROVIDED FOR THE SETTING OUT OF THIS PROJECT. DATA INCLUDES 3D STRINGS AND A TRIANGULATION OF THE DESIGN SURFACE.
- NOT ALL KERB RAMPS AND VEHICLE CROSSOVERS ARE MODELLED IN THE DIGITAL DATA. WHERE THEY DO NOT FORM PART OF THE MODEL THEY ARE TO BE CONSTRUCTED TO THE STANDARD AS SPECIFIED ON THE DRAWINGS.

EROSION AND SEDIMENT CONTROL

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE IMPLEMENTATION OF THE EROSION AND SEDIMENT CONTROL PLAN.
- ALL CONTROL MEASURES SHALL BE IN ACCORDANCE WITH "NORTHERN RIVERS LOCAL GOVERNMENT DEVELOPMENT DESIGN SPECIFICATION D7 EROSION CONTROL AND STORMWATER MANAGEMENT".
- SEDIMENT AND EROSION CONTROL MEASURES ARE INDICATIVE ONLY AND SHALL BE CONFIRMED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL INFORM ALL STAFF AND SUB-CONTRACTORS OF THEIR OBLIGATIONS UNDER THE EROSION AND SEDIMENT CONTROL PLAN.
- CONTROL MEASURES SHALL BE IN PLACE PRIOR TO EACH SITE DISTURBANCE.
- ALL WORKS ARE TO BE INSPECTED, AND MAINTAINED WHERE NECESSARY, ON A WEEKLY BASIS AND AFTER EACH RAIN EVENT.
- ADEQUATE MEASURES SHALL BE TAKEN TO PREVENT DUST FROM AFFECTING THE AMENITY OF THE NEIGHBOURHOOD DURING CONSTRUCTION. WATER SITE AS REQUIRED TO PREVENT DUST GENERATION. USE TEMPORARY SPRINKLERS AS REQUIRED.
- ALL WORKS ON SITE SHALL STOP WHEN WIND SPEEDS REACHES 35km/h.
- CLEANING OF FOOTPATHS AND ROADWAYS SHALL BE CARRIED OUT REGULARLY

- TURF AND VERGE AREAS TO BE REINSTATED WHEN DISTURBED DURING CONSTRUCTION.
- STOCKPILES TO BE AWAY FROM OVERLAND FLOW PATHS AND TO BE IN ACCORDANCE WITH JXXXX-0110.
- ALL DISTURBED AREAS ARE TO BE LEFT IN A STABLE CONDITION. SLOPES SHOULD BE STABILISED USING APPROPRIATE EROSION CONTROL MEASURES.
- ALL TURF/PLANTINGS WILL NEED TO BE MAINTAINED THROUGHOUT THE ESTABLISHMENT PHASE.

EARTHWORKS

- THE GEOTECHNICAL REPORT FOR THE PROJECT REFERRED TO IN THE NOTES IS 23042-00-REP-001 REV 0 DATED 19/04/2023 BY CIVIL CONSULT.
- ALL EARTHWORKS SHALL BE CARRIED OUT IN ACCORDANCE WITH AS3798 AND SUPERVISION TO LEVEL 1 SHALL BE SUPPLIED BY THE CONTRACTOR.
- TOPSOIL AND OTHER ORGANIC MATTER IS TO BE STRIPPED FROM GROUNDWORK AREAS PRIOR TO COMMENCING GROUNDWORKS AND SHALL BE STOCKPILED ON SITE. EARTH STOCKPILES SHALL BE SUITABLY PROTECTED FROM EROSION AND WEED INFESTATION BY COVERING WITH WEED MAT OR OTHER MEANS. RESPREAD TOPSOIL TO FINISHED SURFACE LEVELS AND VEGETATE TO SPECIFICATION PRIOR COMPLETION. EXCESS TOPSOIL TO BE REMOVED FROM SITE.
- NON SUITABLE SITE WON MATERIAL (E.G. SILTY SANDS, REACTIVE, DISPERSIBLE MATERIAL) ARE NOT TO BE USED WITHIN ENGINEERING FILL UNLESS ASSESSED AND CERTIFIED AS SUITABLE FOR USE BY THE LEVEL 1 GEOTECHNICAL ENGINEER.
- ALL IMPORTED (IF ANY) FILL IS TO BE 'CLEAN' AND FREE OF ANY CONTAMINATION.
- UNLESS DIRECTED OTHERWISE IN THE GEOTECHNICAL REPORT FOR THE PROJECT, BY THE GEOTECHNICAL ENGINEER ON SITE OR BY THE RELEVANT LOCAL AUTHORITY SPECIFICATION (FOR WORKS SUBJECT TO APPROVAL/ADOPTION BY THE LOCAL AUTHORITY) FILLING SHALL BE COMPACTED IN ACCORDANCE WITH TABLE 5.1 OF AS3798.
- UNLESS DIRECTED OTHERWISE IN THE GEOTECHNICAL REPORT FOR THE PROJECT, OR BY THE GEOTECHNICAL ENGINEER ON SITE, FILLING AND SUBGRADE AREAS SHALL BE COMPACTED IN MAXIMUM LIFTS OF 300mm LOOSE THICKNESS
- SUITABILITY AND TREATMENT OF ALL BATTERS STEEPER THAN 1 IN 4 TO BE CONFIRMED BY A SUITABLY QUALIFIED GEOTECHNICAL ENGINEER PRIOR TO CONSTRUCTION. THE GEOTECHNICAL ENGINEER SHALL PROVIDE THE APPROPRIATE LEVEL OF SUPERVISION DURING CONSTRUCTION IN ACCORDANCE WITH AS3798 AND CERTIFY THE COMPLETED WORKS PRIOR TO THE REMOVAL OF ANY TEMPORARY STABILISATION MEASURES.

ROADWORKS

- NOTWITHSTANDING THE DETAILS SHOWN ON THE DRAWINGS ALL WORKS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CURRENT LOCAL AUTHORITY STANDARD SPECIFICATIONS AND DRAWINGS.
- SIDE DRAINS SHALL BE CONSTRUCTED UNDER ALL NEW KERBS AS SPECIFIED WITHIN THESE DRAWINGS AND AS DIRECTED BY THE SUPERINTENDENT. REFER CURRENT LOCAL AUTHORITY STANDARD DRAWING.
- FLUSHING POINTS SHALL BE PROVIDED FOR SIDE DRAINS AT THE REQUIRED SPACING IN ACCORDANCE WITH CURRENT LOCAL AUTHORITY STANDARD DRAWINGS AND SPECIFICATIONS.
- EXISTING DRIVEWAYS WITHIN THE LIMITS OF THE CONSTRUCTION WORKS SHALL BE PROTECTED FROM DAMAGE. ANY DAMAGE TO DRIVEWAYS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- ENTRY INTO EXISTING PROPERTIES SHALL BE MAINTAINED AT ALL TIMES.
- TEMPORARY WARNING SIGNS TO BE ERECTED AS PER TRAFFIC CONTROL PLAN, CURRENT EDITION.
- SEAL TO BE A.C. SURFACING - AS SPECIFIED.
- KERB ADAPTERS ARE TO BE INSTALLED FOR ALL LOTS THAT FALL TO THE ROAD.

PAVEMENTS

- GEOTECHNICAL TEST RESULTS ARE TO BE FORWARDED TO THE SUPERINTENDENT PRIOR TO FINAL BOXING. TESTS SHALL INCLUDE SOAKED CBR AND/OR OTHER TESTS AS REQUESTED BY THE SUPERINTENDENT. THESE TESTS SHALL BE USED TO CONFIRM THE PAVEMENT DESIGN SHOW ON THESE DRAWINGS.
- THE PAVEMENT DESIGN ON THE DRAWINGS IS NOT FOR CONSTRUCTION UNTIL FINAL CBR TESTS ARE REVIEWED AND APPROVED BY THE SITE SUPERINTENDENT. CONSTRUCTION OF THE PAVEMENT TO THE DESIGN SHOWN ON THE DRAWINGS PRIOR TO

- RECEIPT OF THE FINAL CBR TEST SHALL BE UNDERTAKEN AT THE CONTRACTOR'S OWN RISK.
- GRANULAR PAVEMENT MATERIAL TO BE IN ACCORDANCE WITH CURRENT LOCAL AUTHORITY CONSTRUCTION SPECIFICATIONS AND ALL DOCUMENTS REFERENCED WITHIN THESE SPECIFICATIONS.
- THE PAVEMENT SEAL IS TO BE IN ACCORDANCE WITH CURRENT LOCAL AUTHORITY CONSTRUCTION SPECIFICATIONS AND ALL DOCUMENTS REFERENCED WITHIN THESE SPECIFICATIONS.
- GEOTECHNICAL TESTING IS TO BE UNDERTAKEN AT 100m INTERVALS AT MINIMUM AT COMPLETION OF THE BULK EARTHWORKS.
- SAMPLING SHALL BE CARRIED OUT IN ACCORDANCE WITH CURRENT LOCAL AUTHORITY GUIDELINES.
- TRAFFIC NUMBERS ARE BASED ON CURRENT LOCAL AUTHORITY GUIDELINES.
- FINAL PAVEMENT DESIGN IS SUBJECT TO RECEIPT OF THE GEOTECHNICAL TEST RESULTS AND MAY RESULT IN AN AMENDMENT TO THE PAVEMENT DESIGN SHOWN ON THIS DRAWING.
- PAVEMENT DESIGN HAS BEEN PROVIDED BY CIVIL CONSULT. REFER 23042-00-REP-001 REV 0 DATED 19/04/2023.

SIGNAGE & LINE MARKING

- ALL SIGNAGE AND LINE MARKING SHALL BE IN ACCORDANCE WITH AS 1742.2 'MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES PART 2'
- FINAL SIGN LOCATIONS TO BE DETERMINED ON SITE BY THE SUPERINTENDENT.
- ALL TRAFFIC SIGNS TO BE SIZE 'A' U.N.O.
- FOR GUIDE POST INSTALLATION AND DETAILS, REFER CURRENT LOCAL AUTHORITY.
- FOR TRAFFIC SIGN SUPPORT DETAILS, REFER CURRENT LOCAL AUTHORITY.
- ALL SIGN MATERIAL TO BE CLASS 1.
- ALL SIGNAGE, LINE MARKING & RRPMS ARE TO BE PREPARED IN ACCORDANCE WITH CURRENT LOCAL AUTHORITY REQUIREMENTS.

STORMWATER

- ALL DRAINAGE STRUCTURES ARE TO BE INSTALLED IN ACCORDANCE WITH THE FOLLOWING DOCUMENTS:
 - CURRENT LOCAL AUTHORITY DRAWINGS AND SPECIFICATIONS;
 - ANY MANUFACTURER'S STANDARD DRAWINGS AND SPECIFICATIONS.
- ALL CONSTRUCTION MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE RELEVANT SPECIFICATION FOR THE WORKS TOGETHER WITH THE REQUIREMENTS OF ALL THE RELEVANT CODES OF PRACTICE REFERRED TO THEREIN AND THE REQUIREMENTS OF THE STATUTORY AUTHORITIES WHERE APPLICABLE.
- STRUCTURES HAVE BEEN DESIGNED FOR OPERATIONAL LOADS ONLY. THE CONTRACTOR IS RESPONSIBLE FOR THE ASSESSMENT OF CONSTRUCTION LOADS AND PROVISIONS OF ANY TEMPORARY BRACING, PROPPING ETC. REQUIRED DURING CONSTRUCTION. STRUCTURES SHALL BE MAINTAINED IN A STABLE CONDITION AND NO PART SHALL BE OVERSTRESSED.
- PRECAST REINFORCEMENT CONCRETE PIPES ARE TO BE MANUFACTURED IN ACCORDANCE WITH AS 4058 AND AS 1992.
- ALL STORMWATER PIPES SHALL BE CLASS '4' RCP. U.N.O.
- ALL PIPES UP TO AND INCLUDING Ø600 ARE TO BE RUBBER RING JOINTED. ALL PIPES ABOVE Ø600 ARE TO BE FLUSH JOINTED U.N.O.
- HEADWALL END STRUCTURES TO BE TYPE A, CAST IN SITU CONCRETE WITH CONCRETE APRONS (INCLUDING CUT OFF WALLS) U.N.O. REFER TO CURRENT LOCAL AUTHORITY STANDARD DRAWINGS FOR DETAILS.
- EXISTING STORMWATER DRAINAGE PIPES AND MANHOLES WITHIN THE LIMIT OF WORK SHALL BE PROTECTED, REMOVED OR MODIFIED AS SPECIFIED.
- WHERE A CONNECTION IS MADE TO AN EXISTING DRAINAGE PIPE OR PIT, THE LEVEL OF THAT ELEMENT MUST BE SURVEYED PRIOR TO CONSTRUCTION. THE SURVEYED LEVELS SHALL BE PROVIDED TO THE SITE SUPERINTENDENT TO CONFIRM THE CONNECTION AND LEVELS PRIOR TO CONSTRUCTION.
- BACKFILL AND BEDDING TO PIPE TO BE IN ACCORDANCE WITH CURRENT LOCAL AUTHORITY STANDARD DRAWINGS AND SPECIFICATIONS.
- UNSUITABLE FOUNDING MATERIAL FOR PIPES AND STRUCTURES SHALL BE REMOVED OR IMPROVED IN ACCORDANCE WITH CURRENT LOCAL AUTHORITY SPECIFICATIONS.
- ALL TRENCH BACK FILL MATERIAL UNDER THE PAVEMENT SHALL BE CBR 15 OR APPROVED EQUIVALENT.
- STEEL GRATES AND FRAMES ARE TO BE FABRICATED FROM MILD STEEL AND HOT DIP GALVANISED. ALL GRATES ARE TO BE CLASS D

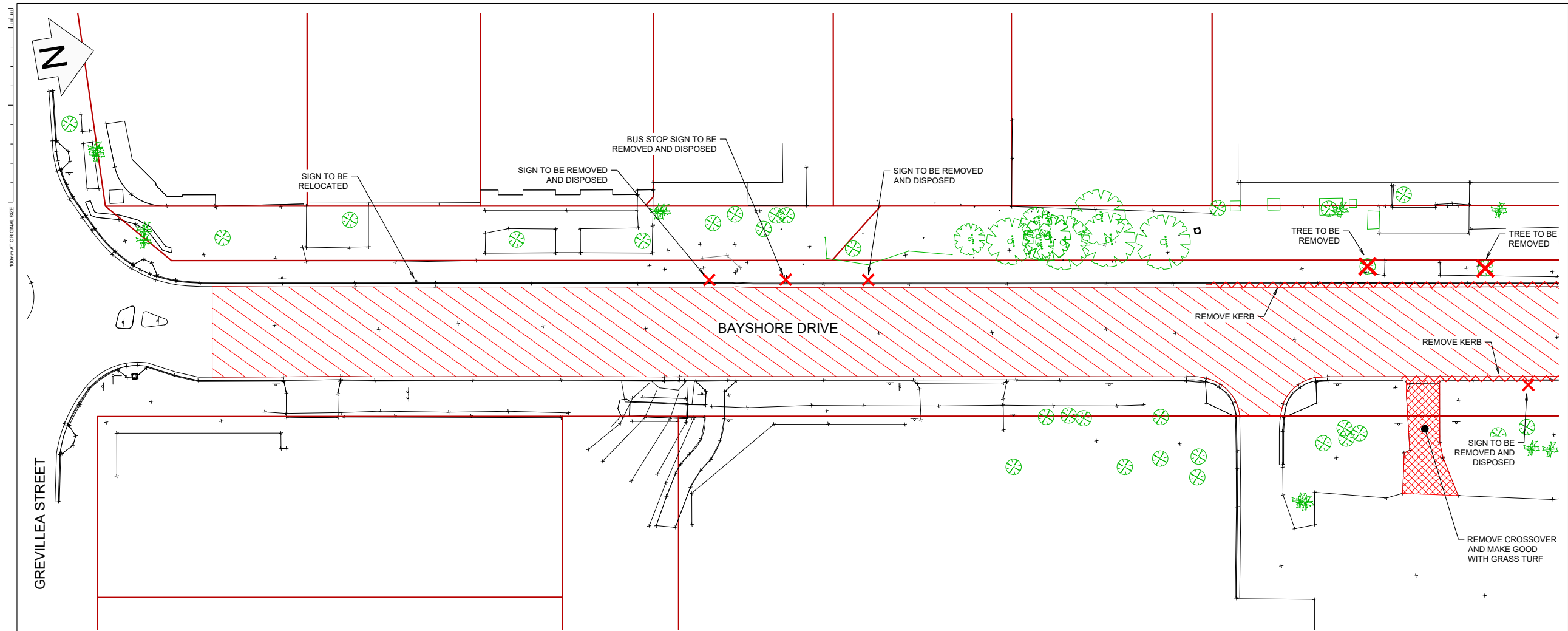
- U.N.O. AND BICYCLE SAFE IN ACCORDANCE WITH AS 3996 U.N.O.
- GRATE SUPPORT TO BE CONSTRUCTED LEVEL TO ENSURE THAT THE GRATE DOES NOT ROCK AFTER INSTALLATION.
- ALL LEVELS ARE APPROXIMATE ONLY AND ARE SUBJECT TO FULL DETAIL SURVEY OF THE EXISTING STRUCTURE.

CONCRETE

- COMPLY WITH AS 3600.
- FORM CONSTRUCTION JOINTS ONLY WHERE APPROVED BY THE ENGINEER.
- SUPPORT REINFORCEMENT IN ITS CORRECT POSITION DURING CONCRETING BY APPROVED BAR CHAIRS, SPACERS OR SUPPORT BARS SUITABLE FOR THE EXPOSURE CONDITIONS.
- LAP MESH REINFORCEMENT BY ONE COMPLETE MESH.
- DO NOT WELD OR SITE BEND REINFORCEMENT UNLESS SHOWN IN THE DRAWINGS OR OTHERWISE SPECIFIED BY THE ENGINEER.
- REINFORCEMENT IS REPRESENTED DIAGRAMMATICALLY AND IS NOT NECESSARILY SHOWN IN TRUE PROJECTION.
- SAMPLE TEST AND ASSESS CONCRETE COMPLIANCE IN ACCORDANCE WITH PROJECT ASSESSMENT OF STRENGTH GRADE TO SECTION 20 OF AS 3600.
- THE CONCRETE SHALL BE COMPACTED USING HIGH-FREQUENCY VIBRATORS.
- ADMIXTURES SHALL NOT BE USED WITHOUT THE WRITTEN APPROVAL OF THE ENGINEER.

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A	80% COMPLETION ISSUE FOR REVIEW	01/08/23	BT	BT	LW	MP					BYRON SHIRE COUNCIL		A1	J7553	1010	0
0	CONSTRUCTION ISSUE	17/11/23	BT	BT	MP	MP					BYRON SHIRE COUNCIL					

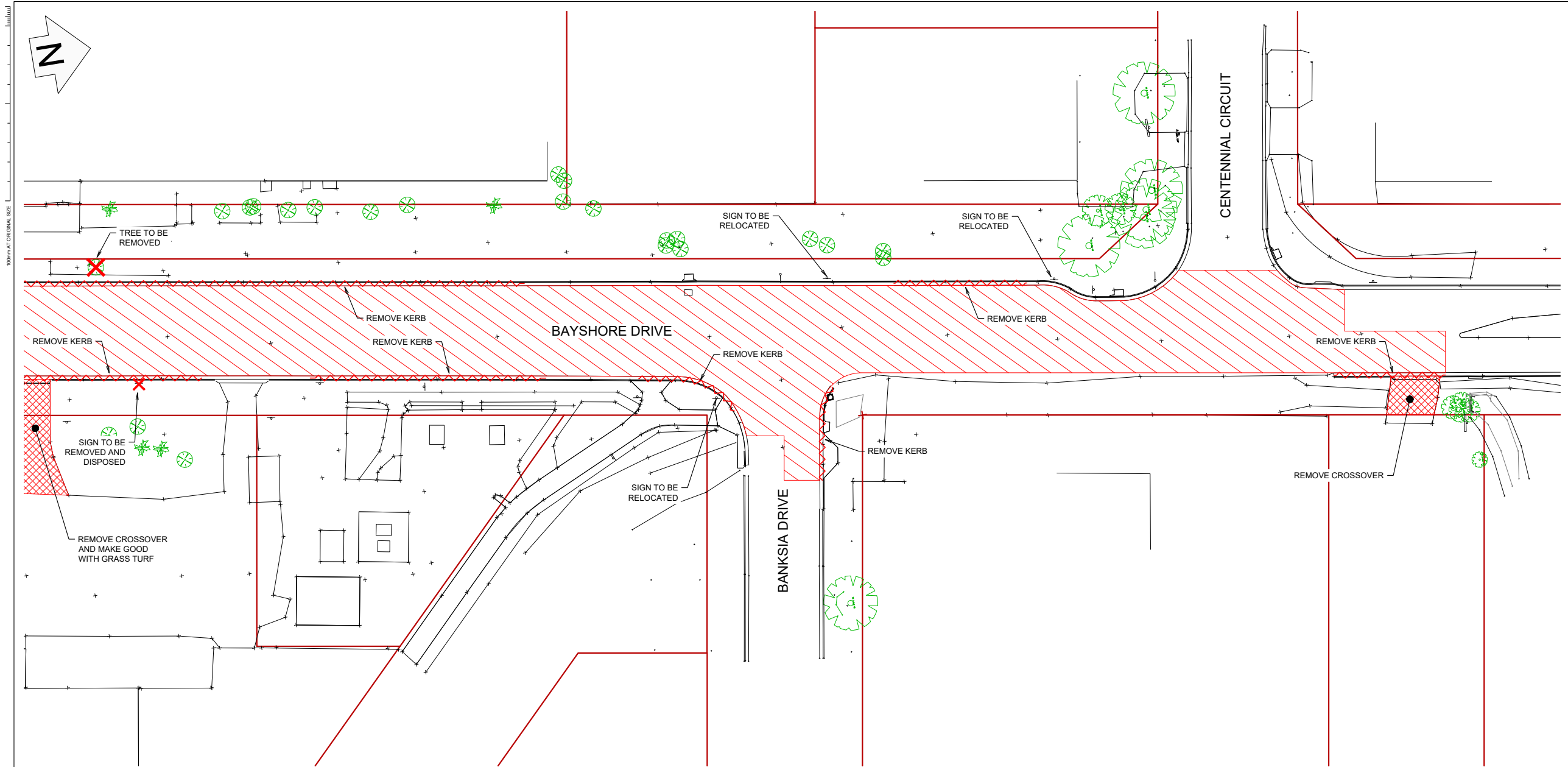


LEGEND

- PROPERTY BOUNDARY
- EXISTING VEGETATION
- ROAD PAVEMENT EXCAVATION AND REMOVAL
- KERB TO BE REMOVED
- X FEATURE TO BE REMOVED

FOR CONSTRUCTION

REV	DESCRIPTION	DATE	DRAWN	DESIGN	CHECK	APPROVED	SCALES:	APPROVED BY:	PLANIT CONSULTING	CLIENT:	PROJECT:		
A	80% COMPLETION ISSUE FOR REVIEW	01/08/23	BT	BT	LW	MP	0 2.5 5 10 15 Full Size 1:250 ; Half Size 1:500 Scale (m)	Copyright in the drawings, information and data recorded in this document ("the information") is the property of Planit Consulting. This document and the information are solely for the use of the authorised recipient and this document may not be used, Copied or reproduced in whole or part for any purpose other than that for which it was supplied by Planit Consulting. Planit Consulting makes no representation, undertakes no duty and accepts no responsibility to any third party who may use or rely upon this document or the information.	SUITE 9A, 80-84 BALLINA STREET PO BOX 161 LENNOX HEAD NSW 2478 PH: 02 6687 4668 ABN: 20 099 261 711	BYRON SHIRE COUNCIL	BAYSHORE DRIVE UPGRADE		
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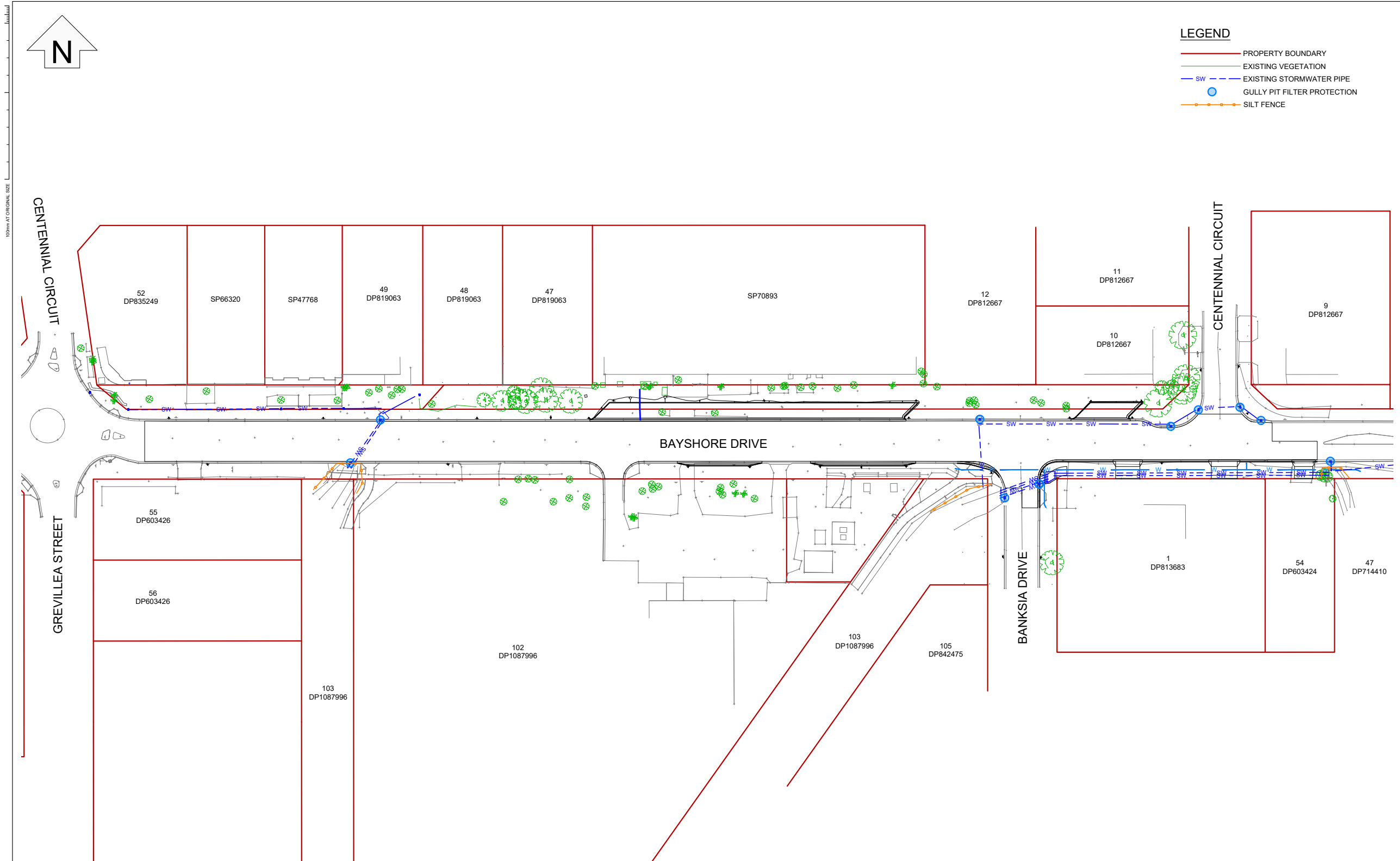


LEGEND

- PROPERTY BOUNDARY
- EXISTING VEGETATION
- ▨ ROAD PAVEMENT EXCAVATION /
- KERB TO BE REMOVED
- ✗ FEATURE TO BE REMOVED

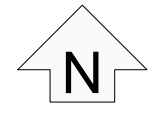
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0	CONSTRUCTION ISSUE	17/11/23	BT	BT	MP	MP						ORIGINAL SIZE: A1 PLANIT JOB No.: J7553 DRAWING No.: 1051 REV: 0



LEGEND

- PROPERTY BOUNDARY
- EXISTING VEGETATION
- - - EXISTING STORMWATER PIPE
- GULLY PIT FILTER PROTECTION
- SILT FENCE



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A	80% COMPLETION ISSUE FOR REVIEW	01/08/23	BT	BT	LW	MP	0 5 10 20 30 Full Size 1:500 ; Half Size 1:1000 Scale (m) DO NOT SCALE FROM DRAWING	THIS DRAWING MUST NOT BE USED FOR CONSTRUCTION UNLESS SIGNED AS APPROVED	DATE: administration@planitconsulting.com.au	CLIENT: BYRON SHIRE COUNCIL	PROJECT: BAYSHORE DRIVE UPGRADE DRAWING TITLE: STAGE 1 EROSION AND SEDIMENT CONTROL PLAN	ORIGINAL SIZE: A1 PLANIT JOB No.: J7553 DRAWING No.: 1100 REV: 0
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Report No. 6.3 Council Depot Bayshore Drive - Permit Holders Only Parking Signage For Staff Parking

File No: I2024/351

5

The purpose for this report is to gain support for an internal exception inside Council's Depot Parking Area requesting the implementation of "Permit Holders Only" beyond this point. This point is to ensure appropriate staff parking is provided to Byron Shire Depot staff and visitors.

- 10 A review of the Council's Bayshore Drive Depot has highlighted some concerns as the number of vehicles and staff growth within the premises. It has been observed several recurring issues regarding parking, including but not limited to:
- Limited parking space leading to congestion and inconvenience.
 - Unauthorized vehicles occupying designated spots, causing frustration among staff.
- 15
- Vehicles parked in restricted areas, hindering the movement of essential equipment.

Please refer to signage plan in Attachment 1 (E2024/22953) for the proposed signs and locations, as per Figure 1 below. Signage dimensions to be in accordance with R5 82A and R5-83A at the exits.



Figure 1: Proposed Parking signage

Current signage in place is as per Figure 2, this is to ensure appropriate enforcement can be undertaken beyond this point.




Figure 2: Current Parking signages

5 **RECOMMENDATION:**

That the Local Traffic Committee endorse the permit holder only exception for Depot Staff and Visitors at Council Depot on Bayshore Drive.

Attachments:

10

1 3037_DEPOT PARKING_SP01 (Rev1), E2024/22953 , page 59 

BYRON SHIRE COUNCIL

LOCAL TRAFFIC COMMITTEE MEETING

7.1

Figure 1: Safety issue Kendall St and Childe St traffic management device

Proposed Solution

5 Following the removal of the chicanes staff are proposing a less invasive speed management treatment for the area. This proposal includes line-marking the travel lanes to provide a narrowing effect which can reduce the likelihood of speeding. Currently the road is un-marked, and due to the width (11m) people are willing to drive further to the right of the road due to the lack of channelisation. Line-marking the area should channelise most drivers further to the left while providing a narrower travel path while potentially slowing vehicle speeds (refer to attachment 3).




10

RECOMMENDATION:

That the Local Traffic Committee endorse the line-marking traffic management treatments proposed in Attachment 3 (E2024/20677) of this report for installation.

15

Attachments:

- 1 Minutes 30/11/2022 Local Traffic Committee Extraordinary, I2022/1779 , page 62 [↓](#) 
- 2 Belongil Parking Changes, E2022/117545 , page 69 [↓](#) 
- 20 3 3068 Belongil Line Marking, E2024/20677 , page 71 [↓](#) 

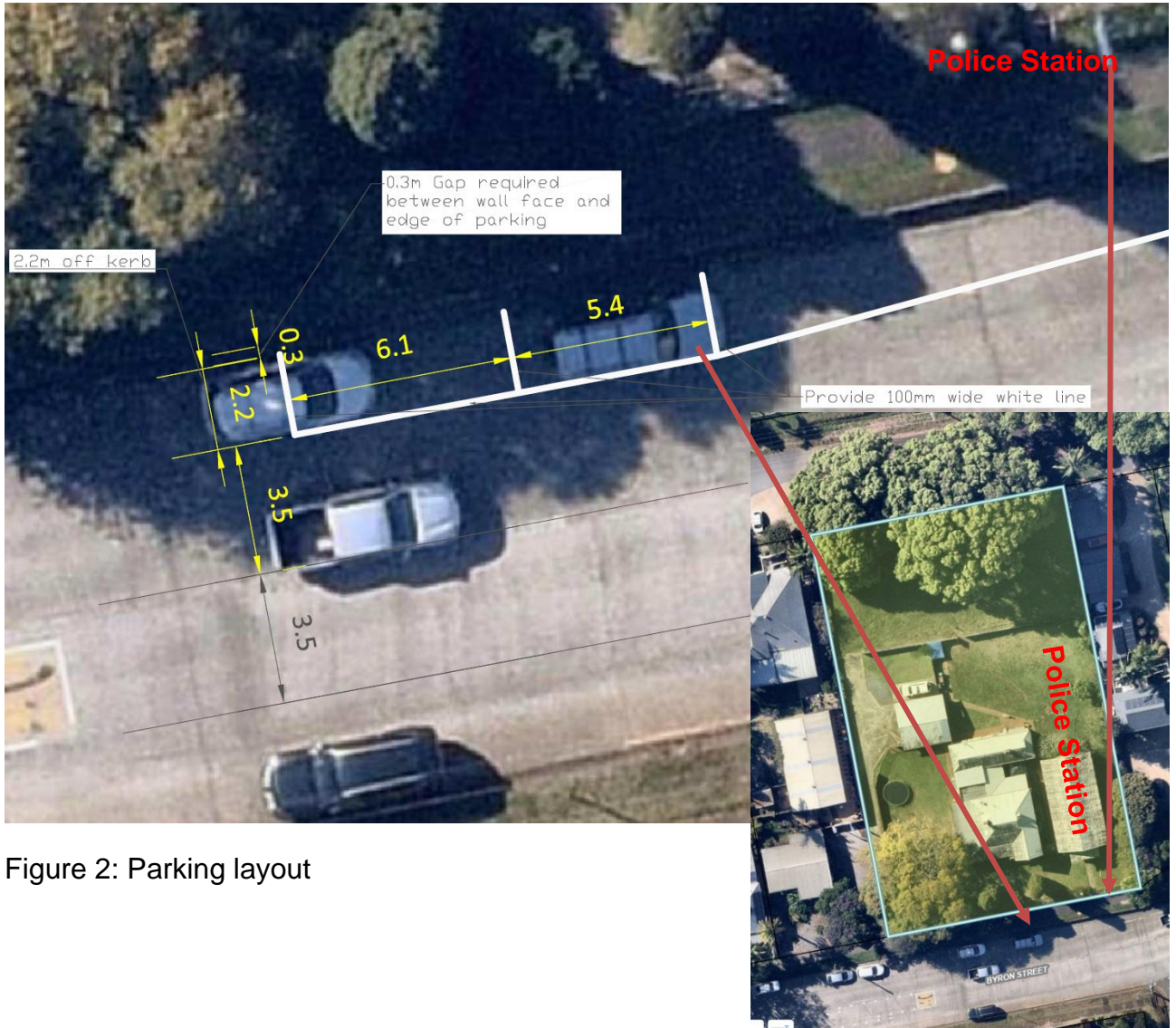


Figure 2: Parking layout

5

RECOMMENDATION:

That Council support the installation of signage and line marking required to provide two on street car parking spaces allowing police to have parking available on Byron Street.

10

Report No. 7.3 Bangalow Weir Parklands - Illegal Camping

File No: I2024/279

5 Police in Bangalow have become aware that illegal camping is on the rise in the Bangalow Wier Parklands and hence have requested that no parking signs (between 1am-5am) be placed at the Bangalow Weir carpark.

10 Therefore, it's proposed that no camping signs be placed at the entrance of the Bangalow Weir Parklands Carpark as well as a secondary no camping sign with in the parking lot supported with no parking signs (between 1am -5am) placed at approximately 25m centre to centre within the parking lot, see Figure 2 below.

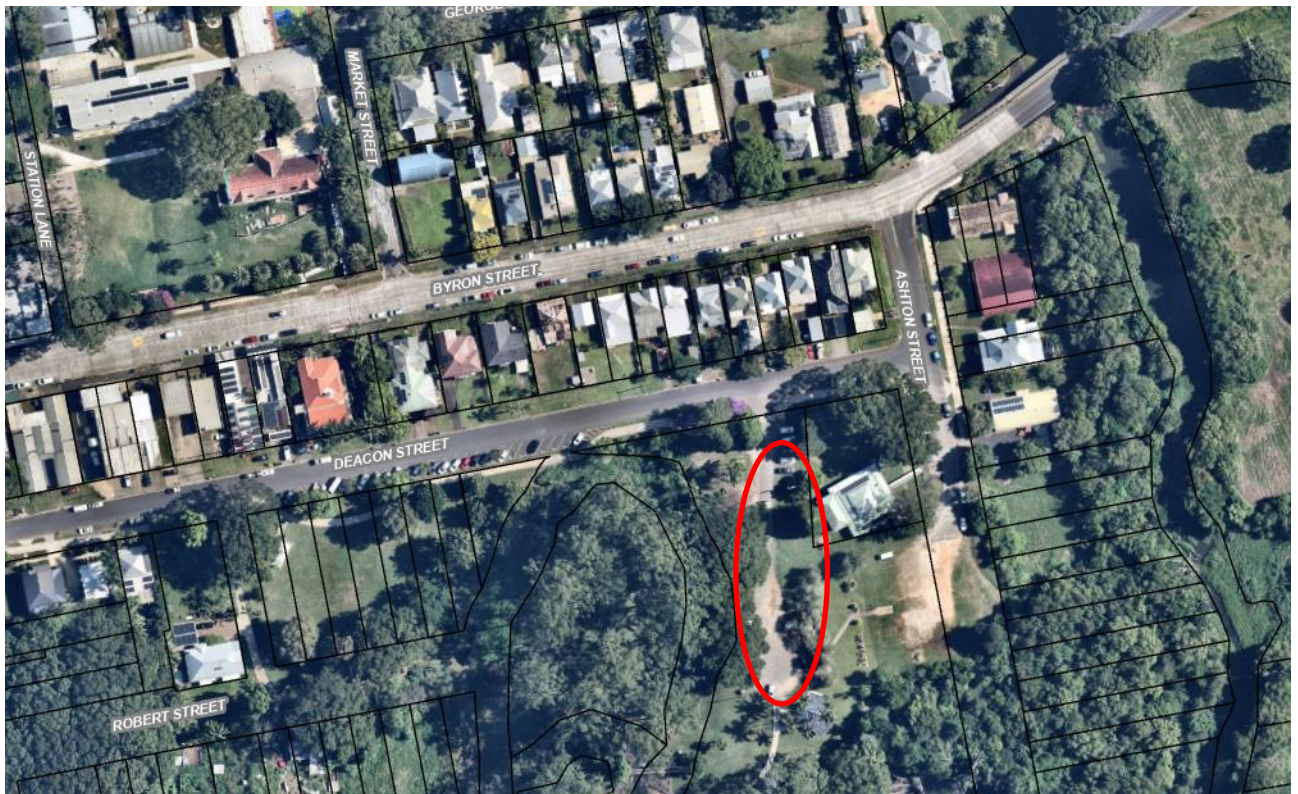


Figure 1- Bangalow Weir Parklands Location (Lat:28.6875° S Lon:153.5267° E)



Figure 2 - Bangalow Weir Parklands Carpark-Proposed Signage

5

RECOMMENDATION:

That Council supports the installation of no camping and no parking signs (between 1am-5am) within the Bangalow Weir Parklands Carpark to allow enforcement officers to fine/move on illegal camping.

10

Report No. 7.4 Burringbar Street Draft Concept Design

File No: I2024/323

5 Following site analysis, consultation and workshops held in 2023, Council have developed a Draft Concept Design for Burringbar Street, Mullumbimby.

The Concept Design looks at ways to make Burringbar Street more:

- Accessible
- Safe for pedestrians
- Vibrant
- 10 • Enjoyable.

The Draft Concept Design is shown on the following page and attached at Attachment 1 (E2024/16870).

Key changes to Burringbar Street proposed in this design include:

- 15 • **Widen existing footpaths** from 3.5m to 5.1m to improve access and allow for afresco dining and public seating.
- **Raised 10km/hr shared zone at Burringbar and Stuart Street intersection** where people have priority to cross in any direction.
- **Additional pedestrian crossings** at Dalley Street and adjacent to laneways.
- **Station Street Intersection priority change.**
- 20 • **Informal pedestrian crossings** across Burringbar Street near Studal Lane and McGoughans Lane.
- **Remove dedicated cycle lanes** on Burringbar Street and integrate cyclists with vehicle lane.
- **Net loss of car parks** in project area is kept to a minimum (10)
- 25 • **Retain existing 60-degree angled carparking** to both sides of the street and include new loading, kiss and ride, taxi and bus stop bays.
- **Relocate taxi zone on Stuart Street** to Burringbar Street and replace with angled parking.
- 30 • **Relocate bus stop zone** pending discussions with Transport NSW and bus companies.

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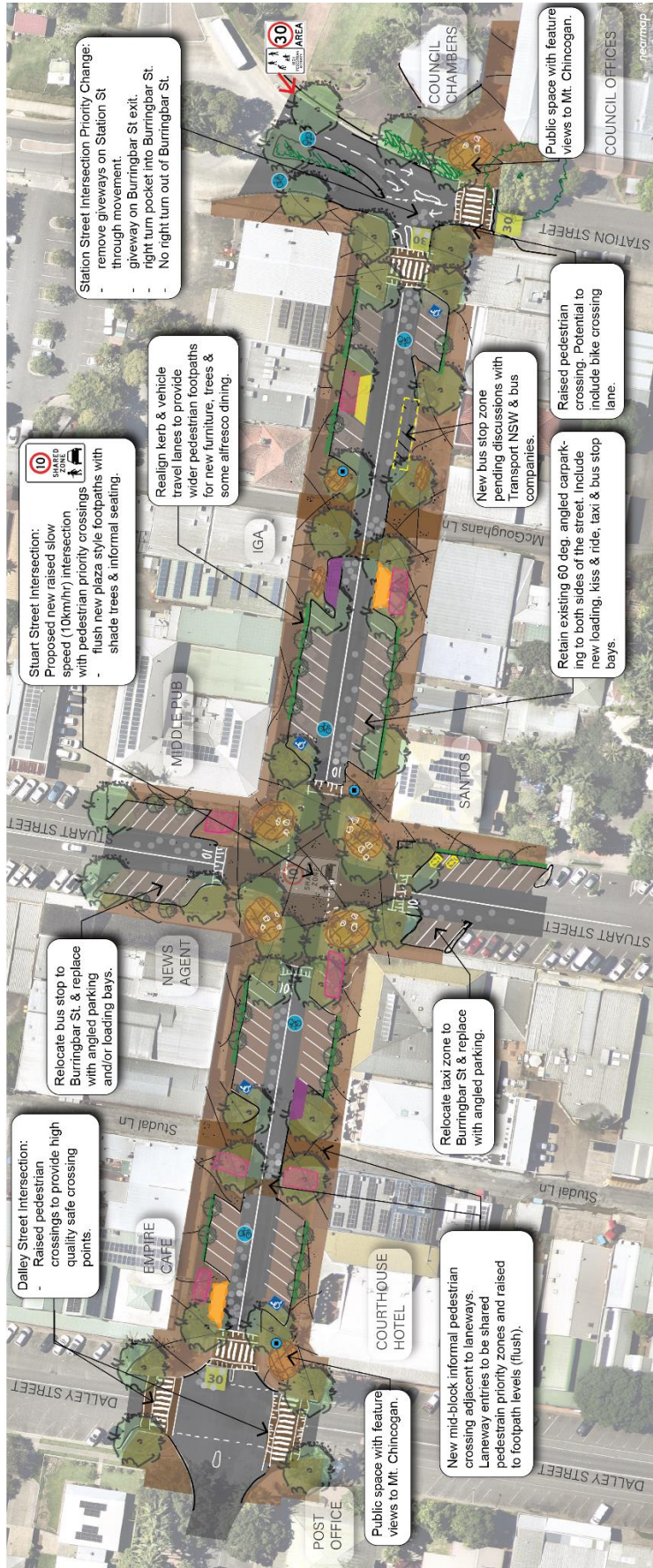
LOCAL TRAFFIC COMMITTEE MEETING

7.4

- **Additional mobility parking spaces** increased from 3 to 4.
- **Loading bays added.**

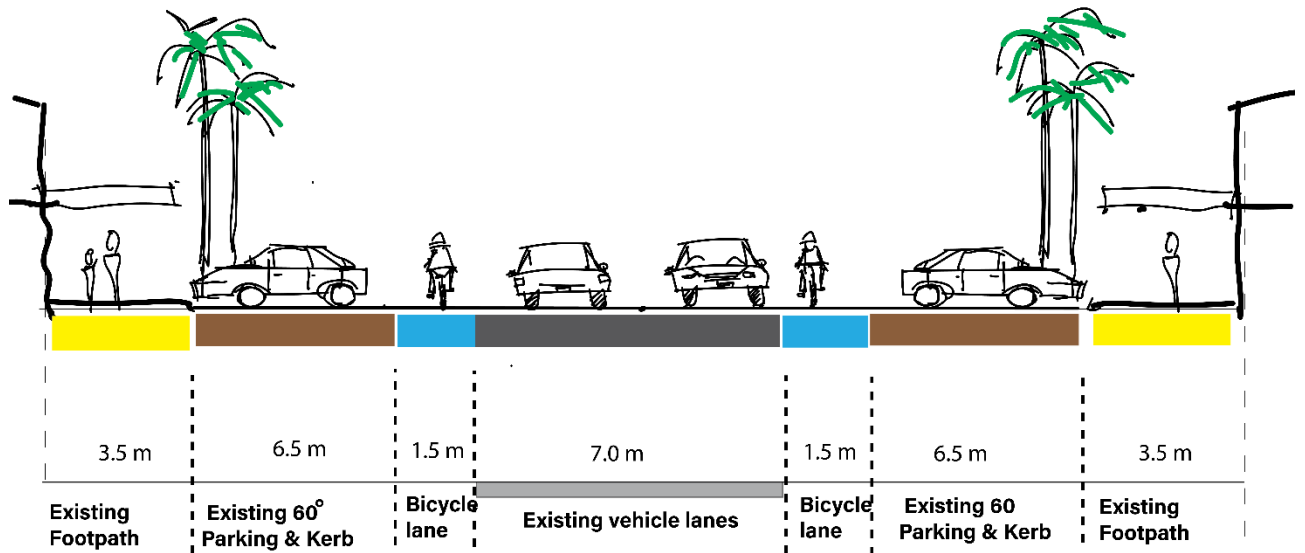
An Exhibition report providing detailed site analysis, background information and further details of the design features is provided at Attachment 2 (E2024/3881).

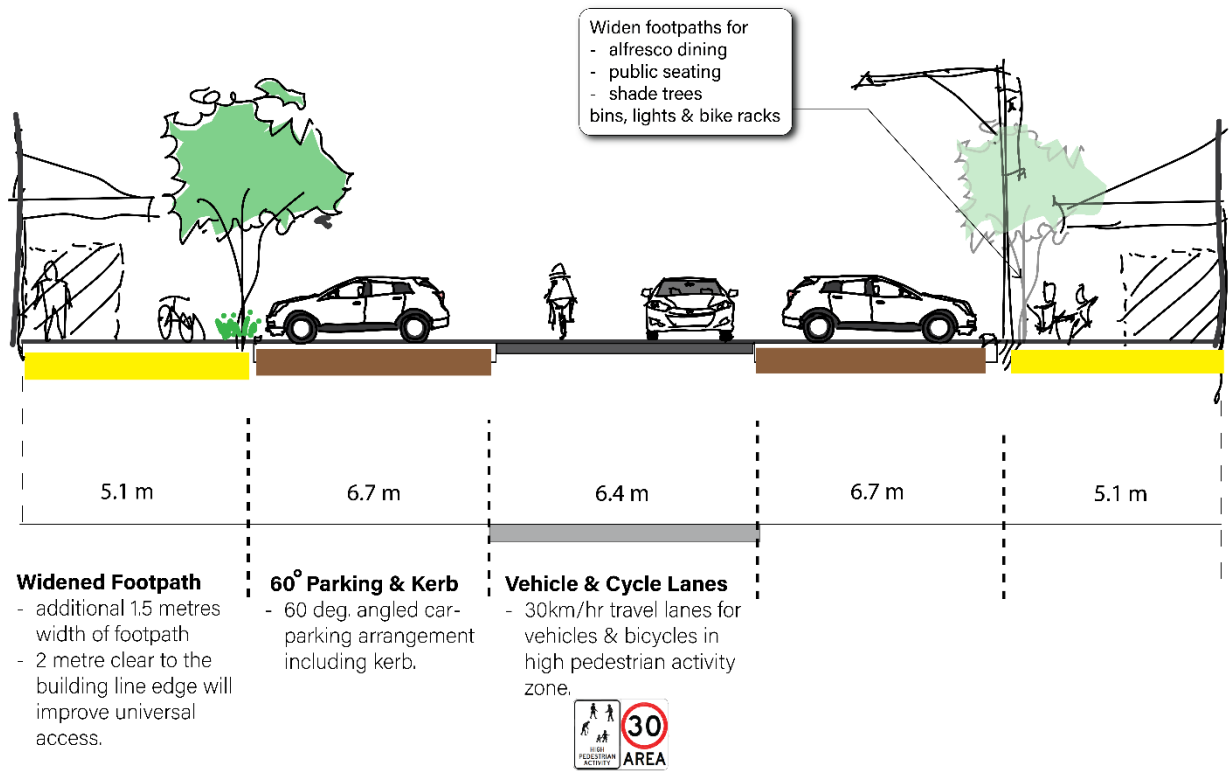
- 5 Council is seeking engineering feedback and advice on this Draft Concept Design to support a final Concept.



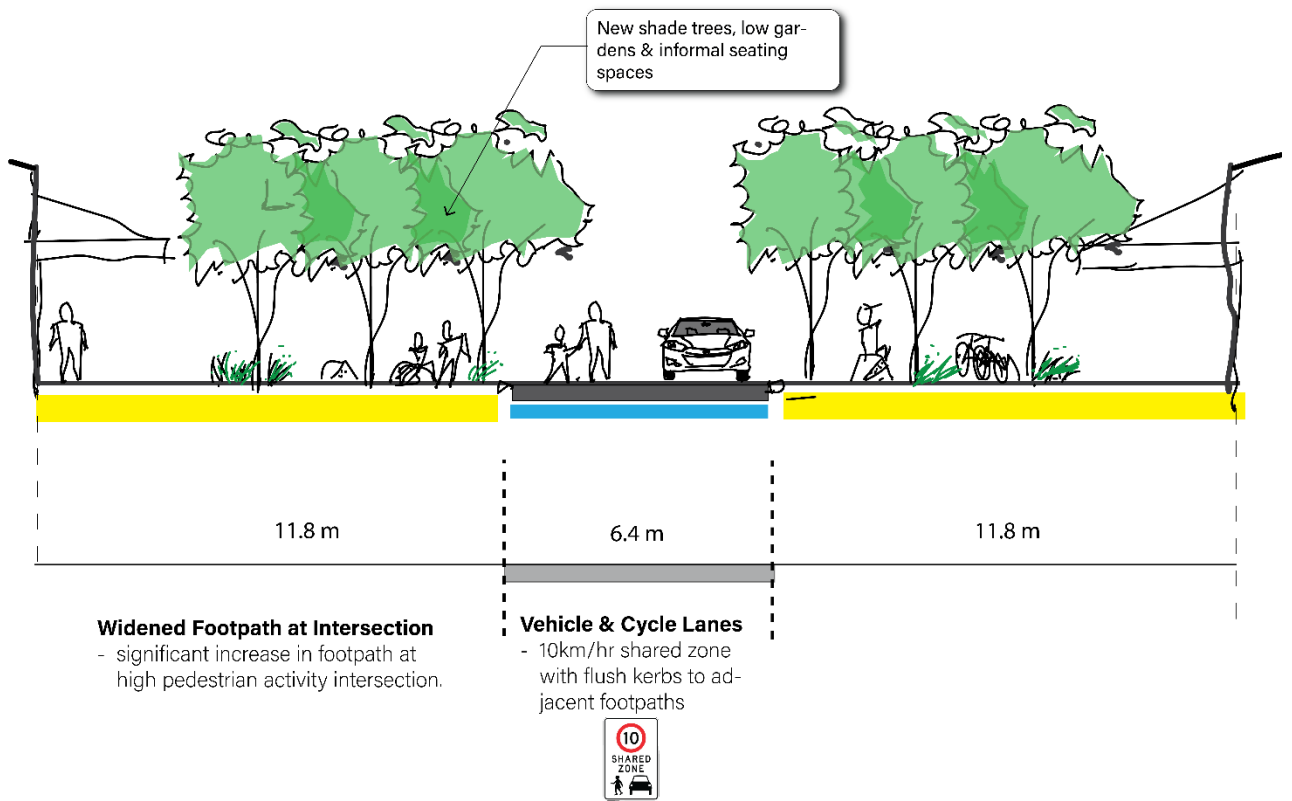
	High pedestrian activity street - 30km/hr lane-vehicles & bicycles.		Informal mid-block pedestrian crossings.		New raised zebra pedestrian crossings.		Dedicated motorbike & bicycle parking zones.		High Pedestrian Activity Areas : 30km/hr low speed travel lanes shared between vehicles & bicycles.
	Proposed 60 deg. angled carparking.		Outdoor dining zones on footpaths		Dedicated mobility parking spaces.		Dedicated electric vehicle charging parks.		10km/hr fully shared pedestrian priority zone.
	Laneway entry/exits at same level as footpath.		Feature public realm spaces with informal seating, gardens and shade trees		New bus stop and bus pull-in zone.		Water fountains.		
	New kerbline with widened footpaths.		New shade trees with low local species refer to shade tree strategy.		Dedicated service/loading bay.		Bicycle users to share travel lane with vehicles.		
	Raised 10km/hr low speed-shared zone with pedestrian priority crossing.				Dedicated kiss & ride/taxi parking.				

The following images present the current street section and the proposed sections for the length of Burringbar Street and the Burringbar Street/Stuart Street intersection (proposed 10km Shared Zone).





SECTIONS - Proposed Burringbar Street BURRINGBAR STREET CONCEPT DESIGN : Talking Street



SECTIONS - Proposed Intersection of Burringbar & Stuart St. BURRINGBAR STREET CONCEPT DESIGN

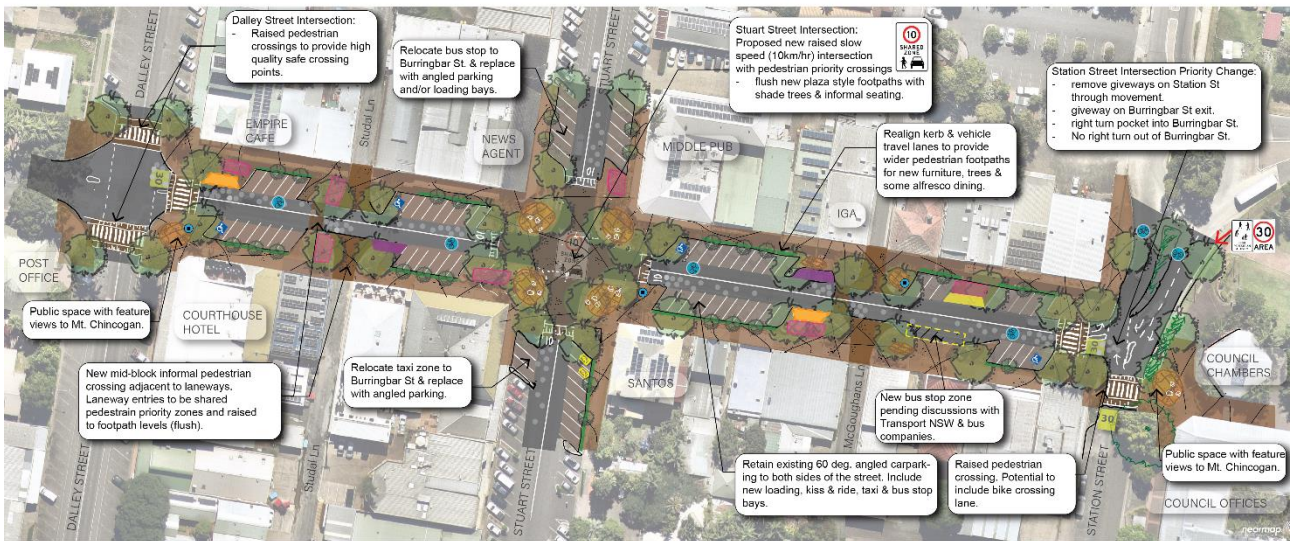
5 **RECOMMENDATION:**

That the Local Traffic Committee note the Draft Concept Design (E2024/16870) and Street Sections for Burringbar Street and provide advice.

Attachments:

10

- 1 Burringbar Street Draft Concept Design, E2024/16870 , page 89 [↓](#) 
- 2 Burringbar Street Concept Design Exhibition Report, E2024/3881 , page 90 [↓](#) 



	High pedestrian activity street - 30km/hr lane-vehicles & bicycles.		Informal mid-block pedestrian crossings.		New raised zebra pedestrian crossings.		Dedicated motorbike & bicycle parking zones.	 High Pedestrian Activity Areas - 30km/hr low speed travel lanes shared between vehicles & bicycles. 10km/hr fully shared pedestrian priority zone.
	Proposed 60 deg. angled carparking.		Outdoor dining zones on footpaths.		Dedicated mobility parking spaces.		Dedicated electric vehicle charging parks.	
	Laneway entry/exits at same level as footpath.		Feature public realm spaces with informal seating, gardens and shade trees.		New bus stop and bus pull-in zone.		Water fountains.	
	New kerbline with widened footpaths.		New shade trees with low gardens - local species refer to shade tree strategy.		Dedicated service/loading bay.		Bicycle users to share travel lane with vehicles.	
	Raised 10km/hr low speed-shored zone with pedestrian priority crossing.		Dedicated kiss & ride/taxi parking.					20m <small>©2021 The Council of Byron Shire</small>

DRAFT CONCEPT PLAN

BURRINGBAR STREET CONCEPT DESIGN



EVENTS

Report No. 8.1 Mullum Laneways 2024 - Kaleido Mullumbimby

5 **File No:** I2024/135

Council has received an Event Application for Mullum Laneways Festival, an event to be held on 5 May 2024.

10 Previous Event presented to LTC was under the name of Blues on Burringbar. The organisers are hoping to do similar event each year.

RECOMMENDATION:

- 15 1. That Council supports Mullum Lane Ways Festival to be held 5 May 2024 10am to 10.00pm.
2. That Council support in Part 1 is subject to:
- a) separate approvals by NSW Police and TfNSW being obtained;
- b) the event organiser providing council with an updated Traffic Management Plan and Traffic Guidance Scheme/s for the event;
- 20 c) development and implementation of a Traffic Management Plan and Traffic Guidance Scheme/s by those with appropriate TfNSW accreditation and the holding of current and appropriate levels of insurance and liability cover;
- d) the impact of the event be advertised, and charged at cost to the organisers, via a notice in the local weekly paper and appropriate variable message signage a minimum of one week prior to the operational impacts taking effect, noting it must include the event name, specifics of any traffic impacts or road closures and times, alternative route arrangements, event organiser, a personal contact name and a telephone number for all event related enquiries or complaints;
- 25
- e) the event be notified on Council's web page with the event organiser supplying Council with the relevant information.
- 30
3. The event organiser to:
- a) inform the community and businesses that are directly impacted (e.g. within road closure zones) via written information which is delivered to the property

BYRON SHIRE COUNCIL




LOCAL TRAFFIC COMMITTEE MEETING

8.1

in a timely manner so as to document, consider and respond to any concerns raised;

- b) arranging for private property access and egress affected by the event;
 - 5 c) liaising with bus, taxi and waste operators and ensuring arrangements are made for provision of services during conduct of the event;
 - d) consulting with emergency services and any identified issues be addressed;
 - e) holding \$20m public liability insurance cover which is valid for the event;
 - f) paying Council's Road Event Application Fee prior to the event;
 - g) not place any signage on the road related area of the Pacific Highway.
- 10 4. That provisions are made for safe access for heavy vehicles at the intersections of Stuart Street/Burringbar Street, and Stuart Street/Tincogan Street.

Attachments:

- 15 1 Event - Mullum Lane Way Fest 2024 - TGS - Burringbar St Mullumbimby, E2024/9367 , page 120 [↓](#) 
- 2 Event - Mullum Laneways Festival 2024 - Summary, E2024/9369 , page 123 [↓](#) 
- 3 Event - LTC - Mullum Laneway Festival - Burringbar St Mullumbimby (showing detour), E2024/11496 , page 127 [↓](#) 

20

Background

Byron Shire Council received an application in 2023 through the Events & Festivals Sponsorship Program from Mullum Lane Ways Festival (Kaleido-Mullumbimby). The program is aimed at supporting community events.

- 5 The event, under the auspice of Mullumbimby Chamber of Commerce, were a recipient of this funding. Their program allows a series of creative producers to bring together the arts, music, creative workshops, puppetry, and family-kids interactive activities.

10 The Mullumbimby Chamber of Commerce have experience with a similar event (Biggest Little Banquet) they used to run, that was successfully run for several years and a similar concept to this Blues on Burringbar in 2023.

Event Overview

This community event will bring locals together in a day of free musical entertainment and family fun activities.

15 The event will temporarily close a section of Burringbar Street between Stuart Street and Daley Street from 10am to 10pm. Traffic will be detoured around Stuart Street and Whian Street and Tincogan Street both linking back up with Daley Street.

The Traffic Guidance Scheme for the event is provided at attachment 1.

The event overview is provided at attachment 2.

The event detour layout is provided at attachment 3.

20 Key Issues

While the proposal closes the road it will not prevent pedestrian access to businesses and will support bringing more people to local businesses, therefore, the closure is not seen as being a negative for local business or those using the town centre.

Council Implications

25 Financial Considerations

Nil, costs borne by applicant.

Asset Management

Not applicable.

Policy or Regulation

30 All TGS's are to be developed and implemented by people with the appropriate level and type of accreditation.

Consultation

A condition of the endorsement of this event is that the event organisers conduct appropriate community consultation, including:

- 5
1. Advertising the impact of the event in the local newspaper and on the Council website.
 2. Informing community and business that are directly impacted.
 3. Liaising with bus and taxi operators.
 4. Consulting with emergency services.

Legal and Risk Management

10

