

# Notice of Meeting

## Local Traffic Committee Meeting

A Local Traffic Committee Meeting of Byron Shire Council will be held as follows:

Venue	Conference Room, Station Street, Mullumbimby
Date	Tuesday, 10 September 2024
Time	10.30am

Phil Holloway  
Director Infrastructure Services

*I2024/1244  
Distributed 03/09/24*

**BYRON SHIRE COUNCIL**  
**LOCAL TRAFFIC COMMITTEE MEETING**

**BUSINESS OF MEETING**

**1. APOLOGIES**

**2. DECLARATIONS OF INTEREST – PECUNIARY AND NON-PECUNIARY**

**3. ADOPTION OF MINUTES FROM PREVIOUS MEETINGS**

3.1 Local Traffic Committee Meeting held on 11 June 2024

**4. MATTERS ARISING**

**5. OUTSTANDING ISSUES/RESOLUTIONS**

**6. REGULATORY MATTERS**

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REGULATORY MATTERS

**Report No. 6.1 Bayshore Drive Roundabout**

**File No:** I2024/986

5

The purpose of this report is to gain Council support for new regulatory signage and line marking for a proposed roundabout on Bayshore Drive, Byron Bay as shown in Attachment 1 (E2024/72969).

10 Council has received a Roads Act application associated with a development approval (10.2022.107.2) for a three (3) lot subdivision at 23 Bayshore Drive BYRON BAY. As part of stage 2 works, construction of a roundabout, footpaths, pedestrian crossing, refuge island, landscaping and associated works are required.

15 A Traffic Management Plan (TMP), Traffic Guidance Scheme (TGS) & Public Safety Management Plan (PSMP) have been submitted in order to construct the roundabout safely with the least disturbance to the existing traffic and pedestrians – refer Attachment 2 (E2024/73330).

20 Existing no stopping signage and speed limits will need to be covered or removed during construction. The new proposed final regulatory signage and line marking is indicated on the plans in Attachment 1(E2024/72969). Temporary Lane/road closures with temporary traffic lights and diversions are proposed around the staged work zone.

Its proposed to notify local businesses 1-2 weeks prior with letter box drops. VMS boards will also be installed. Night works should be considered as an alternative to mitigate traffic congestion and improve safety.

25 Traffic in and out Byron Fair (The IGA) has been considered for each stage of works as listed in the PSMP.

Conditions 2, 3 & 4 of development consent 10.2022.107.2 partly state:

**2. Delivery of Stage 2 – Construction Works**

30 Stage 2 Construction works must be completed in accordance with the approved detailed design of the roundabout and associated works within 24 months of the issue of the Subdivision Certificate. No further development (being construction work approved by way of Construction Certificate, Complying Development Certificate, or Subdivision Works Certificate to occur on Lots 1 or 2 until such time as the Roundabout is constructed.

**3. Engineering Construction Plans**

35 Engineering construction plans and specifications must accompany the Subdivision Works Certificate application demonstrating compliance with Council's standards for the required engineering works.

# BYRON SHIRE COUNCIL

## LOCAL TRAFFIC COMMITTEE MEETING

6.1

### a) Roundabout in Bayshore Drive

The design must meet the minimum requirement in Austroads Guide to Road Design

## **4. Staging of Construction Works**



### **Stage 2**

- 5
    - Separate application of Section 138 of the Roads Act of 1993
    - Roundabout in Bayshore Drive. Refer to Geolink drawing 3544/D/004 C.
    - Associated footpath, pedestrian crossing, refuge island and other pedestrian facilities extended to connect onto the existing pedestrian network infrastructure. Refer to Geolink drawing 3544/D/004 C.
  - 10
    - Drainage, Streetlighting and associated relocation of services
- 

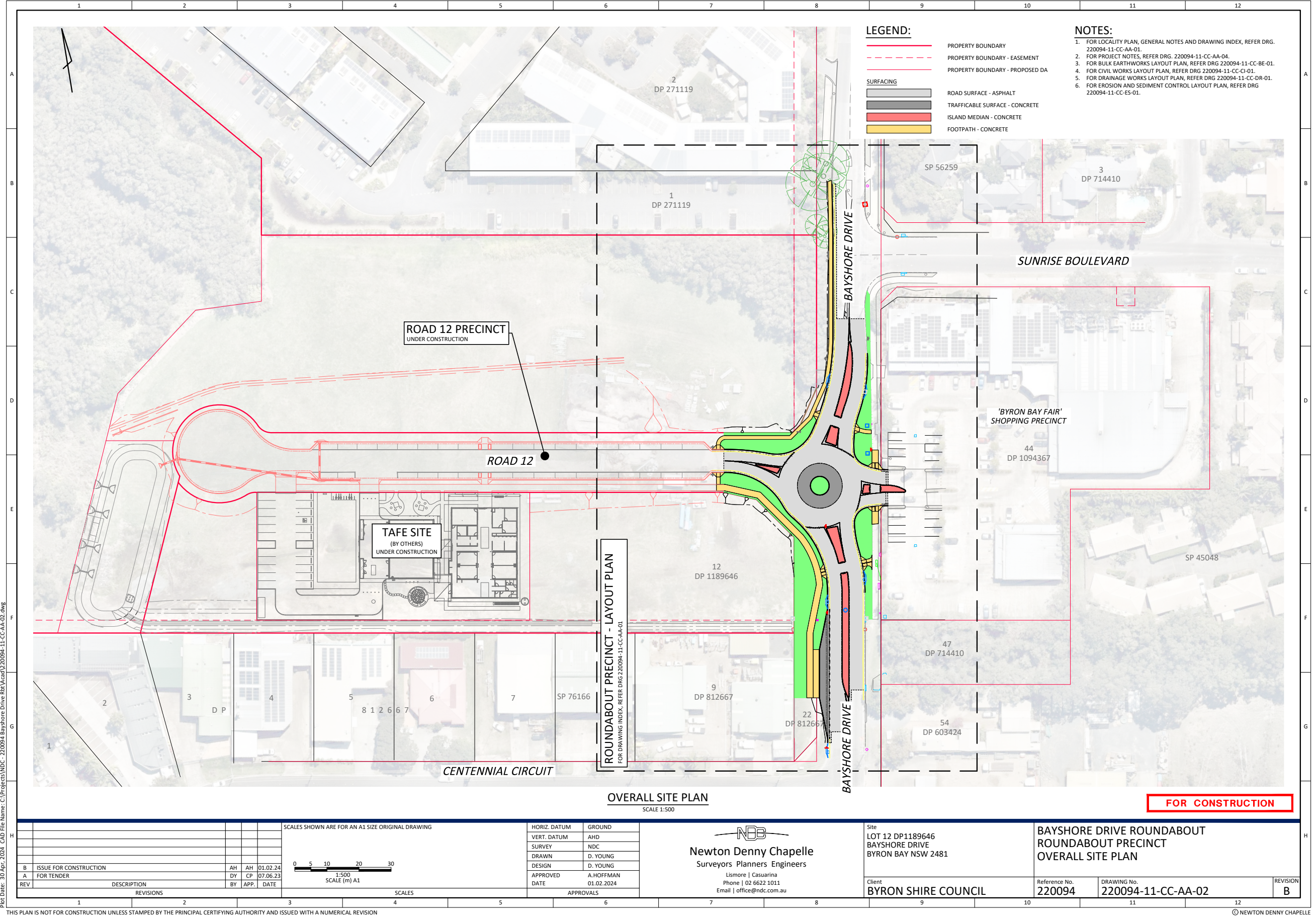
### **RECOMMENDATION:**

- 15 **That Council support the new signage, line markings and traffic control devices associated with the new Roundabout on Bayshore Drive, Byron Bay as shown in Attachment 1 (E2024/72969).**

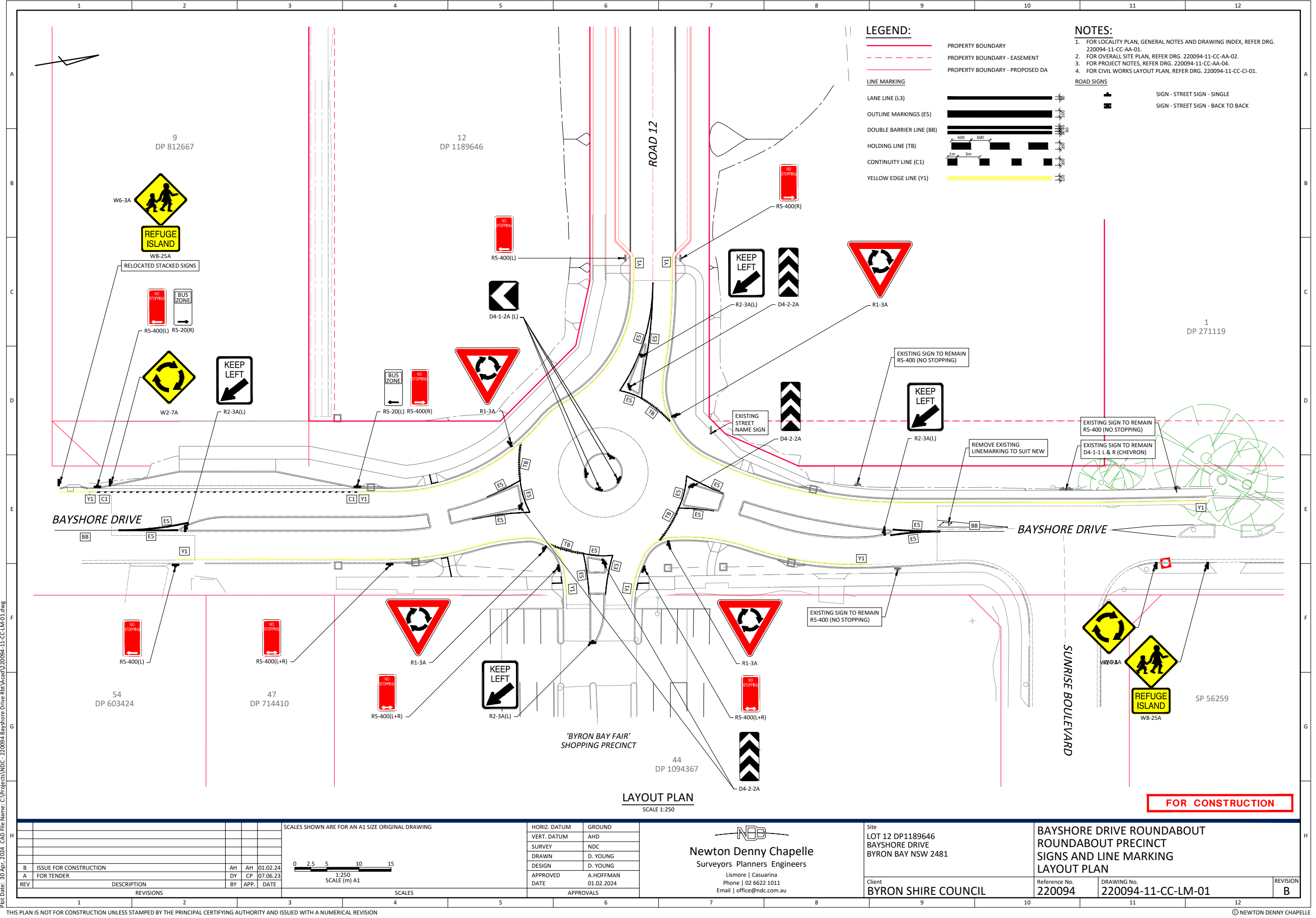
### **Attachments:**

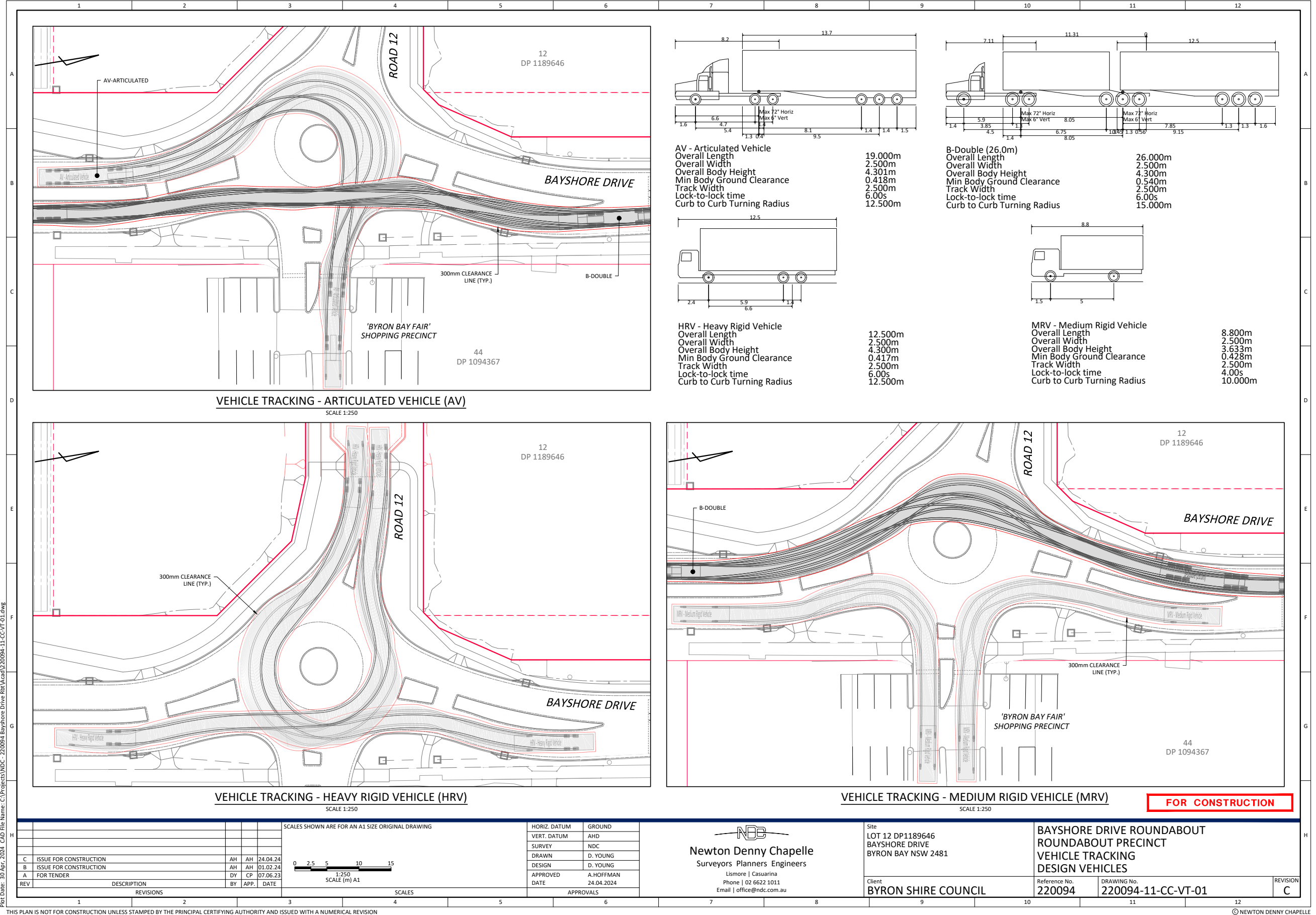
- 1 51.2022.107.1 & 14.2022.107.1- LTC submission, E2024/72969 , page 5 [↓](#) 
- 20 2 51.2022.107.1 TMP & PSMP, E2024/73330 , page 8 [↓](#) 

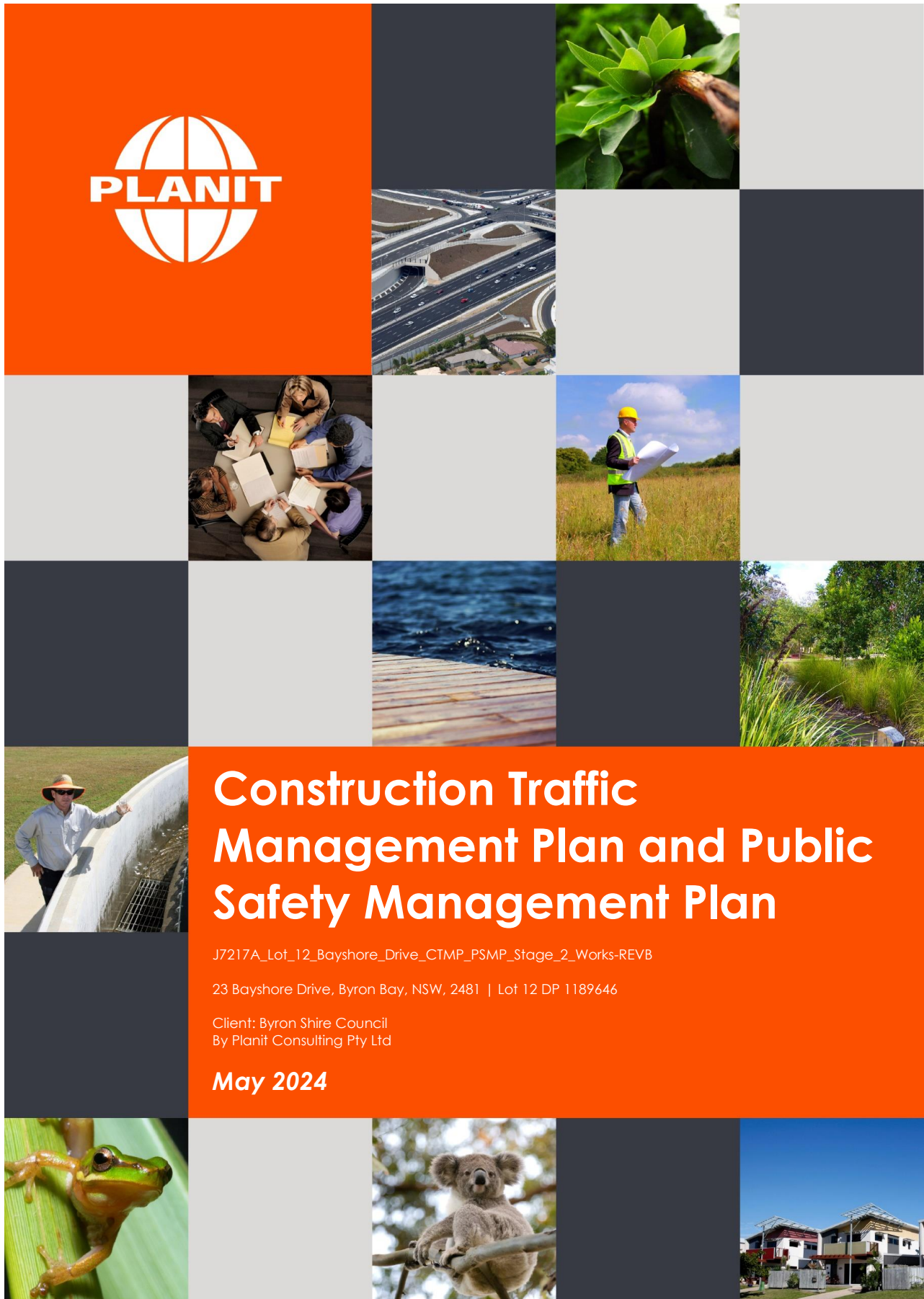












# Construction Traffic Management Plan and Public Safety Management Plan

J7217A\_Lot\_12\_Bayshore\_Drive\_CTMP\_PSMP\_Stage\_2\_Works-REVB

23 Bayshore Drive, Byron Bay, NSW, 2481 | Lot 12 DP 1189646

Client: Byron Shire Council  
By Planit Consulting Pty Ltd

**May 2024**

Construction Traffic Management Plan (CTMP) & Public Safety Management Plan (PSMP)  
23 Bayshore Drive, Byron Bay, NSW, 2481 | Lot 12 DP1189646  
Byron Shire Council  
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01	31/01/2023	Issued for Council Approval - REV A	CW	
02	16/03/2023	RFI response	CW	
03	14/05/2024	Stage 2 Works- Section 138 (S138) Application	CW	

**Project Details:**

<b>Project Name</b>	Lot 12 Bayshore Drive, Byron Bay
<b>Client</b>	Byron Shire Council
<b>Client Project Manager</b>	Matt Plain
<b>Authors</b>	Chaej Wrencher
<b>Planit Reference</b>	J7217A_Lot_12_Bayshore_Drive_CTMP_PSMP_Stage_2_Works-REVB

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### 1.1 Conditions of Consent

<p><b>4. Staging of Construction Works</b></p> <p><b>Stage 1</b></p> <ul style="list-style-type: none"> <li>• Tafe Access (Chainage 0.000 to Chainage 35.025). Refer to Newton Denny Chapelle drawing 220094-12-CC-CI-01 A</li> <li>• Shared Path including Pedestrian Crossing in Bayshore Drive. Refer to Newton Denny Chapelle drawing 220094-12-CC-CI-01 B</li> <li>• Internal Road 12 Roadworks including Drainage, Services Infrastructure &amp; associated relocation of existing services and Streetlighting. Refer to Geolink drawing 3544/D/004 C.</li> </ul> <p><b>Stage 2</b></p> <ul style="list-style-type: none"> <li>• Separate application of Section 138 of the Roads Act of 1993</li> <li>• Roundabout in Bayshore Drive. Refer to Geolink drawing 3544/D/004 C.</li> <li>• Associated footpath, pedestrian crossing, refuge island and other pedestrian facilities extended to connect onto the existing pedestrian network infrastructure. Refer to Geolink drawing 3544/D/004 C.</li> <li>• Drainage, Streetlighting and associated relocation of services</li> </ul>	<p><b>8. Public Safety Management Plan required</b></p> <p>Prior to certification of Crown building works, consent from Council must be obtained for a Public Safety Management Plan for those works within the road reserve pursuant to Section 138 of the Roads Act 1993. This public safety management plan is to include provision for (but not be limited to):</p> <ol style="list-style-type: none"> <li>a) a pedestrian barrier, alternative footpaths and ramps as necessary;</li> <li>b) lighting of the alternative footpath between sunset and sunrise;</li> <li>c) the loading and unloading of building materials;</li> <li>d) parking space for tradesman's vehicles, where such vehicles must be located near the site due to tools and equipment contain within the vehicle;</li> <li>e) Removal of any such hoarding, fence or awning as soon as the particular work has been completed.</li> </ol> <p>The temporary use of Council land/road reserve to enable construction work or an event is subject to fees in accordance with Council's adopted fees and charges. The use fee must be paid upfront prior to use and will generally be required to be paid prior to issue of the Council's consent for such use.</p> <p><b>9. Traffic Management Plan (TMP)</b></p> <p>Prior to issue of the Crown building works certification, consent from Council must be obtained for a Traffic Management Plan (TMP) pursuant to Section 138 of the Roads Act 1993. The plans and specifications are to include the measures to be employed to control traffic (inclusive of construction vehicles) during construction of the development. The TMP is to be designed in accordance with the requirements of the current version of the Transport for NSW Traffic Control at Work Sites Technical Manual.</p> <p>The report must incorporate measures to ensure that motorists using road adjacent to the development, residents and pedestrians in the vicinity of the development are subjected to minimal time delays due to construction on the site or adjacent to the site.</p> <p>The TMP and associated traffic guidance scheme/s must be prepared by a suitably qualified Transport for NSW accredited person.</p>
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Figure 2: Excerpt From Conditions of Consent (Condition 4,8 & 9)

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## 2 Site Context

### 2.1.1 Site Description

The proposed development relates to the following site located on 23 Bayshore Drive, Byron Bay - Lot 12 DP1189646. The property is 5.8ha in total and is located within the Byron Bay industrial Centre. The subject site has recently been developed to include new TAFE building, internal road, and bioretention basin, and with the remainder of the site being left as undeveloped wetland.

Refer to Figure 3 for the Nearmap aerial imagery below of the existing land usages surrounding the subject site.



Figure 3: TAFE and New Roundabout Location (Source: Nearmaps)

As part of the conditions of consent, the new TAFE will be subjected to additional traffic generation thus a new roundabout is required to support the new development and increase in traffic in the surrounding area.

The purpose of this report is to provide a Construction Traffic Management Plan (TMP) and Public Safety Management Plan (PSMP) in support of the proposed new roundabout on Bayshore Drive.

### 2.1.2 Surrounding Land Uses

The new roundabout location is situated in a **B1 – Neighborhood Centre** adjacent to the new TAFE which is situated in an **B7 – Business Park** as defined by the NSW Department of Planning – Byron Local Environmental Plan 2014 and is illustrated in Figure 4.

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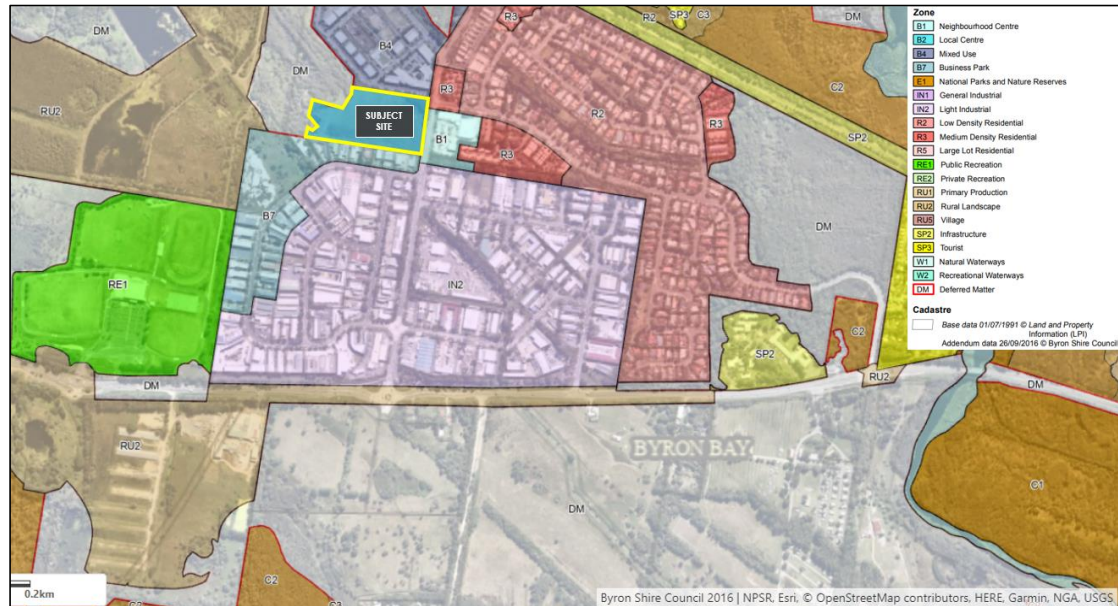


Figure 4: Byron Local Environment Plan 2014 - Local Land Use Map



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### 3 Existing Transport Facilities

#### 3.1 Road Hierarchy

The NSW administrative road hierarchy comprises the following road classifications, that align with the generic road hierarchy as follows:

- **State Roads:** Freeways and Primary Arterials (TfNSW managed);
- **Regional Roads:** Secondary or Sub-arterials (Council managed, partly funded by the state);
- **Local Roads:** Collector and local access roads (Council managed).

##### 3.1.1 Bayshore Drive

Bayshore drive is the main collector road that fronts the subject site and is managed by Byron Shire Council (Figure 5).



Figure 5: Byron Shire Council Interactive Mapping - Road Hierarchy

The existing road network fronting the subject site is defined in Table 1:

Table 1: Existing Road Network Summary - Bayshore Drive

Bayshore Drive	
Road Hierarchy	Collector Road
Alignment	North – South
Number of Lanes	2 Lanes
Carriageway Type	Single Carriageway (two lanes)
Carriageway Width	Approximately 12.8m
Posted Speed Limit	50km/hr
School Zone	No
Parking Controls	No Stopping
Forms Site Frontage	Yes

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Figure 6: Bayshore Drive - Northbound Aspect (Source: Google Maps)



Figure 7: Bayshore Drive - Southbound Aspect (Source: Google Maps)

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### 3.2 Primary Intersections Surrounding the site

The primary intersections in the vicinity of the development site are as follows:

- Ewingsdale/Bayshore Drive Roundabout; and
- Bayshore Drive and Sunrise Boulevard (Blvd) – T- Intersection.

Refer to Figure 8 below for the exact locations of the key intersections.

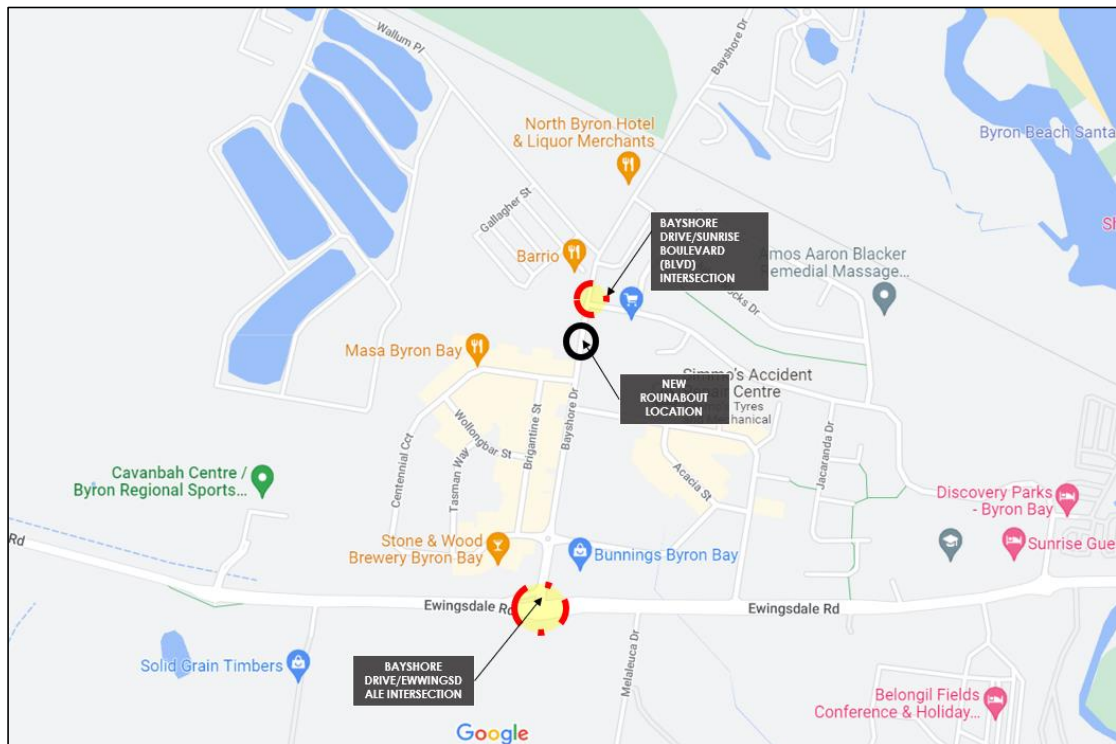


Figure 8: Key Intersections Surrounding the Subject Site (Source: Google Maps)

### 3.3 Public Transport (PT)

The locality has been evaluated in relation to the public transportation options available for prospective staff and visitors. The "NSW Guidelines to Walking & Cycling" (2004) indicate that 400m-800m is considered a "comfortable" walk, and this area is depicted in Figure 9. This information was used to determine the accessibility of the location.



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Figure 9: Public Transport Accessibility (Source: Nearmap)

### 3.3.1 Bus Services

Blanch's Bus Company operates within the vicinity of the subject site and has multiple bus stops within comfortable walking distance. This allows workers, staff, and visitors to use PT to travel to and from the development.

The primary bus routes and frequencies are detailed in Figure 10.

 Bus & Coach	ROUTE	PLACES	DAYS	OPERATOR	PHONE	TIMETABLE
	640X/641X	Byron Bay > Lennox Head > Ballina > Lismore	Mon-Fri	Blanch's	02 6686 2144	<a href="#">Timetable</a>
	637	Byron Bay > Sunrise Beach Arts & Industry Estate	Mon-Sa	Blanch's	02 6686 2144	<a href="#">Timetable</a>
	637	Byron Bay > Suffolk Pk > Byron Hills > Baywood Chase	Mon-Sa	Blanch's	02 6686 2144	<a href="#">Timetable</a>
	610/635	Mullumbimby > Brunswick Heads > Byron Bay > Bangalow > Goonemerry > Federal > Eureka > Ellimah > Lismore	Mon-Fri	Ballina Bussines	02 6686 9666	<a href="#">Timetable</a>
	640	Ballina Airport > Ballina > Lennox Head > Suffolk Park > Byron Bay > Mullumbimby	Mon-Sun	Blanch's	02 6686 2144	<a href="#">Timetable</a>
	641	Ballina > Ballina Heights > Newrybar > Bangalow > Ewingsdale > Byron Bay	Mon-Fri	Blanch's	02 6686 2144	<a href="#">Timetable</a>
	645	Mullumbimby > Brunswick Heads > Ocean Shores > Shopping Centre > New Brighton > South Golden Beach > Billinudgel	Mon-Sat	Brunswick Valley Coaches	02 6680 1566	<a href="#">Timetable</a>
		Coorabell > Byron Bay	School days	Campbell	02 6684 1593	
		Sydney > Brisbane	Mon-Sun	Greyhound	1300 473 945	<a href="#">Website</a>
		Coach connection to Sydney rail service	Mon-Sun	NSW TrainLink	13 22 32	<a href="#">Website</a>
		Sydney > Brisbane	Mon-Sun	Premier Motor Services	13 34 10	<a href="#">Website</a>
		Lismore > Brisbane (via Byron Bay)	Mon-Sun	Premier Motor Services	13 34 10	<a href="#">Website</a>

Figure 10: Bus Operational Hours Surrounding the Subject Site (Source: <https://goingplaces.org.au/regions/byron-shire/byron-bay/>)

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### 3.3.2 Alternative Vehicle Services

Additionally, there is several alternative vehicle services currently operating in the town and surrounding areas. The following alternative vehicles include but are not limited to:

- Uber drivers;
- Taxis;
- Electric Bikes;
- Solar Train;
- Bat Bus;
- Go Byron;
- Shared car scheme; and
- Community transport.

Refer to Figure 11 and 12 for existing PT services.

 Services	SERVICE	PHONE	MORE INFORMATION
	Byron Shire Transport Guide		<a href="#">Download</a>
	Tweed Byron Ballina Community Transport	1300 875 895	<a href="#">Website</a>
	BAT Bus	02 6685 8771	<a href="#">Website</a>
	Brisbane 2 Byron	1800 626 222	<a href="#">Website</a>
	Byron Bay Taxis & Limousines	02 6685 5008	<a href="#">Website</a>
	Byron easyBus	02 6685 7447	<a href="#">Website</a>
	Byron Bay to Brisbane greyhound Express	1300 473 946	<a href="#">Website</a>

Figure 11: Alternative PT Services (Source: <https://goingplaces.org.au/regions/byron-shire/byron-bay/>)

SOLAR TRAIN TIMETABLE									
WEDNESDAY JAN 3 – SATURDAY JAN 20 (NO SERVICE SUNDAY JAN 7, 14 + 21)									
NORTH BEACH DEPART	10.00	11.00	12.00	13.00	14.00	15.00	16.00	17.00	
BYRON BEACH ARRIVE	10.10	11.10	12.10	13.10	14.10	15.10	16.10	17.10	
BYRON BEACH DEPART	10.15	11.15	12.15	13.15	14.15	15.15	16.15	17.15	
NORTH BEACH ARRIVE	10.25	11.25	12.25	13.25	14.25	15.25	16.25	17.25	

Figure 12: Byron Solar Train Timetable

### 3.3.3 Parking

Parking for workers shall be provided at the New TAFE, IGA precinct and available free parking.

### 3.3.4 Pedestrian and Cyclist Facilities

Existing footpaths and bike paths are provided along Bayshore Drive, and the work area is to be clearly delineated to ensure it does not affect pedestrian safety or access.



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## 4 Public Protection Controls

### 4.1 Public Protection Controls

Planit have identified that specific public protection and exclusion controls need to be implanted throughout the duration of the project. These include signage informing of any site activity and barriers preventing entry to the site. It is noted majority of works will be on road reserve and there will be impacts to road users and pedestrians on Bayshore Drive and all adjacent roads to site.

To achieve adequate public protection and exclusion zones the following is proposed on the Public Safety Management Plan (Refer to the attached PSMP – J7217 – SK001):

- Provide minimum clearance of workers to traffic as described in Technical Manual – traffic Control at Work Sites Section 4.3.4
- Adequate safety barriers must be installed to protect workers from adjacent traffic if minimum clearances are not achieved. Safety barriers as recommended by TfNSW in TS 00028:1.0
- Adequate lighting of temporary devices during low light conditions;
- Control Signs will be erected to inform Pedestrians to use other footpath during the construction phases of the project (refer to PSMP and the TGS plans in Appendix A);
- For loading and unloading of building materials, public safety measures shall be with the Traffic Guidance Scheme (TGS) measures. Refer to the attached Traffic Guidance Scheme (TGS) and Public Safety Management Plan (PSMP) in Appendix A; and
- Removal of any hoarding, fence or awning shall take place as soon as the works for which they are required is completed.

#### 4.1.1 Cyclists and Pedestrians

Cyclists and pedestrians currently use this area to access local shops and businesses. Due to the large volume of traffic in the area most cyclists would be using the footpaths to travel through this area. The footpaths are used by pedestrians to access shops in the area.

To enable businesses to still function during construction, it is proposed to delineate the works area with safety barriers and 1.8-metre fencing, this shall allow safe pedestrian access around proposed work areas. The fencing will have warning signs erected on it to advise the public to keep out of the works area.

Throughout the various stages of construction, pedestrians will require alternative access points to reach local businesses. As construction progresses, pedestrians will be directed to cross streets at designated locations, adhering to standard road rules. Traffic control signs are proposed to guide pedestrians safely around the construction site.

#### 4.1.2 Existing Signage

There are numerous signs currently in this area (i.e., advertising signs, posted speed limits, advisory signs, no stopping signs). It is proposed all traffic signs and advance warning signs will be placed by a suitably qualified traffic officer and checked and reported daily. If signs are being moved or removed than permanent fixtures may be required.

#### 4.1.3 Public Notification

Occupants of the properties and business that adjoin the works will be notified through a letter drop 1-2 weeks prior to construction. The letter drop will outline the following as a minimum:

- Proposed works;
- Timeframes;
- Potential delays to residents; and
- Relevant Contact information.

It is imperative for the contractor to keep a record of all communications with the public and to close out any concerns in a reasonable time frame. BSC and Contractor will meet weekly to discuss community communications and safety concerns.

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#### **4.1.4 Variable Message Signs**

A VMS will be set up on Bayshore drive to advise road users and the community of the proposed works. The VMS board will be set up 1-2 weeks prior to the proposed start date of the construction works.

#### **4.1.5 Emergency Arrangements**

Emergency services will have continual access to all businesses and properties hence no specific facilities are required. The project supervisor shall assist Emergency vehicles requiring entry/exit through the works area.

Refer to the attached Public Safety Management plan (Appendix A) for further details.

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## 5 Construction Operations

### 5.1 Construction Stages

The proposed new roundabout is anticipated to be constructed through a staged approach. It is noted during the tender process, the contractor is likely to present innovative construction methodologies to stage the proposed construction process of the new roundabout to improve safety, and efficiency of construction.

For example, due to traffic constraints surrounding the area – night works may be considered as an alternative option to mitigate traffic congestion, improve safety and construction efficiencies.

For the purpose of this TMP, the general concept construction stages are outlined below:

#### 5.1.1 Stage 1: Western Area Construction Scope:

Stage 1 involves the construction of the western and southern sections of the roundabout – as depicted in the figure 13 below:

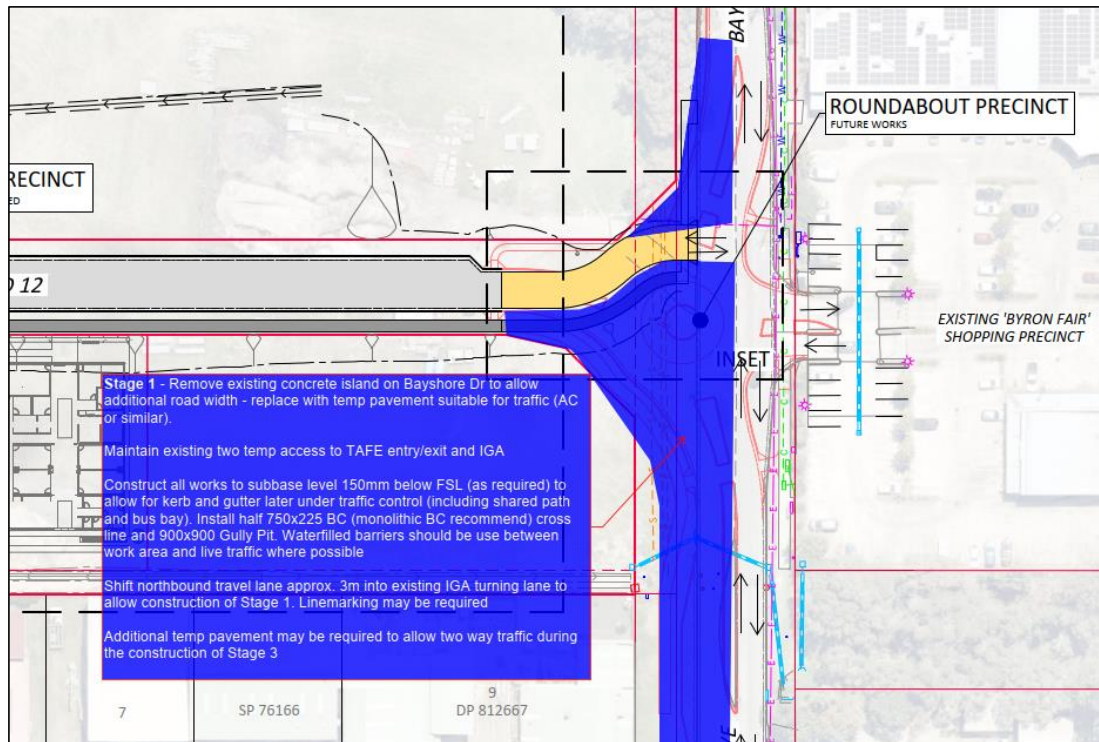


Figure 13: Conceptual Stage 1 - Western Area Construction Scope

The traffic travelling northbound (NB) on Bayshore drive will be shifted approximately 3-metres east. Access to Lot 12 internal road and Byron Fair during this stage will not be affected. It is noted that the recently constructed temporary access road shall be used during this stage of construction for vehicles entering and egressing the TAFE.

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Traffic control will be required to ensure the safe movement of vehicles, pedestrians, and construction vehicles through the project. It is anticipated traffic lights/traffic controllers will be required to manage the proposed stage 1 works. Pedestrians will not be able to use the western footpath and will need to utilise the existing footpath on the eastern side of Bayshore Drive.

#### 5.1.2 Stage 2: Removal of Temporary TAFE Access

Stage 2 construction works will involve the removal of the temporary TAFE access driveway. Access to Byron Fair (i.e., IGA complex) during this stage will not be affected, however, traffic travelling into the TAFE will be shifted on the constructed new permanent pavement (Approx. 150mm below FSL) completed in Stage 1. Minor additional temporary works will be required to the Lot 12 road batter profile. Refer to Figure 14 below for the anticipated traffic movements depicted by the arrows.

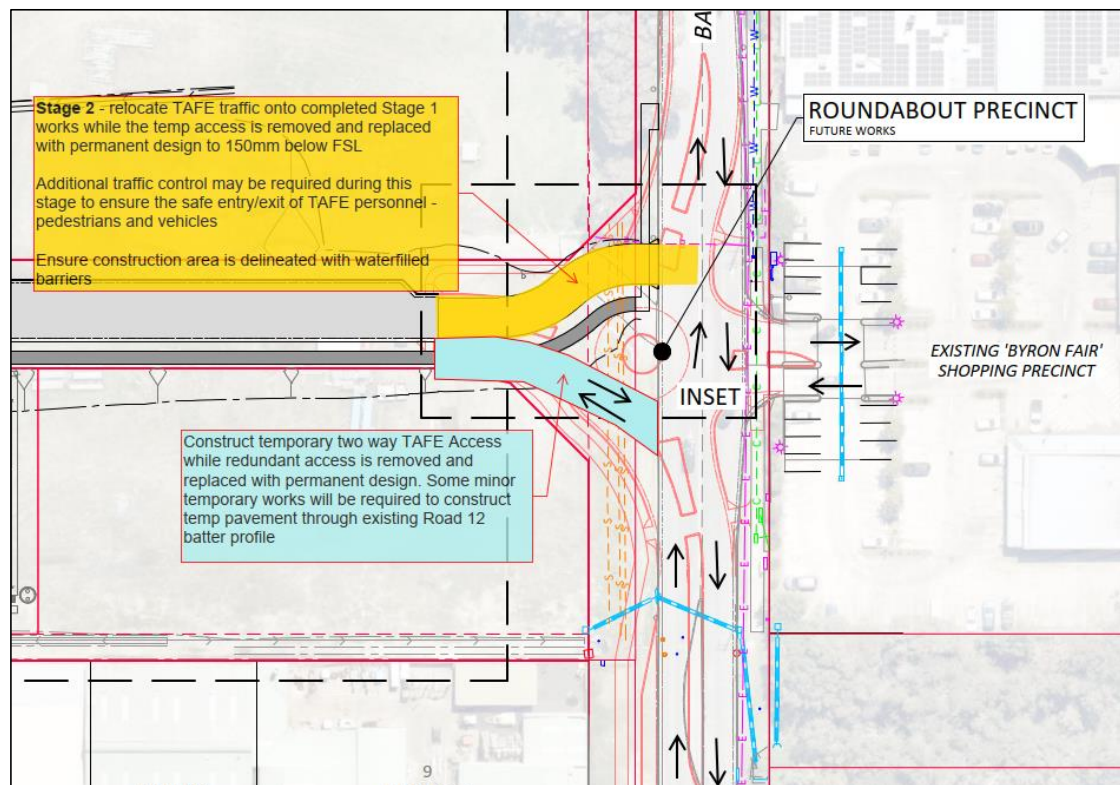


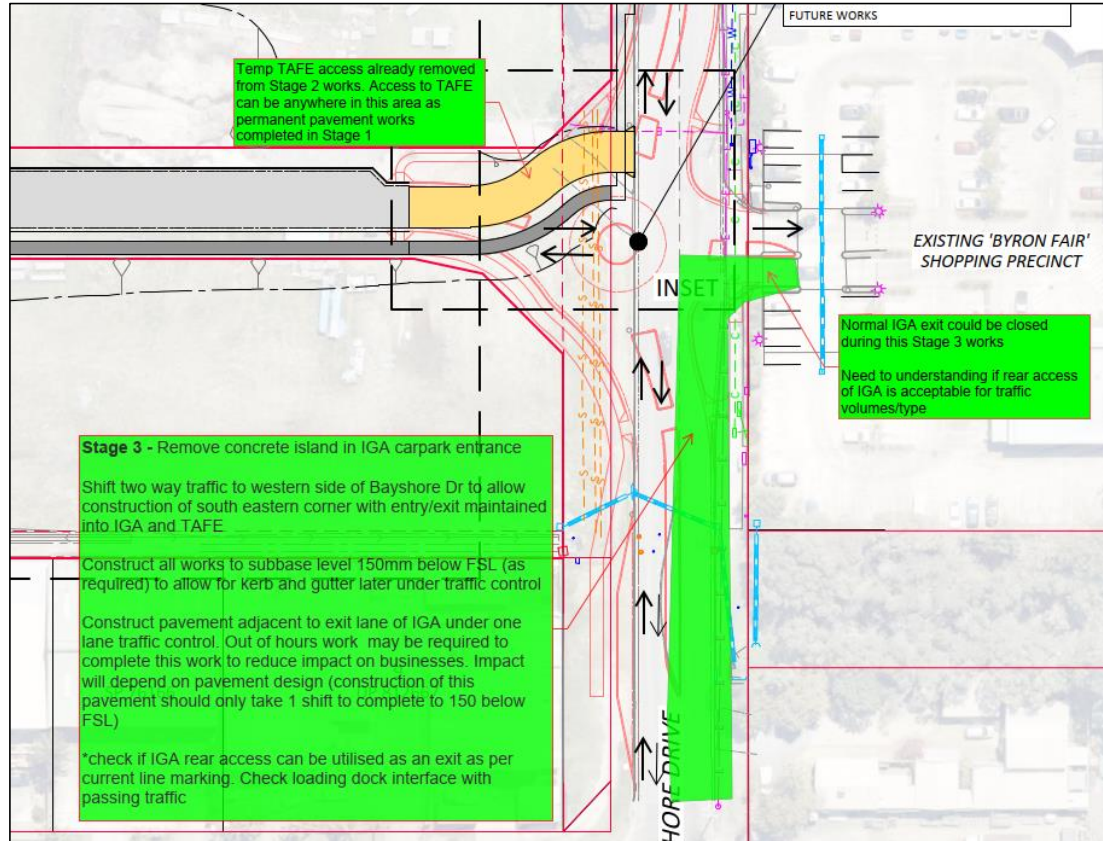
Figure 14: Conceptual Stage 2 Works - Removal of Temporary TAFE Access

#### 5.1.3 Stage 3: South-Eastern Construction Scope:

The works associated with this stage include the removal of existing kerb and gutter, concrete island within Byron Fair Precinct driveway, and constructing pavement layers will be constructed to approximately 150mm below FSL and one RCP line will be partially installed.

During this stage Bayshore Drive through traffic will be shifted west approximately 5-metres to allow the construction of the southeastern corner of the roundabout. Traffic entering the Byron Fair Precinct will not be affected by the anticipated construction works, however, traffic egressing the Byron Fair Precinct will require an alternative egress location. Refer to Figure 15 for a schematic illustration of the proposed traffic movements/arrangements.

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**Figure 15: Conceptual Stage 3 Works**

Traffic controls will be required to delineate the temporary traffic arrangements during this construction.

Furthermore, the following options are recommended for the proposed alternative traffic egression location from the Byron Bay Precinct during this stage:

- (1) Stop/go Personal to be utilised during this stage 3 construction;
- (2) It is proposed as an option traffic will enter the precinct via the existing entry location and egress the Byron precinct towards the eastern rear driveway as depicted in Figure 16 below. It is noted consultation with business owners is required to ensure that the public is notified of this temporary traffic arrangement.



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**Figure 16: Alternative Byron Bay Precinct Egress Location – Option One**

(3) Provide a temporary access location for vehicles to enter and egress the precinct.

The proposed options will be subject to Councils approval.

### 5.1.4 Stage 4: North-Eastern Scope Works

During this stage Bayshore Drive through traffic will remain the similar to Stage 3. Access to the TAFE will not be affected however only inbound traffic will only be able to access (pending consultation with business owners etc.) Byron Fair precinct through the normal outbound driveway as pavement works on Bayshore Drive adjacent to the normal entry will need to be constructed.

All existing kerb and gutter will be removed, pavement layers will be constructed to approximately 150mm below FSL and the remaining RCP line, Box Culvert line and last Gully will be installed.

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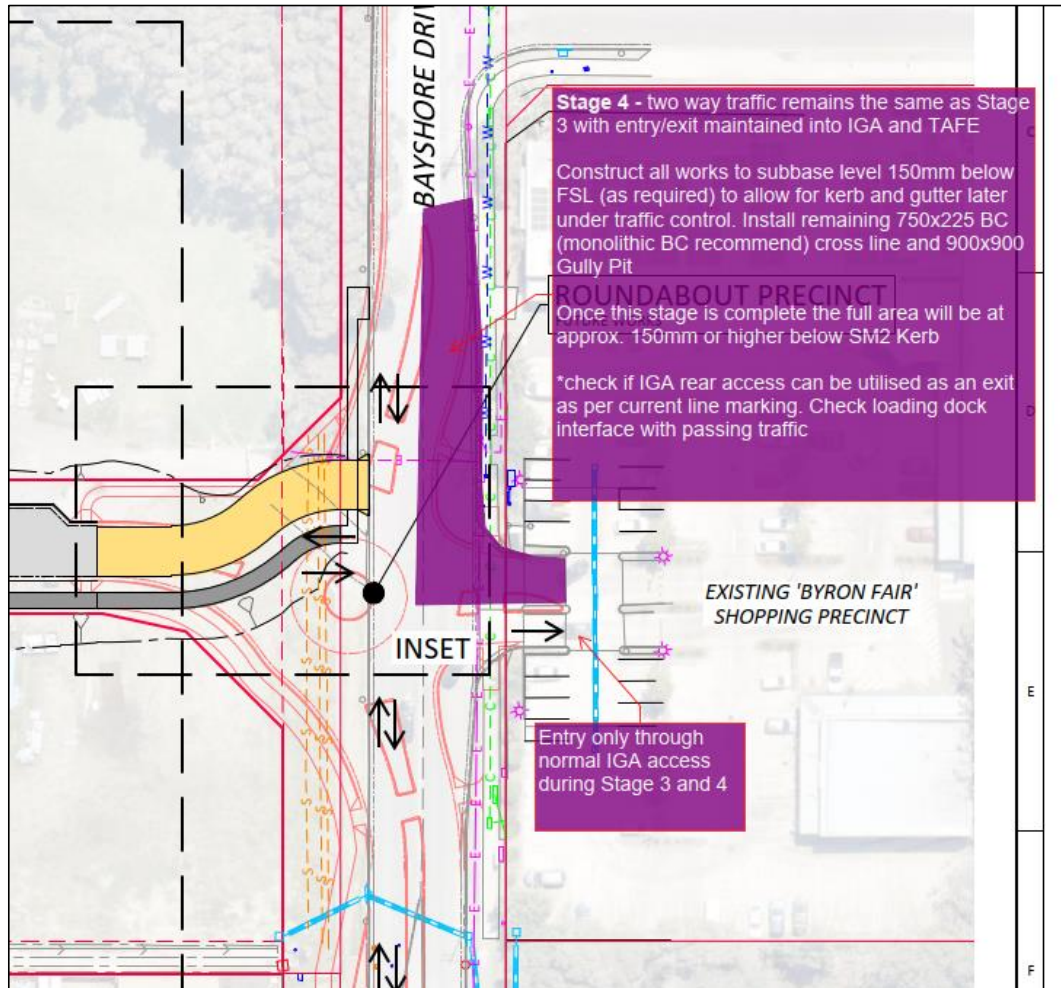


Figure 17: Stage 4 North-Eastern works

### 5.1.5 Stage 5: Full roundabout Operations

1. During this stage the pavement will be approximately 150mm below FSL to allow the construction of all new Kerb and Gutter (Upright and Semi-Mountable) and footpaths under variable stop/go traffic control conditions. This work will also include the concrete island infills and verge material behind the K&G. Access to the TAFE and Byron Fair may be restricted to one lane at times during this stage; and
2. Once all K&G is complete the remaining pavement works will take place under stop/go traffic control conditions. This will likely be the most disruptive period of the project for traffic user – for minimal disruption Asphalt layers should be placed during nightshift operations. Likely 3 x nightshifts would be required (pending detailed pavement design) to complete AC and Line marking. Finishing works such as pedestrian fencing, signage, landscaping etc. can be completed with almost disruption to the traffic user.

General Traffic Guidance Schemes (TGS) and Public Safety management plans (PSMP) are detailed in section 5.3 and Appendix A of this report.

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## 5.2 Construction Hours

As per **Condition 22 – Construction times**, construction works for the development, including delivery of machinery and materials to and from the site will occur between the hours of:

- Monday to Friday 7:00am-6:00pm; and
- Saturday 8:00am – 1:00pm.

No Construction work to take place on Saturdays and Sundays adjacent to Public Holidays and the Construction Industry Awarded Rostered Days off (RDO) adjacent to Public Holidays.

Where practicable, access and activities should be scheduled to occur outside of the peak periods. Based on existing traffic data and onsite inspections, the following peak periods are defined below:

Peak period traffic between 8:15am-9:15am in the morning, 4:00pm and 5:00pm in the afternoon.

**Note:** Council may impose on-the-spot fines for non-compliance with this condition.

## 5.3 Traffic Guidance Scheme (TGS) Plans

The following TGS plans are included in Appendix A:

- Lot 12 Bayshore Drive, Byron Bay, Traffic Guidance Scheme – SK001



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### **5.4 Construction Worker Induction & Safe Work Procedures**

#### **5.4.1 Safe Work Requirements**

To protect the safety of workers and the public, the work site should be adequately secured (i.e., security fence) to prevent access by unauthorized personnel. Additionally, all workers must be inducted at all times in accordance with the relevant SafeWork requirements and Contractor Safety Management Plan.

#### **5.4.2 Safe Work Method Statements (SWMS)**

A SWMS should be completed whenever any person undertaking works on or adjacent to the public domain or site specific.

#### **5.4.3 Truck Driver Code of Conduct**

Drivers associated with the project are to abide by a code of conduct in order to:

- Minimise impacts of construction on the local road network;
- Minimise conflict with other road users;
- Minimise road traffic noise;
- Ensure truck drivers used specified and appropriate routes; and
- Obey HHVR Regulations.

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## 6 Truck Movements

### 6.1 Construction Vehicle Routes

It is anticipated that majority of the necessary materials shall be delivered/sourced from the south (e.g., Mullum, Ballina, Lennox Head/Tweed Shire, and Gold Coast Region) and it is expected that majority of the construction trucks will be utilising the Pacific Highway as the preferred route to the subject site.

Construction vehicle movements will occur to and from the site from the Pacific Highway onto Ewingsdale Road and onto Bayshore Drive.

The key construction access and egress routes are as follows:

- **Construction traffic travelling on Ewingsdale Road/Bayshore Drive will access the site via the following proposed route:**
  - Continue straight on Ewingsdale Road for 3.0km and take the 2<sup>nd</sup> exit at the roundabout;
  - Continue onto Ewingsdale Road for 1km and take the 1<sup>st</sup> exit onto Bayshore Drive;
  - Continue Straight on Bayshore drive for approximately 100m and take the 2<sup>nd</sup> exit at the roundabout;
  - Proceed straight on Bayshore drive for 500m; and
  - Access the site by turning left into the designated site access area.
- **Construction Traffic Travelling on Bayshore Drive/Ewingsdale Road will egress the subject site via the following route:**
  - Exit the site by turning right onto Bayshore Drive and continue to travel southbound for approximately 500m;
  - Take the 2<sup>nd</sup> exit at the Bayshore roundabout and proceed straight for 100m;
  - Take the 2<sup>nd</sup> exit at the Ewingsdale Road Roundabout and proceed straight on Ewingsdale Road for 1km;
  - Take the 1<sup>st</sup> exit at the Cavanbah/Ewingsdale Road Roundabout and proceed straight for 3km; and
  - Traffic to follow signs onto the Pacific Highway.

Refer to Figure 18 for the proposed construction vehicle routes for vehicles travelling along Ewingsdale Road/Bayshore Drive to and from the subject site and Appendix B for the proposed swept paths.

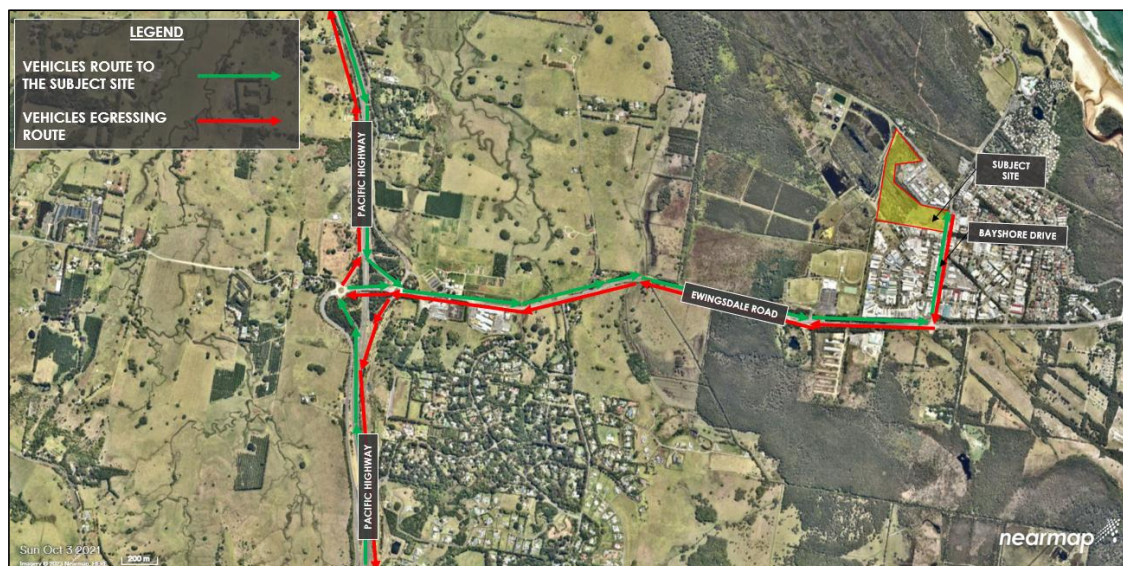


Figure 18: Proposed Construction Vehicle Routes to the Subject Site (Source: Nearmaps)

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### 6.1.1 Construction Vehicle Access to Stockpile Material

Construction Vehicles (E.g., Trucks + dog) will enter and exit the site through the existing driveway access located towards the eastern boundary of Lot 12, 23 Bayshore Drive. The location of the recently constructed temporary driveway is shown in Figure 19.



**Figure 19: Existing Driveway Access off Bayshore Drive, Byron Bay**

No issues are foreseen with swept paths and turning movements.

### 6.2 Types of Construction Vehicles

A combination of truck and vehicle types will be used during the construction of the project. Typical vehicle traffic is expected to comprise of the following vehicles:

- Light vehicles for the movement of construction personnel, including contractors, the project labour force and management staff;
- Small and medium vehicles for machinery delivery; and
- Heavy vehicle movements for the delivery and removal of construction machinery and materials, spoil and waste.

The largest vehicles used in terms of length and mass for construction activities will be:

- 16.5m Truck and Dog; and
- Semi-trailers (19m Articulated Vehicle AV).

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### 6.3 Number of Truck Movements

The number of truck movements required to cut to fill the site has been assessed based on the estimated bulk earthworks volumes and proposed earthworks construction phases. These truck movements are summarised in Table 2 with the inclusion of an estimate of the number of days required to excavate the site. The estimates below have been prepared on the assumption of excavation of approximately 20m<sup>3</sup> of cut/fill per load.

**Table 2: Earthworks and Truck Movements**

Earthworks and Truck Movements			
Stage	Balance	Number of truck loads (20m <sup>3</sup> per truck)	Number of required days
	(m <sup>3</sup> )		
Demolition	45	3	2
Cut to Fill (Approx)	470	25	10

\*It is anticipated that 16.5m Truck and dog will carry out the bulk earthworks.

### 6.4 Proposed Construction Traffic Impact on Bayshore Drive

During the construction phase, the maximum traffic volumes expected to occur during the bulk earthwork stage are described in Table 3.

**Table 3: Summary of Truck Movements**

Stage	Total Truck Movements	Total Duration (Days)	Maximum Daily Truck Volumes (Trips/day)	Daily Truck Trips (O/D)	Maximum Allowable Peak Hour Truck Trips
Pavement Works	120	20	6	3	6

As demonstrated in Table 3, during the bulk earthwork phase, the proposal development is expected to generate a maximum of 50 vehicle movements per day and approximately 4 vehicle per hour in both the AM and PM peak periods. This equates to approximately one (1) vehicle movement every (15) minutes during peak periods.

Trucks entering the site are not to exceed 4 trucks in any 1-hour period. The Project Manager and Supervisor on-site will be responsible for monitoring truck movements and reporting back to Byron Shire Council (BSC). Furthermore, trucks are restricted to one truck on or around the subject site at any given time.

Considering the low traffic volumes generated through the bulk earthwork process, the traffic impacts associated with the project are anticipated to be minimal.

### 6.5 Truck Queuing Location

Due to the low anticipated truck movements, no queuing issues are foreseen based on the proposed construction phases of the development. In the case queuing is required, this shall take place within the site boundary.

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## 7 Hazard Identification/Risk Assessment

A Hazard identification and Risk Assessment has been completed for public protection to demonstrate that hazards and risk can be managed and mitigated in an efficient and controlled manner. The hazard identification/risk assessment is conducted throughout the design and has the following process:

1. Identifies key hazards associated with pedestrian and traffic activities;
2. Assesses the likelihood and consequences of the hazard to give a risk score;
3. Provide recommendations for risk control measures following the hierarchy of controls; and
4. Re-assess the likelihood and consequences of the hazard under the proposed control measures to give an updated risk score.

The risk assessment matrix, likelihood and consequence descriptions are shown in Figures 20-22 respectively.

RISK ASSESSMENT MATIRX					
LIKELIHOOD	CONSEQUENCES				
	1 Insignificant	2 Minor	3 Moderate	4 Major	5 Catastrophic
5 Almost Certain	5 Low (+)	10 Medium (+)	15 High	20 Very High	25 Extreme
4 Likely	4 Low (-)	8 Medium (-)	12 Medium (+)	16 High	20 Very High
3 Moderate	3 Negligible	6 Low (+)	9 Medium (-)	12 Medium (+)	15 High
2 Unlikely	2 Negligible	4 Low (-)	6 Low (+)	8 Medium (-)	10 Medium (+)
1 Rare	1 Negligible	2 Negligible	3 Negligible	4 Low (-)	5 Low (+)

Figure 20: Risk Assessment Matrix

LIKELIHOOD	
Almost Certain	The event is expected to occur in most circumstances
Likely	The event will probably occur in most circumstances
Moderate	The event should occur at some time
Unlikely	The event could occur at some time
Rare	The event may occur only in exceptional circumstances

Figure 21: Likelihood Description

CONSEQUENCES	
Catastrophic	Death; very large financial loss
Major	Extensive injuries requiring hospitalisation; major financial loss
Moderate	Medical treatment required; high financial loss
Minor	First aid treatment; some financial loss
Insignificant	No injuries; no financial loss

Figure 22: Consequences Description

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Hazards and Risks	Risk Assessment			Recommended Risk Control Measures	Residual Risk Assessment			Responsible for monitoring and reviewing this activity to ensure safety and compliance
	Likelihood	Consequence	Risk Rating		Residual Likelihood	Residual Consequence	Residual Risk Rating	
Interaction of construction vehicles and traffic at Bayshore Drive	3	2	Medium	Prestart with all drivers to detail safe route and protocol for entry and exit of site. Develop VMP and discuss in pre start meeting. Provide adequate signage to warn other road users of construction nworks	2	2	Low	Site Manager
Trucks accessing site interacting with pedestrians	2	5	Medium	The closure of footpaths and roads, will require the employment of traffic controllers (e.g., stop/go person) or the preparation of a traffic management plan in accordance with relevant standards.	1	5	Low	Site Manager
Partial road closure causing delays to road users	2	2	low	Obtain approvals from road authority to implement traffic management. Council to advertise project to warn road users Install VMS boards prior to works to warn road users	1	2	Negligible	Site Manager
Individuals entering construction site resulting in injuries and site damage.	2	4	Medium	"Construction Site - No Un-authorised entry" signs and fencing will be used to clearly identify areas on and around the perimeter of the sites as being a place of work.	1	4	Low	Site manager/Foreman

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## 8 Environmental Control Measures

### 8.1 Dust Mitigation

To mitigate the impact of dust, on-site watering of equipment will be on hand to dampen any dust activities. A project specific Construction Environmental Management Plan has been compiled by ENV, this plan will be implemented. Monitoring of dust will include visual inspection of the quantity of dust on surrounding vegetation and particles suspended within the air. This shall provide an indication of the effectiveness of dust control measures. The results will be documented in a monthly report, including inspections, non-conformances detected and corrective actions.

In the case of an non-conformances (e.g., a complaint from residents or if the site manager or relevant authorities believes there is excessive dust being generated), the following actions will be taken:

- Installation of dust deposition gauges to quantify dust movement/generation on the property boundary of the closest resident downwind of the development site.

The values obtained from the dust deposition gauge will be compared with the National Environment Protection Council (NEPC) standards shown in Table 4.

**Table 4: NEPC Standards**

Pollutant	Averaging Period	Maximum Concentration
PM10	1-day	50 µg/m <sup>3</sup>
PM10	1-year	25 µg/m <sup>3</sup>
PM2.5	1-day	25 µg/m <sup>3</sup>
PM2.5	1-year	8 µg/m <sup>3</sup>

Should dust onsite exceed NEPC standards, immediate actions shall be taken to mitigate the quantity of dust being generated. This will be achieved by the proposed mitigation strategies below:

**Table 5: Air Quality Management Actions**

Hazard	Control	Timing	Responsibility
Dust	In winds exceeding 35km/hr, works shall cease unless further dust controls are implemented and approved by the superintendent.	Construction	Project Manager/Environmental Officer
Dust	Water carts shall be used regularly to dampen stockpiles, stripped areas and other exposed surfaces.	Construction	Project Manager/Supervisor/Environmental Officer
Dust	The site access is to ensure it is designed for the traffic volume and appropriately stabilised with gravel.	Construction	Project Manager/Environmental Officer
Dust	Loads are to be securely covered on both internal and public roads whereby	Construction	Supervisor/Environmental officer



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	there is a risk of release.		
Dust	Erosion and Sediment controls are to be maintained to ensure devices do not increase dust generation.	Construction	Supervisor/Environmental officer
Dust	Disturbed areas are to be stabilised as soon as practical to minimised exposure earth.	Construction	Supervisor/Environmental officer
Dust	Excavated topsoil shall be isolated for reuse during the restoration process.	Construction	Supervisor/Environmental Officer
Dust	Excavated material shall be stockpiled in an appropriate area.	Construction	Supervisor/Environmental Officer
Dust	Contaminated soil shall be disposed of at an approved facility.	Construction	Project Manager/Environmental Officer
Dust	Roads/haul roads shall be clearly defined and marked out to reduce the area of traffic loading.	Construction	Supervisor/Environmental Officer
Dust	All vehicles entering/egressing the site shall use the designated stabilised access location.	Construction	Supervisor/Environmental Officer
Dust	Transportation of soil material onto pavement surfaces is to be cleaned and removed.	Construction	Supervisor/Environmental Officer
Dust	Material Spilled from trucks onto any roads is to be removed and cleaned immediately.	Construction	Project Manager/Environmental Officer
Dust	Loads are to be securely covered on both internal and public roads whereby there is a risk of release.	Construction	Project Manager/Environmental Officer
Dust Complaints	Ensure public concerns can be heard by making contact numbers available.	Construction	Project Manager/Environmental Officer



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C O N S U L T I N G

Dust Complaints	Make appropriate corrective action when a complaint is made.	Construction	Project Manager/Environmental Officer
Air Quality	Prepare monitoring program and report on observations, non-conformances, and corrective actions.	Pre-construction/construction	Project Manager/Environmental Officer
Fumes	No burning or incinerating of waste to take place on-site.	Construction	Supervisor/Environmental Officer
Fumes	Construction Plants, Vehicles, and equipment shall be well maintained, ensuring an efficient exhaust system.	Construction	Supervisor

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## 9 Monitoring and Evaluation

### 9.1 Inspections and Monitoring

In addition to traffic control safety inspections, formal and documented daily and weekly inspections shall be undertaken by individuals holding the prepare Work Zone Traffic Management Plan qualification (i.e., Site supervisor and work site traffic manager).

The work site traffic control manager will ensure the TMP is implemented and evaluated for effectiveness. The traffic control manager shall inspect and monitor traffic movements around the site in conjunction with the personnel who have erected the safety control measures.

As a minimum the following inspections are required:

- (1) Prior to construction activities onsite;
- (2) During construction works; and
- (3) Closing the site at the end of work.

A daily checklist of the inspections should be kept indicating:

- When traffic control signage and measures were visually checked;
- When changes to controls occurred and why the changes were undertaken;
- Any significant incidents or observations associated with the traffic controls and their impacts on road users or adjacent properties.

It should be noted any incidents (e.g., 'Close calls') must be recorded and documented. This shall be reviewed as part of any inspection and action on immediately.

### 9.2 What the Works Supervisor Shall Do

For all long-term work sites, the works supervisor who is appropriately qualified shall:

- Inspect the traffic control layout on the day before the works begin and at least once per week during the duration of the work;
- Inspect the traffic control layout between shifts at least once during the first week and at least once every two months for the duration of work;
- Review the reported 'close calls', 'near miss incidents', and any incidents around the subject site;
- Provide after-hours contact to local police for the duration of the work period;
- Inspect the site on the final day to ensure that un-necessary signs and devices are removed;
- Record results of these inspections noting date, time, deficiencies and any corrective action taken or specified; and
- Ensure that any specified corrective action is taken.

### 9.3 What the Team Leader Shall Do

For all works, the team leader (or site supervisor) shall:

- Keep a record of the TCP that was implemented;
- Have a copy of the TCP used on site;
- Record start and finish times and location of the works;
- Record near misses;
- Carry out inspections before work starts, during the works and pre-closedown of the site using the nominated checklist, noting:
  - Date and time of inspection;
  - Deficiencies identified and corrective action taken; and
  - Changes or modifications made to the site.
- Periodically check that all signs and devices are satisfactory and in their correct position; and

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- Make these records readily available to authorised staff.

### 9.4 What the Project Manager Shall Do

The project manager shall:

- Ensure that a traffic control safety inspection is carried out at least once per month by a person qualified in Prepare Work Zone Traffic Management Plans and that the date, time and deficiencies are recorded;
- Ensure that a Traffic Control safety inspection or road safety audit is carried out prior to the implementation of any changes in traffic control or a TCP;
- Ensure that near miss incidents are being reported and recorded then reviewed; and
- Ensure that any corrective action specified is taken and recorded.

### 9.5 Legal Procedures

In the case of accidents, either witnessed or reported, involving the public or from which legal proceedings might arise, the following is to be recorded:

- Actual type, size, and location of signs;
- Devices in use at the time of the accident should be recorded and the sign arrangement photographed for subsequent reporting;
- The actual travelled path width and condition;
- Weather conditions should also be recorded;
- Personal injury;
- Extent of vehicle damage; and
- Vehicle details (e.g., rego).

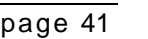


## Appendix A

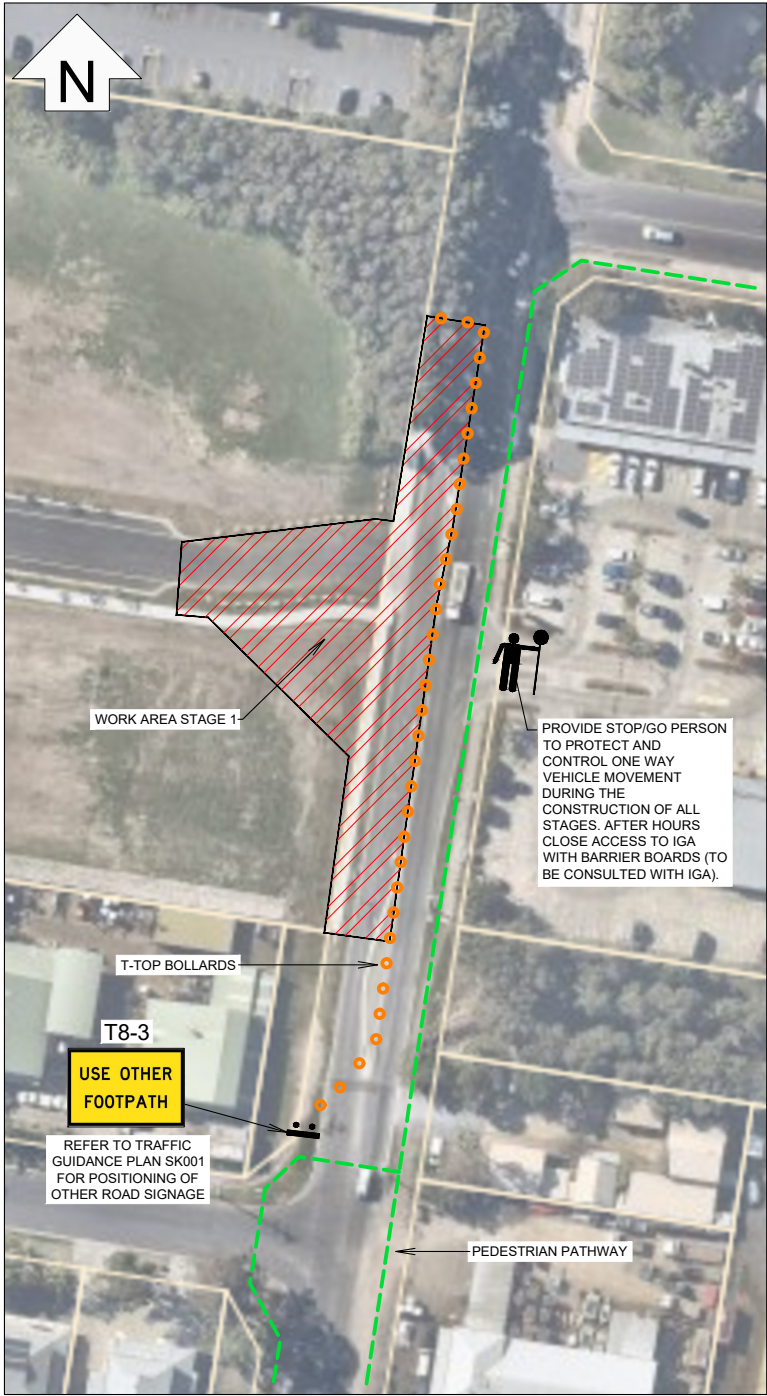
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Traffic Guidance Scheme (TGS) and Public Safety Management Plan (PSMP)

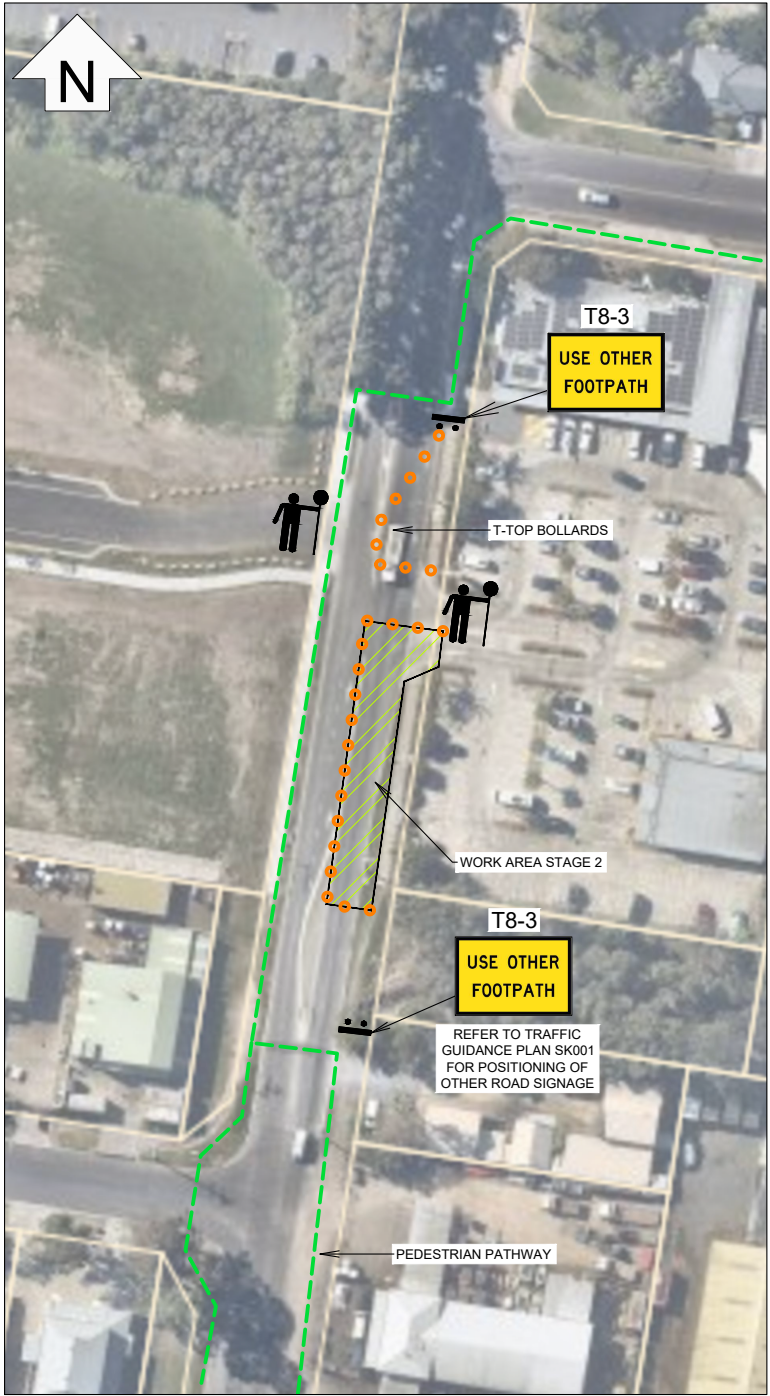




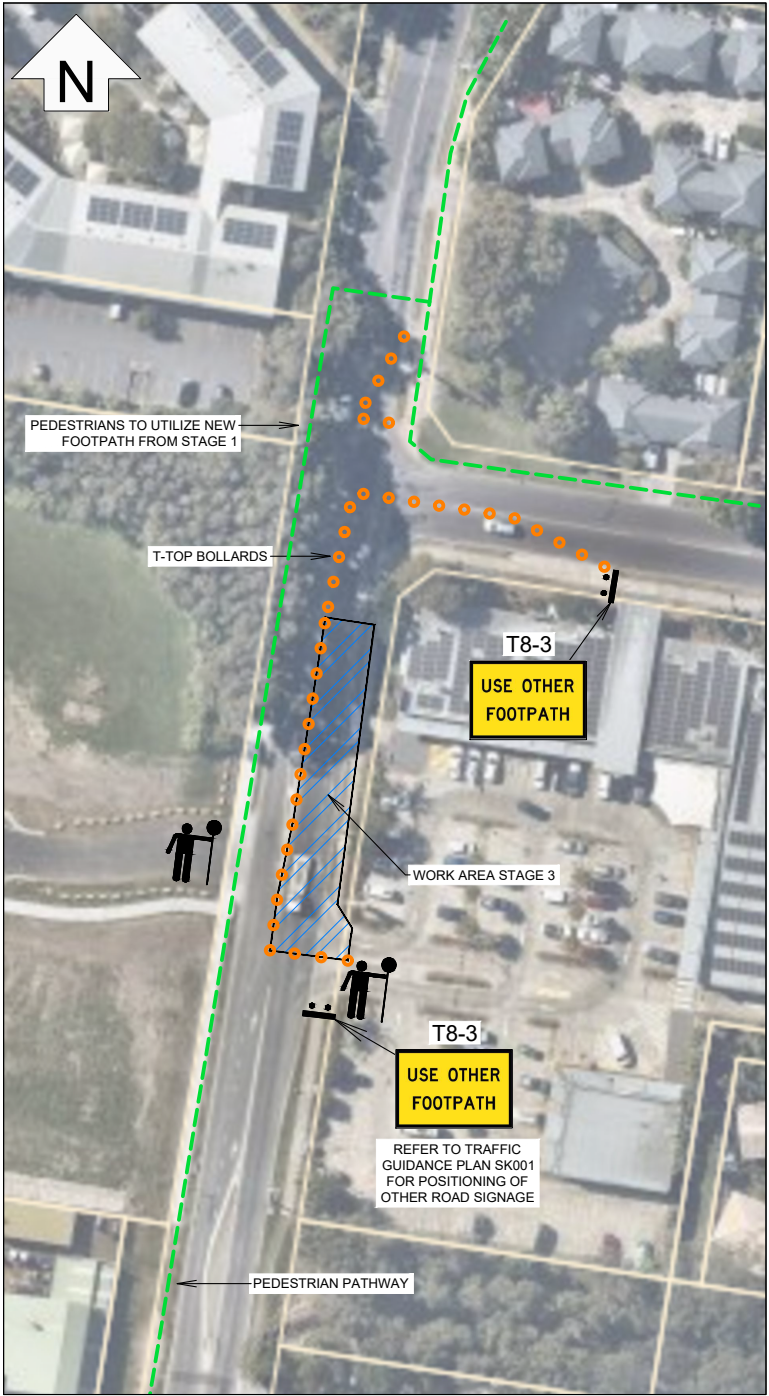




PEDESTRIAN MANAGEMENT PLAN STAGE 1  
AND 2



PEDESTRIAN MANAGEMENT PLAN STAGE 3



PEDESTRIAN MANAGEMENT PLAN STAGE 4

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A	ISSUED FOR COUNCIL APPROVAL	27/02/24	DM	DM	CG	CG							



## **Report No. 6.2      Northern Beaches Illegal Camping**

**File No:** I2024/1215

5 Council have received community complaints in relation to illegal camping occurring near the beach areas in South Golden Beach, New Brighton and Ocean Shores.

10 At the northern end of the shire illegal camping enforcement is practically non-existent due to lack of signage. Because of this, these areas do not get patrolled often unless Council receives complaints. However, as parking restrictions are increasing in Byron Bay and Brunswick Heads, these quieter areas will become more popular with campers, particularly as it gets warmer. Introducing parking signage prior to the busy holiday season should help prevent further community complaints.

### **1. Park Street, New Brighton**

15 Installation of 4 “No parking 1-6am” signs with arrows along the western side of Park Street (beside the soccer fields) as shown in Figure 1 below and as summarised in the table below. The sign layout shown in Figure 1 is indicative and dependant on actual site conditions encountered during installation.



<b>Location (see fig.1)</b>	<b>Code</b>	<b>Description</b>
<b>A</b>	R5-41R	No Parking (symbolic) 1am-6am (right arrow)
<b>B</b>	R5-41L&R	No Parking (symbolic) 1am-6am (double arrow)
<b>C</b>	R5-41L&R	No Parking (symbolic) 1am-6am (double arrow)
<b>D</b>	R5-41L	No Parking (symbolic) 1am-6am (left arrow)



Figure 1: Parking signage locations on Park Street, New Brighton

### 2. The Esplanade, New Brighton

- 5 Installation of 4 “No parking 1-6am” signs with arrows along the western side of The Esplanade, New Brighton as shown in Figure 2 below and as summarised in the table below. The eastern side is No Stopping. The sign layout shown in Figure 2 is indicative and dependant on actual site conditions encountered during installation.



Location (see fig.2)	Code	Description
A	R5-41R	No Parking (symbolic) 1am-6am (right arrow)
B	R5-41L&R	No Parking (symbolic) 1am-6am (double arrow)
C	R5-41L&R	No Parking (symbolic) 1am-6am (double arrow)

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D	R5-41L	No Parking (symbolic) 1am-6am (left arrow)
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Figure 2: Parking signage locations on The Esplanade, New Brighton

### 3. Pacific Esplanade, South Golden Beach

- 5 Installation of 5 “No parking 1-6am” signs with arrows along the eastern side of Pacific Esplanade, South Golden Beach as shown in Figure 3 below and as summarised in the table below. The sign layout shown in Figure 3 is indicative and dependant on actual site conditions encountered during installation.





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Location (see fig.3)	Code	Description
A	R5-41R	No Parking (symbolic) 1am-6am (right arrow)
B	R5-41L&R	No Parking (symbolic) 1am-6am (double arrow)
C	R5-41L&R	No Parking (symbolic) 1am-6am (double arrow)
D	R5-41L&R	No Parking (symbolic) 1am-6am (double arrow)
E	R5-41L	No Parking (symbolic) 1am-6am (left arrow)



Figure 3: Parking signage locations on Pacific Esplanade, South Golden Beach

4. Helen Street, South Golden Beach
5. Installation of 4 “No parking 1-6am” signs with arrows in the carpark between the beach and skatepark on Helen Street, South Golden Beach as shown in Figure 4 below and as summarised in the table below. The sign layout shown in Figure 4 is indicative and dependant on actual site conditions encountered during installation.



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Location (see fig.4)	Code	Description
A	R5-41R	No Parking (symbolic) 1am-6am (right arrow)
B	R5-41L	No Parking (symbolic) 1am-6am (left arrow)
C	R5-41R	No Parking (symbolic) 1am-6am (right arrow)
D	R5-41L	No Parking (symbolic) 1am-6am (left arrow)



Figure 4: Parking signage locations on Helen Street, South Golden Beach

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### 5. Shara Boulevard, Ocean Shores

Installation of 2 “No parking area 1-6am” signs with arrows at the entry points to the carpark beside the soccer fields on Shara Boulevard, Ocean Shores as shown in Figure 5

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below and as summarised in the table below. The sign layout shown in Figure 5 is indicative and dependant on actual site conditions encountered during installation.



Location (see fig.5)	Code	Description
A	R5-81A	No Parking (symbolic) Area 1am-6am
B	R5-81A	No Parking (symbolic) Area 1am-6am



5 Figure 5: Parking signage locations on Shara Boulevard, Ocean Shores

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### RECOMMENDATION:

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**That the Local Traffic Committee support installation of the following signage:**

1. **“No Parking 1am – 6am” signs on the western side of Park Street, New Brighton;**
- 5 2. **“No Parking 1am – 6am” signs on the western side of the The Esplanade, New Brighton;**
3. **“No Parking 1am – 6am” signs on the eastern side of the Pacific Esplanade, South Golden Beach;**
4. **“No Parking 1am – 6am” signs in the skate park carpark on Helen Street, South Golden Beach;**
- 10 5. **“No Parking Area 1am – 6am” signs at the entry to the soccer club carpark on Shara Boulevard, Ocean Shores.**