

# Notice of Meeting

## Local Traffic Committee Meeting

A Local Traffic Committee Meeting of Byron Shire Council will be held as follows:

|       |  |
|-------|--|
| Venue | Conference Room, Station Street, Mullumbimby |
| Date  | Tuesday, 19 November 2024                    |
| Time  | 9.00am                                       |

Phil Holloway  
Director Infrastructure Services

I2024/1541  
Distributed 12/11/24

**BYRON SHIRE COUNCIL**  
LOCAL TRAFFIC COMMITTEE MEETING

**BUSINESS OF MEETING**

**1. APOLOGIES**

**2. DECLARATIONS OF INTEREST – PECUNIARY AND NON-PECUNIARY**

**3. ADOPTION OF MINUTES FROM PREVIOUS MEETINGS**

3.1 Local Traffic Committee Meeting held on 15 October 2024

**4. MATTERS ARISING**

**5. OUTSTANDING ISSUES/RESOLUTIONS**

**6. REGULATORY MATTERS**

- 6.1 119-123 Jonson Street, Byron Bay - new line marking and signage..... 3
- 6.2 Slessor Lane, Brunswick Heads - No Stopping area ..... 50
- 6.3 Mullumbimby Road Upgrade ..... 54
- 6.4 Lighthouse Road - Shared path - 'Get NSW Active' Grant Application ..... 63
- 6.5 Byron Street shared path – Bangalow - 'Get NSW Active' Grant Application ... 77

**7. FOR INFORMATION ONLY**

- 7.1 Brunswick Heads Pay Parking - Notification of Completion ..... 92



- across the full frontage of the site, extending across the full frontage of 125 Jonson St and the intersection of Middleton Lane) and Kingsley St (including any driveway upgrading to the existing driveway of 14 Kingsley St;
- retaining walls;
- upgrade existing driveways;
- electrical infrastructure in accordance with service providers requirement
- streetlighting in accordance with AS1158;
- underground drainage;
- other services utilities upgrading requirement; and
- linemarking & signage.

**b) Roadworks in Kingsley St**

- (i) Roadworks including the design and construction of on-street parking generally in accordance with the approved plans;
- (ii) Footpath design to meet part D4.2.13 of Chapter D4 of BDCP 2014;
- (iii) Ramps in accordance with Council's standard drawing R-04;
- (iv) Redundant driveway to be removed;
- (v) Walkway and ramps gradient to meet the accessible requirement in AS1428 and must transition with the existing footpath levels with neighbouring sites in Kingsley St and Jonson St;
- (vi) Crossfall of 1 % or 1:100 (maximum 2.5% or 1 in 40);
- (vii) Redesign the 45° metered parking to 90° metered parking to accommodate a minimum of 11 parking spaces including 1 accessible parking;
- (viii) Parking design in accordance with Table 3.3 of AS2890.5;
- (ix) Accessible parking design to address all parts of clause 4.5 of AS2890.5;
- (x) Stormwater drainage upgrade;
- (xi) Electricity & streetlighting in accordance with AS1158; and
- (xii) Linemarking and signage including markings to guide traffic in accordance with Figure 2.1 typical end clearances of AS2890.5.

**c) Roadworks in Jonson St**

- (i) Roadworks generally in accordance with the approved plans;
- (ii) Footpath design to meet part D4.2.13 of Chapter D4 of BDCP 2014;
- (iii) Ramps in accordance with Council's standard drawing R-04;
- (iv) Walkway and ramps gradient to meet the accessible requirement in AS1428 and must transition with the existing footpath levels with neighbouring sites in Jonson St;
- (v) Crossfall of 1 % or 1:100 (maximum 2.5% or 1 in 40);
- (vi) Redundant driveways to be removed and replace kerb & gutter to match the existing;
- (vii) Re-linemark the 45° metered parking bays in accordance with the approved plans;
- (viii) Parking design in accordance with Table 3.3 of AS2890.5;

# BYRON SHIRE COUNCIL

## LOCAL TRAFFIC COMMITTEE MEETING

6.1

- (ix) Accessible parking design to address all parts of clause 4.5 of AS2890.5;
- (x) Linemarking and signage including markings to guide traffic in accordance with Figure 2.1 typical end clearances of AS2890.5.

### **d) Local Traffic Committee requirements**





All works associated with the provisions of regulatory signage and linemarking are subject to Local Traffic Committee Recommendation.

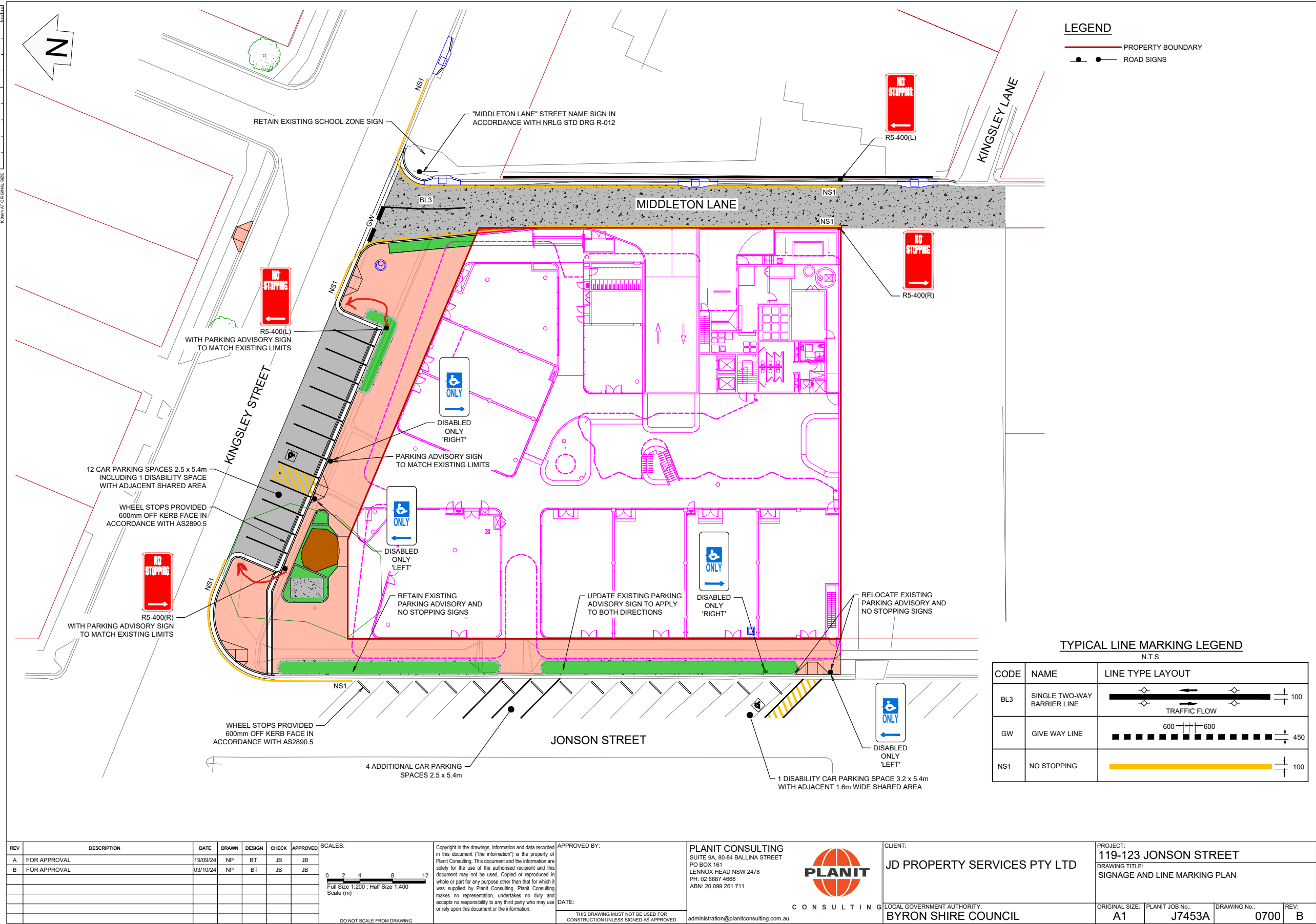
---

### **RECOMMENDATION:**

- 5 **That Council support the new signage, line markings and traffic control devices associated with the 119-123 Jonson St, Byron Bay development as shown in Attachment 1 (E2024/121173)**

### **Attachments:**

- 10    1    51.2023.2325.1 10.2023.235.1 LTC submission, E2024/121173 , page 6  
- 2    51.2023.2325.1 10.2023.235.1 PSMP & TMP submission, E2024/121172 , page 7  





# **Construction Traffic Management Plan and Public Safety Management Plan**

119-123 Jonson Street, Byron Bay

Prepared for Cobild  
By Planit Consulting Pty Ltd

Rev C - September 2024  
Job No: J7453A

---

Construction Traffic Management Plan and Public Safety Management Plan  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



#### Company Details

|                        |  |
|------------------------|--|
| <b>Name</b>            | Planit Consulting Pty Ltd  |
| <b>ABN</b>             | 20 099 261 711   |
| <b>Address</b>         | Suite 9A, 80-84 Ballina Street, Lennox Head NSW 2478   |
| <b>Mailing Address</b> | PO Box 161, Lennox Head NSW 2478   |
| <b>Telephone</b>       | (02) 6687 4666   |
| <b>Email</b>           | <a href="mailto:administration@planitconsulting.com.au">administration@planitconsulting.com.au</a> |
| <b>Website</b>         | <a href="http://www.planitconsulting.com.au">www.planitconsulting.com.au</a>                       |

#### Document Control

|                         |                                      |
|-------------------------|--------------------------------------|
| <b>Document</b>         | J7453A-119-123 Jonson St-CTMP01-RevC |
| <b>Project Name</b>     | 119-123 Jonson Street, Byron Bay     |
| <b>Client</b>           | Cobild                               |
| <b>Planit Reference</b> | J7453A                               |
| <b>Revision Number</b>  | Rev C                                |

#### Revision History

| Revision | Date       | Prepared By | Reviewed By | Approved By |
|----------|------------|-------------|-------------|-------------|
| V1       | 30/05/2024 | AP          | JB          | CW          |
| V2       | 4/06/2024  | AP          | JB          | CW          |
| V2       | 19/09/2024 | JB          | JB          | CW          |

#### Approval Details

|                    |                |
|--------------------|----------------|
| <b>Approved By</b> | Chaej Wrencher |
| <b>Signature</b>   |                |

**Construction Traffic Management Plan and Public Safety Management Plan**  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



---

### **Disclaimer**

The information within this document is and shall remain the property of Planit Consulting Pty Ltd ("Planit"), including drawings, plans and figures.

This document must be read as a whole and cannot be read or reproduced except in its entirety. The document supersedes all previous draft or interim documents, whether written or presented orally, before the date of this report. Any subsequent reports must be read in conjunction with this document.

This document has been prepared for the sole use of our client, Cobild, for the particular brief and on the terms and conditions agreed. It may not be used or relied on (in whole or part) by anyone else, or for any other purpose or context without prior written agreement from Planit.

No unauthorised third party is entitled to use or rely on this document whatsoever. Planit accept no liability if any of the advice is used or relied on by the Client for any unauthorised purpose or by any unauthorised third party.

Construction Traffic Management Plan and Public Safety Management Plan  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



## Contents

|  |           |
|--|-----------|
| <b>Document Control.....</b>   | <b>2</b>  |
| <b>Contents.....</b>   | <b>4</b>  |
| <b>1 Project Background.....</b>   | <b>6</b>  |
| 1.1 Site Description.....  | 6         |
| 1.2 Conditions of Consent – Condition 22, 23 and 24.....   | 7         |
| 1.3 Existing Conditions.....   | 8         |
| 1.3.1 Objectives.....  | 8         |
| 1.4 Subject Site Surrounding Infrastructure .....  | 8         |
| 1.4.1 Jonson Street.....   | 8         |
| 1.4.2 Public Parking.....  | 8         |
| 1.4.3 Pedestrian and Cyclist Facilities.....   | 8         |
| <b>2 Public Protection Controls .....</b>  | <b>9</b>  |
| 2.1 Public Protection Controls.....  | 9         |
| 2.1.1 Cyclists and Pedestrians .....   | 9         |
| 2.1.2 Existing Signage.....  | 9         |
| 2.1.3 Public Notification.....   | 10        |
| 2.1.4 Emergency Arrangements.....  | 10        |
| 2.1.5 Proximity to the existing public school and Fire Station.....  | 10        |
| 2.1.6 Traffic Uniformity with the adjacent development: 116-118 Jonson Street, Byron Bay Development (Bonobo Development)..... | 10        |
| <b>3 Construction Operations.....</b>  | <b>12</b> |
| 3.1 Site Location and Loading Area.....  | 12        |
| 3.2 Construction Activities.....   | 13        |
| 3.3 Construction Hours .....   | 13        |
| 3.3.1 Hours of Work – Condition 49 Requirements .....  | 13        |
| 3.4 Use of Council Land/Road Reserve to Enable Construction Works, Events or Temporary use.....                                | 13        |
| 3.5 Construction Worker Induction & Safe Work Procedures.....  | 13        |
| 3.5.1 Safe Work Requirements.....  | 13        |
| 3.5.2 Safe Work Method Statements (SWMS).....  | 13        |
| 3.5.3 Truck Driver Code of Conduct.....  | 14        |
| 3.6 Dilapidation Report – Condition 34 Requirements.....   | 14        |
| <b>4 Truck Movements.....</b>  | <b>15</b> |
| 4.1 Construction Vehicle Routes.....   | 15        |
| 4.2 Types of Construction Vehicles .....   | 18        |
| 4.3 Number of Truck Movements.....   | 18        |
| 4.4 Proposed Construction Traffic Impact on Jonson and Kingsley Street.....  | 18        |
| 4.5 Truck Queuing Location .....   | 19        |
| <b>5 Proposed Mobile Crane Setups.....</b>   | <b>20</b> |
| <b>6 Hazard Identification/Risk Assessment .....</b>   | <b>21</b> |
| <b>7 Environmental Control Measures.....</b>   | <b>25</b> |
| 7.1 Noise and Vibration.....   | 25        |
| 7.2 Dust Mitigation.....   | 26        |
| <b>8 Monitoring and Evaluation.....</b>  | <b>28</b> |
| 8.1 Inspections and Monitoring .....   | 28        |
| 8.2 What the Works Supervisor Shall Do.....  | 28        |
| 8.3 What The Leader Should Do.....   | 28        |
| 8.4 What The Project Manager Shall Do .....  | 28        |

Construction Traffic Management Plan and Public Safety Management Plan  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



|          |  |           |
|----------|--|-----------|
| 8.5      | Legal Procedures .....                 | 29        |
| <b>9</b> | <b>Service Bay Occasional Use.....</b> | <b>30</b> |

## Appendices

|  |           |
|--|-----------|
| <b>Appendix A – Traffic Guidance Schemes (TGS) .....</b> | <b>31</b> |
|--|-----------|

## List of Tables

|   |    |
|---|----|
| Table 1: Construction Activities .....            | 13 |
| Table 2: Earthworks and Truck Movements .....     | 18 |
| Table 3: Summary of Truck Movements.....          | 18 |
| Table 4: Risk Assessment.....                     | 22 |
| Table 5: Construction Mitigation Strategies ..... | 25 |
| Table 6: NEPC Standards.....                      | 26 |
| Table 7: Air Quality Management Actions .....     | 26 |

## List of Figures

|   |    |
|---|----|
| Figure 1: Subject Site Location (Source: Nearmaps).....   | 6  |
| Figure 2: Excerpt From Conditions of Consent (Condition 22, 23 & 24) .....                      | 7  |
| Figure 3: Subject Site – Locality Plan.....   | 8  |
| Figure 4: Site Locality of Adjacent Development 'BONOBO' 116-118 Jonson Street, Byron Bay.....  | 11 |
| Figure 5: Construction Site Arrangement.....  | 12 |
| Figure 6: Proposed Construction Vehicle Routes to the Subject Site (Source: Nearmaps).....      | 16 |
| Figure 7: Proposed Construction Vehicle Routes exiting the Subject Site (Source: Nearmaps)..... | 17 |
| Figure 8: Proposed Truck Queuing Location Option (Source: Nearmaps).....                        | 19 |
| Figure 9: Proposed Mobile Crane & Tower Crane Setups.....                                       | 20 |
| Figure 10: Risk Assessment Matrix .....   | 21 |
| Figure 11: Likelihood Description .....   | 21 |
| Figure 12: Consequences Description.....  | 21 |

Construction Traffic Management Plan and Public Safety Management Plan  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



## 1 Project Background

### 1.1 Site Description

This Construction Traffic Management Plan (CTMP) and Public Safety Management Plan (PSMP) has been prepared in accordance with Council's Development application approval (DA No. 10.2023.235.1), detailing the condition of consent that are required to be satisfied before commencement of works and during construction.

Specifically, this report addresses condition 22, 23 and 24 (Figure 2) of the DA Conditions of Consent for 119-123 Jonson Street, Byron Bay, NSW 2481. Refer to Figure 1 for the subject site location and section 1.2 for an extract of the relevant conditions of consent.



Figure 1: Subject Site Location (Source: Nearmaps)

Construction Traffic Management Plan and Public Safety Management Plan  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



### 1.2 Conditions of Consent – Condition 22, 23 and 24

|     |  |
|-----|--|
| 22. | <p><b>Public Safety Management Plan required</b></p> <p>Consent from Council must be obtained for a Public Safety Management Plan for those works within the road reserve pursuant to Section 138 of the Roads Act 1993. This public safety management plan is to include provision for (but not be limited to):</p> <ul style="list-style-type: none"> <li>a) a pedestrian barrier, alternative footpaths and ramps as necessary;</li> <li>b) an awning sufficient to prevent any substance from, or in connection with, the work falling into the road reserve;</li> <li>c) lighting of the alternative footpath between sunset and sunrise;</li> <li>d) the loading and unloading of building materials;</li> <li>e) parking space for tradesman's vehicles, where such vehicles must be located near the site due to tools and equipment contain within the vehicle;</li> <li>f) Removal of any such hoarding, fence or awning as soon as the particular work has been completed.</li> </ul>   |
| 23. | <p><b>Traffic Management Plan</b></p> <p>Consent from Council must be obtained for a Traffic Management Plan pursuant to Section 138 of the Roads Act 1993. The plans and specifications are to include the measures to be employed to control traffic (inclusive of construction vehicles) during construction of the development. The traffic management plan is to be designed in accordance with the requirements of the Roads and Traffic Authority's Manual, Traffic Control at Work Sites Version 2, and the current Australian Standards, Manual of Uniform Traffic Control Devices Part 3, 'Traffic Control Devices for Works on Roads'.</p> <p>The report must incorporate measures to ensure that motorists using road adjacent to the development, residents and pedestrians in the vicinity of the development are subjected to minimal time delays due to construction on the site or adjacent to the site.</p> <p>The traffic management plan must be prepared by a suitably qualified and RTA accredited Work Site Traffic Controller.</p> |
| 24. | <p><b>Traffic Management Plan for Occasional Service of Loading Bay</b></p> <p>Prior to issue of the construction certificate, consent from Council must be obtained for a Traffic Management Plan (TMP) pursuant to Section 138 of the Roads Act 1993. The plans and specifications are to include the measures to be employed to control traffic for occasional service of Loading Bay. The TMP is to be designed in accordance with the requirements of the current version of the Transport for NSW <i>Traffic Control at Work Sites Technical Manual</i>.</p> <p>The plan shall incorporate measures to ensure that motorists using road adjacent to the development, residents and pedestrians in the vicinity of the development are subjected to minimal time delays and safety due to occasional service requirements of the loading bay during business hours of the development to satisfy clause 3.2.2 of AS2890.2.</p> <p>The TMP must be prepared by a suitably qualified Transport for NSW accredited person.</p>                           |

Figure 2: Excerpt From Conditions of Consent (Condition 22, 23 & 24)

Construction Traffic Management Plan and Public Safety Management Plan  
 J7453A - 119-123 Jonson Street, Byron Bay  
 Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



### 1.3 Existing Conditions

#### 1.3.1 Objectives

The purpose of this report is to provide a Concept Traffic Management Plan (CTMP) and Public Safety Management Plan (PSMP) in support of the proposed mixed used development located at 119-123 Jonson Street, Byron Bay, NSW, 2481 (Figure 3).



Figure 3: Subject Site – Locality Plan

### 1.4 Subject Site Surrounding Infrastructure

#### 1.4.1 Jonson Street

Jonson Street is the main distributor road that fronts the subject site. The site posted speed limit is 50km/hr. Jonson Street is a classified (state) regional road and the primary road authority is Byron Shire Council.

#### 1.4.2 Public Parking

There are some formalised parking spaces in the vicinity of the project site and opportunities for parallel kerb side and 45-degree parking. Specifically, restricted paid parallel and unrestricted 45-degree angle paid parking is proposed along Jonson Street in proximity to the subject site.

Currently, the site has 11 x car spaces along Jonson Street adjacent to the western boundary of the subject site. The existing car spaces adjacent to the northern boundary of the subject site in Kingsley Street is inclusively proposed for upgrade.

#### 1.4.3 Pedestrian and Cyclist Facilities

Existing footpaths and bike paths are provided along Jonson Street.

Construction Traffic Management Plan and Public Safety Management Plan  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



## 2 Public Protection Controls

### 2.1 Public Protection Controls

Public protection and exclusion controls are required to be implemented throughout the duration of the project. These include signage informing of any site activity and barriers preventing entry to the site.

To achieve adequate public protection and exclusion zones the following is required as part of the Public Safety Management plan:

- A pedestrian barrier, alternative footpaths and ramps as necessary;
- A 1.8m high temporary fence around the property boundary is to be provided;
- Awnings will be required if works are to be located over the footpath to prevent any materials or substance from or in connection with the works falling into the road reserve;
- Lighting shall be provided on any water filled barriers to provide pedestrians with adequate lighting between sunset and sunrise;
- For loading and unloading of building materials, public safety measures shall be with the Traffic guidance scheme control measures. Refer to the attached Public Safety Management Plan (PSMP) in Appendix A and Traffic Guidance Scheme (TGS) in Appendix B for further details;
- Contractors will park along Jonson, Kingsley Street and internally on the site. All work equipment and tools to be located within the subject site;
- Removal of any hoarding, fence or awning shall take place as soon as the works for which they are required is completed.

#### 2.1.1 Cyclists and Pedestrians

Cyclists and pedestrians currently use this area to access local shops, businesses, Fire and Rescue NSW, Byron Bay Fire Station and the public school surrounding the subject site. Due to the large volume of traffic in the area most pedestrians and cyclists use the footpaths to travel through this area.

To enable safe access around the worksite for pedestrians and cyclist during construction, it is proposed to delineate all works area with safety barriers and 1.8-metre fencing, this shall allow safe pedestrian access around proposed work areas. The fencing will have warning signs erected on it to advise the public to keep out of the works area.

Throughout the various stages of construction, pedestrians will require alternative access points. As construction progresses, pedestrians will be directed to cross streets at designated locations, adhering to standard road rules. It is noted Jonson Street, Byron Bay has numerous pedestrian median refuges to allow for safe crossings.

Traffic control signs are proposed to guide pedestrians safely around the construction site.

#### 2.1.2 Existing Signage

There are numerous signs currently in this area (i.e., advertising signs, posted speed limits, advisory signs, no stopping signs). It is proposed all traffic signs and advance warning signs will be placed by a suitably qualified traffic officer and checked and reported daily. If signs are being moved or removed then permanent fixtures may be required.

**Construction Traffic Management Plan and Public Safety Management Plan**  
 J7453A - 119-123 Jonson Street, Byron Bay  
 Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



### 2.1.3 Public Notification

#### 2.1.3.1 Letter Drop

Occupants of the properties, businesses, school and fire station that surround the works will be notified through a letter drop 2-weeks prior to construction. The letter drop will outline the following as a minimum:

- Proposed works;
- Timeframes;
- Potential delays to the community; and
- Relevant Contact information.

It is imperative for the contractor to keep a record of all communications with the public and to close out any concerns in a reasonable time frame. BSC and Contractor will meet weekly to discuss community communications and safety concerns.

#### 2.1.3.2 Council Website Updates

Council is to be notified of proposed work and council websites to be updated, accordingly.

#### 2.1.3.3 Bus Services

Blanch buses and other bus services are to be notified of proposed traffic activities to ensure no delays in scheduled timeframes.

#### 2.1.3.4 Variable Message Signs (VMS)

A VMS will be set up on Jonson or Kingsley Street to advise road users and the community of the proposed works. The VMS board will be set up 1-2 weeks prior to the proposed start date of the construction works.

The contractor to confirm preferred location with BSC.

### 2.1.4 Emergency Arrangements

Emergency services will have continual access to all businesses and properties hence no specific facilities are required. Furthermore, the Byron Bay Fire Station opposite the subject site at Fletcher Street, Byron Bay will have continual access throughout the duration of the project.

The project supervisor shall assist emergency vehicles requiring entry/exit through the works area.

### 2.1.5 Proximity to the existing public school and Fire Station

The existing public school (i.e., Byron Bay Public) and fire station are adjacent to the subject site, and it is important the site supervisor to provide periodic updates of the proposed works and traffic, public and emergency operations. This will ensure safety and transparency during the construction process of the project.

### 2.1.6 Traffic Uniformity with the adjacent development: 116-118 Jonson Street, Byron Bay Development (Bonobo Development)

It is imperative for the site supervisor to liaise with the Bonobo (116-118 Jonson Street, Byron Bay Development) site supervisor regarding public safety and proposed traffic controls. This will ensure safety and uniformity with traffic and pedestrian safety controls. Refer to the Figure 4 for the location of the Bonobo development.

Construction Traffic Management Plan and Public Safety Management Plan  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



Figure 4: Site Locality of Adjacent Development 'BONOBO' 116-118 Jonson Street, Byron Bay.

It is noted majority of the construction activities for Bonobo development are located at the rear of the subject site along the old railway tracks.

Refer to the attached Public Safety Management Plan (PSMP) in Appendix A for further details.



**PLANIT**  
CONSULTING

### 3.1 Site Location and Loading Area

**Stage 1**

REFER TO PUBLIC SAFETY MANAGEMENT PLAN FOR ADDITIONAL REQUIRED CONTROLS FOR PEDESTRIANS SAFETY WHEN WORKS IN VERGES ARE OCCURRING

INDICATIVE ACCESS LOCATION

TEMPORARY POWER POLE AS PER TEMPORARY POWER SUPPLY PLAN

DEWATERING EQUIPMENT, MACHINERY, AMENITIES, OFFICE, LUNCH ROOM ETC.

W5-22

W5-205

W5-207(L)

W5-207(R)

SUBJECT SITE

TRUCKS TURNING

PROPOSED 1.8m HIGH TEMPORARY FENCE TO BE PROVIDED AROUND PROPERTY BOUNDARY AND SITE COMPOUND AREA. EXACT LOCATION TO BE CONFIRMED BY CONTRACTOR.

**Stage 2**

TEMPORARY POWER POLE AS PER TEMPORARY POWER SUPPLY PLAN

DEWATERING EQUIPMENT, MACHINERY, AMENITIES, OFFICE, LUNCH ROOM ETC.

CRANE

T5-5

SUBJECT SITE

REFER TO PUBLIC SAFETY MANAGEMENT PLAN FOR ADDITIONAL REQUIRED CONTROLS FOR PEDESTRIANS SAFETY WHEN WORKS IN VERGES ARE OCCURRING

PROPOSED 1.8m HIGH TEMPORARY FENCE TO BE PROVIDED AROUND PROPERTY BOUNDARY AND SITE COMPOUND AREA. EXACT LOCATION TO BE CONFIRMED BY CONTRACTOR.

**Stage 3**

INDICATIVE ACCESS LOCATION

TEMPORARY POWER POLE AS PER TEMPORARY POWER SUPPLY PLAN

BARRIER BOARD

MACHINERY, AMENITIES, OFFICE, LUNCH ROOM ETC.

T5-5

T2-4

T5-1R

SUBJECT SITE

REFER TO PUBLIC SAFETY MANAGEMENT PLAN FOR ADDITIONAL REQUIRED CONTROLS FOR PEDESTRIANS SAFETY WHEN WORKS IN VERGES ARE OCCURRING

PROPOSED 1.8m HIGH TEMPORARY FENCE TO BE PROVIDED AROUND PROPERTY BOUNDARY AND SITE COMPOUND AREA. EXACT LOCATION TO BE CONFIRMED BY CONTRACTOR.

SITE FENCE TO BE EXTENDED DURING WORKS IN SOUTHERN END OF MIDDLETON LANE. SAFE RESIDENTIAL DRIVEWAY ACCESS TO REMAIN OPEN AT ALL TIMES.

ROAD CLOSED

DETOUR

ROAD CLOSED

DETOUR

Page 12 of 31

**Construction Traffic Management Plan and Public Safety Management Plan**  
 J7453A - 119-123 Jonson Street, Byron Bay  
 Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



### 3.2 Construction Activities

The proposed typical construction activities for the development are shown in Table 1 below:

Table 1: Construction Activities

| Onsite Works                         | Approx. Duration (Weeks/Months) |
|--------------------------------------|---------------------------------|
| Stage 1 – Retention and Excavation   | 5 Months                        |
| Stage 2 – Structure                  | 7 Months                        |
| Stage 3 – External Works and Fit out | 10 Months                       |

### 3.3 Construction Hours

#### 3.3.1 Hours of Work – Condition 49 Requirements

As per condition 68 (Hours of Work), Construction works for the development, demolition or vegetation removal including delivery of machinery and materials to and from the site will occur between the hours of:

- Monday to Friday 7:00am-6:00pm.
- Saturday 8:00am – 1:00pm; and
- No work on Sundays or Public Holidays.

Where practicable, access and activities should be scheduled to occur outside of the peak periods. Based on traffic data and site inspections, peak period traffic between 8:15am-9:15am in the morning, 3:00pm and 4:30pm in the afternoon, and Weekend Peak 1:00-2:00pm.

It must be ensured by the principal certifier that the building work, demolition, or vegetation removal is not carried out on Sundays and public holidays, except where there is an emergency.

Furthermore – unless approved within the construction site management plan, construction vehicles, machinery, goods, or materials must not be delivered to the site outside the approved hours of site works.

#### 3.4 Use of Council Land/Road Reserve to Enable Construction Works, Events or Temporary use.

During the Construction phases of the project, it is proposed to utilise Council Land/Road reserve to enable construction works. The Project Construction manager will be required to liaise with BSC to use council land/road reserve to enable construction works, events or temporary use.

Construction Manager is to contact council with the proposed area/number of parking bays and duration proposed, and an invoice from council shall be issued for payment. Council will require details of each phase prior to any of these associated works starting. Failure to do so, will lead councils compliance team to take appropriate actions.

### 3.5 Construction Worker Induction & Safe Work Procedures

#### 3.5.1 Safe Work Requirements

To protect the safety of workers and the public, the work site should be adequately secured (i.e., security fence) to prevent access by unauthorized personnel. Additionally, all workers must always be inducted in accordance with the relevant SafeWork requirements.

#### 3.5.2 Safe Work Method Statements (SWMS)

A SWMS should be completed whenever any person undertaking works on or adjacent to the public domain.

**Construction Traffic Management Plan and Public Safety Management Plan**  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



### **3.5.3 Truck Driver Code of Conduct**

Drivers associated with the project are to abide by a code of conduct in order to:

- Minimise impacts of construction on the local road network;
- Minimise conflict with other road users;
- Minimise road traffic noise; and
- Ensure truck drivers used specified and appropriate routes.

### **3.6 Dilapidation Report – Condition 34 Requirements**

As per DA Condition 34 – **Dilapidation Report**, a full dilapidation report will be undertaken:

A second Dilapidation Report must be submitted to Council, prior to the issue of an Occupation Certificate, to ascertain if any structural damage has occurred to any adjoining building, infrastructure, or roads.

Construction Traffic Management Plan and Public Safety Management Plan  
 J7453A - 119-123 Jonson Street, Byron Bay  
 Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



## 4 Truck Movements

### 4.1 Construction Vehicle Routes

It is anticipated that majority of the pre-cast materials shall be delivered/sourced from the south (e.g., Ballina, Lennox Head) and it is expected that majority of the construction trucks will be utilising the M1 Highway as the preferred route to the subject site.

Construction vehicle movements will occur to and from the site through the Byron Bay town centre from Ewingsdale Road and Bangalow Road.

The key construction access and egress routes are as follows:

- Construction traffic travelling on Ewingsdale Road will access the site via the following proposed route:
  - Continue straight from Ewingsdale road onto Shirley Street;
  - Proceed on Shirley Street and continue straight onto Butler Street (Byron Bay bypass);
  - Proceed along Butler Street (Byron Bypass) and turn left on the Jonson/Browning Street;
  - Proceed approximately 250m straight along Jonson Street; and
  - Access the site by turning right into the designated site access area.
- Construction traffic travelling on Bangalow Road will access the site via the following proposed route:
  - Continue Straight on Bangalow Road and turn left onto Browning Street;
  - Proceed along Butler Street/Jonson Street Roundabout and turn right onto Jonson Street;
  - Proceed approximately 250m straight along Jonson Street; and
  - Access the site by turning right into the designated site access area.
- Construction Traffic Travelling on Jonson Street will egress the subject site via the following route:
  - Exit the site by turning right onto Jonson Street and continue to travel Northbound;
  - Turn right onto Marvell Street and continue along Tennyson Street;
  - Continue on Tennyson Street and turn right onto Browning Street; and
  - Either turn left onto Bangalow Road or right onto Butler street.

Refer to Figure 6 (to the site) and 7 (from the site) for the proposed construction vehicle routes for vehicles travelling along Ewingsdale Road and Bangalow Road to and from the subject site and Appendix B for the proposed swept paths.

Construction Traffic Management Plan and Public Safety Management Plan  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



Figure 6: Proposed Construction Vehicle Routes to the Subject Site (Source: Nearmaps)

Construction Traffic Management Plan and Public Safety Management Plan  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)

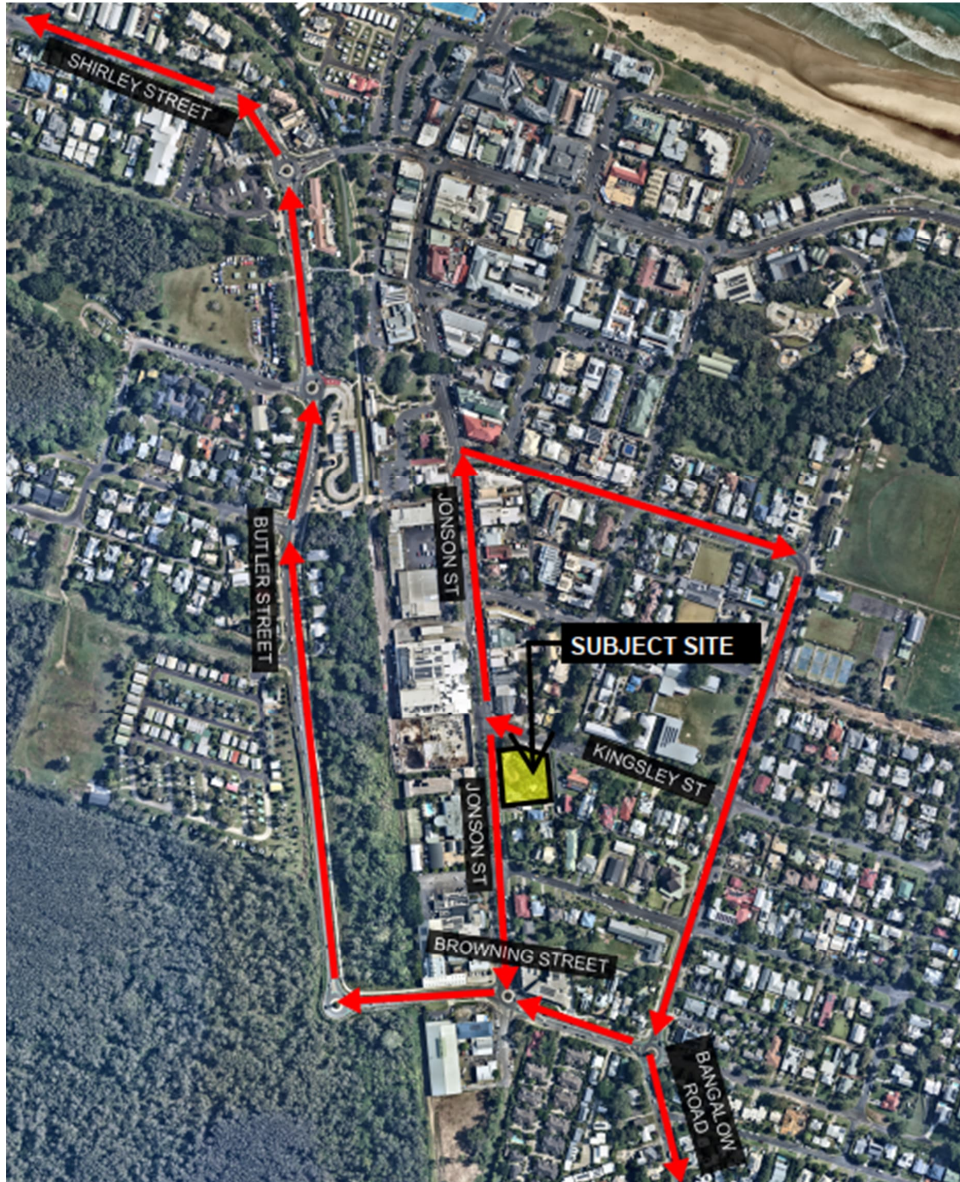


Figure 7: Proposed Construction Vehicle Routes exiting the Subject Site (Source: Nearmaps)  
Construction Vehicle Access to the Site will be primarily from Kingsley Street, Byron Bay.

Construction Traffic Management Plan and Public Safety Management Plan  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



## 4.2 Types of Construction Vehicles

A combination of truck and vehicle types will be used during the construction of the project. Typical vehicle traffic is expected to comprise of the following vehicles:

- Light vehicles for the movement of construction personnel, including contractors, the project labour force and management staff;
- Small and medium vehicles for machinery delivery; and
- Heavy vehicle movements for the delivery and removal of construction machinery and materials, spoil and waste.

The largest vehicles used in terms of length and mass for construction activities will be:

- 16.5m Truck and Dog; and
- Semi-trailers (19m Articulated Vehicle AV).

## 4.3 Number of Truck Movements

The number of truck movements required for cut/fill operations has been assessed based on the estimated bulk earthworks volumes and proposed earthworks construction phases. These truck movements are summarised in Table 2 with the inclusion of an estimate of the number of days required to excavate the site. The estimates below have been prepared on the assumption of excavation of approximately 15m<sup>3</sup> of material per load.

Table 2: Earthworks and Truck Movements

| Earthworks and Truck Movements |                          |                           |                              |   |                               |
|--------------------------------|--------------------------|---------------------------|------------------------------|---|-------------------------------|
| Stage                          | Cut<br>(m <sup>3</sup> ) | Fill<br>(m <sup>3</sup> ) | Balance<br>(m <sup>3</sup> ) | Number of truck loads<br>(15m <sup>3</sup> per truck) | Number of<br>required<br>days |
| <b>Bulk Earthworks</b>         | 12,000                   | -                         | 12,000                       | 800   | 90                            |

## 4.4 Proposed Construction Traffic Impact on Jonson and Kingsley Street

During the construction phase, the maximum traffic volumes expected to occur during the bulk earthwork stage are described in Table 3.

Table 3: Summary of Truck Movements

| Stage                  | Total Truck Movements | Total Duration (Days) | Daily Truck Volumes (Trips/day) | Maximum Allowable Peak Hour Truck Trips |
|------------------------|-----------------------|-----------------------|---------------------------------|---|
| <b>Bulk Earthworks</b> | 800                   | 90                    | 9                               | 6                                       |

As demonstrated in Table 3, during the bulk earthwork phase, the proposal development is expected to generate a maximum of 20 vehicle movements per day and a maximum of 6 vehicle trips per hour in both the AM and PM peak periods. This equates to approximately one (1) vehicle movement every (10) minutes during peak periods.

Trucks entering the site are not to exceed 6 trucks in any 1-hour period. The Project Manager and Supervisor on-site will be responsible for monitoring truck movements and reporting back to Byron Shire Council (BSC). Furthermore, trucks are restricted to one truck on or around the subject site at any given time.

**Construction Traffic Management Plan and Public Safety Management Plan**  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



### 4.5 Truck Queuing Location

During the construction phases, it is proposed that at any given time only one truck shall be located on or around the subject site at a time. Therefore, it is proposed that a truck queuing location is required to mitigate any queuing within the CBD of Byron Bay. It is proposed to utilise the southern portion of the Cavanbah centre carpark when appropriate as a truck queuing location.

Cavanbah Centre is approximately 4-5km away (8-minutes) from the subject and it is anticipated that trucks queuing in the southern end of the Cavanbah centre carpark shall have minimal impact to the surrounding areas.

Refer to Figure 8 for the proposed truck queuing location.



Figure 8: Proposed Truck Queuing Location Option (Source: Nearmaps)

Construction Traffic Management Plan and Public Safety Management Plan  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



## 5 Proposed Mobile Crane Setups

Based on the proposed construction activities, it is proposed that mobile cranes will be used as required for 'special deliveries' only. Figure 9 shows indicative temporary mobile crane setup locations, these mobile crane lifts will occur at scheduled times of the project and are proposed to be utilised for 'special deliveries' only subject to separate TGS.

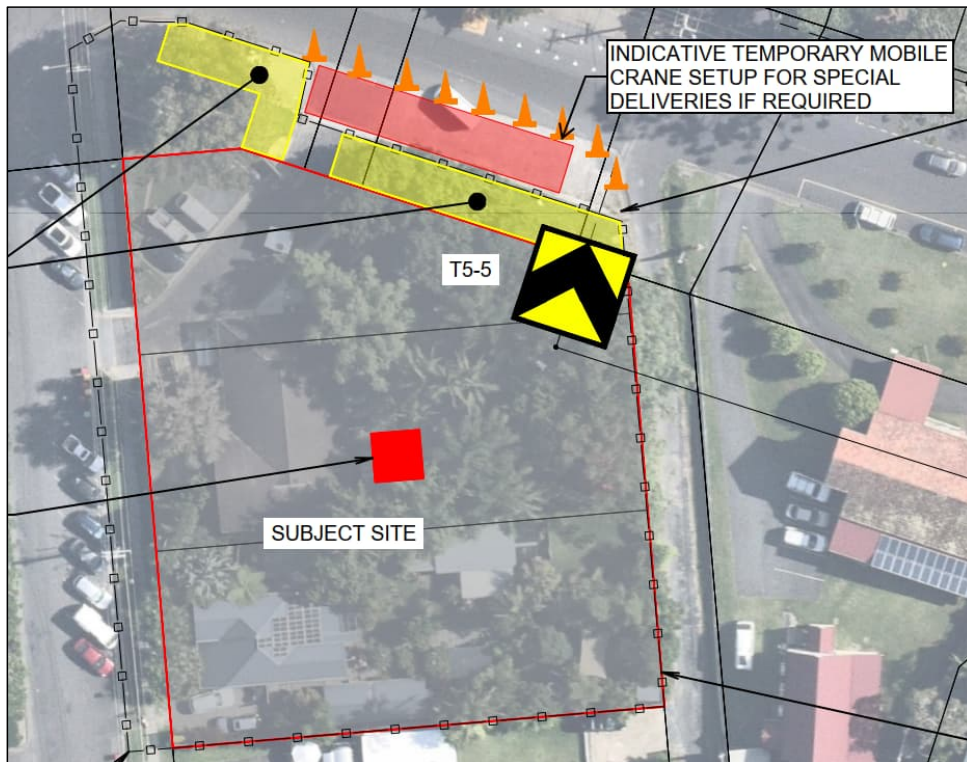


Figure 9: Proposed Mobile Crane & Tower Crane Setups

Construction Traffic Management Plan and Public Safety Management Plan  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



## 6 Hazard Identification/Risk Assessment

A Hazard identification and Risk Assessment has been completed for public protection to demonstrate that hazards and risk can be managed and mitigated in an efficient and controlled manner. The hazard identification/risk assessment is conducted throughout the design and has the following process:

1. Identifies key hazards associated with pedestrian and traffic activities;
2. Assesses the likelihood and consequences of the hazard to give a risk score;
3. Provide recommendations for risk control measures following the hierarchy of controls; and
4. Re-assess the likelihood and consequences of the hazard under the proposed control measures to give an updated risk score.

The risk assessment matrix, likelihood and consequence descriptions are shown in Figures 10-12 respectively.

| RISK ASSESSMENT MATIRX |                 |               |               |               |                |
|------------------------|-----------------|---------------|---------------|---------------|----------------|
| LIKELIHOOD             | CONSEQUENCES    |               |               |               |                |
|                        | 1 Insignificant | 2 Minor       | 3 Moderate    | 4 Major       | 5 Catastrophic |
| 5 Almost Certain       | 5 Low (+)       | 10 Medium (+) | 15 High       | 20 Very High  | 25 Extreme     |
| 4 Likely               | 4 Low (-)       | 8 Medium (-)  | 12 Medium (+) | 16 High       | 20 Very High   |
| 3 Moderate             | 3 Negligible    | 6 Low (+)     | 9 Medium (-)  | 12 Medium (+) | 15 High        |
| 2 Unlikely             | 2 Negligible    | 4 Low (-)     | 6 Low (+)     | 8 Medium (-)  | 10 Medium (+)  |
| 1 Rare                 | 1 Negligible    | 2 Negligible  | 3 Negligible  | 4 Low (-)     | 5 Low (+)      |

Figure 10: Risk Assessment Matrix

| LIKELIHOOD     |   |
|----------------|---|
| Almost Certain | The event is expected to occur in most circumstances  |
| Likely         | The event will probably occur in most circumstances   |
| Moderate       | The event should occur at some time                   |
| Unlikely       | The event could occur at some time                    |
| Rare           | The event may occur only in exceptional circumstances |

Figure 11: Likelihood Description

| CONSEQUENCES  |  |
|---------------|--|
| Catastrophic  | Death; very large financial loss                                   |
| Major         | Extensive injuries requiring hospitalisation; major financial loss |
| Moderate      | Medical treatment required; high financial loss                    |
| Minor         | First aid treatment; some financial loss                           |
| Insignificant | No injuries; no financial loss                                     |

Figure 12: Consequences Description

Refer to Table 4 Below for the risk assessment prepared.

**Construction Traffic Management Plan and Public Safety Management Plan**  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



Table 4: Risk Assessment

| Hazards and Risks  | Risk Assessment |             |             | Recommended Risk Control Measures   | Residual Risk Assessment |                      |                      | Responsible for monitoring and reviewing this activity to ensure safety and compliance |
|--|-----------------|-------------|-------------|---|--------------------------|----------------------|----------------------|--|
|  | Likelihood      | Consequence | Risk Rating |   | Residual Likelihood      | Residual Consequence | Residual Risk Rating |  |
| Truck entering the site and not seeing pedestrians resulting in a collision. | 5               | 5           | Extreme     | The closure of footpaths and roads, will require the employment of traffic controllers (e.g., stop/go person) or the preparation of a traffic management plan in accordance with relevant standards.  | 2                        | 5                    | Medium               | Site Manager   |
| Falling Objects landing on public places.                                    | 3               | 5           | High        | When work is carried out at a height exceeding 4m above the lowest ground level of the adjoining public place the following will be implemented: <ul style="list-style-type: none"> <li>A hoarding at least 1800mm high that is fully sheeted with timber; Plywood, metal, or sturdy synthetic sheets, and one of the following, a gantry (or overhead protective structure), (incorporating the hoarding), or the adjoining area is closed to prevent and mitigate objects falling on or otherwise hitting members of the public in that vicinity, or a catch platform with vertical sheeting or perimeter containment sheeting is installed.</li> </ul> | 2                        | 5                    | Medium               | Site Manager   |

Construction Traffic Management Plan and Public Safety Management Plan  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



| Hazards and Risks  | Risk Assessment |             |             | Recommended Risk Control Measures   | Residual Risk Assessment |                      |                      | Responsible for monitoring and reviewing this activity to ensure safety and compliance |
|--|-----------------|-------------|-------------|---|--------------------------|----------------------|----------------------|--|
|  | Likelihood      | Consequence | Risk Rating |   | Residual Likelihood      | Residual Consequence | Residual Risk Rating |  |
| Slips, trips and falls by members of the public  | 3               | 4           | Medium      | Public areas will be kept clear of any debris, trip or slip hazards and protected from any projectiles, dust etc. that may be released as a product of work undertaken  | 2                        | 3                    | Medium               | Site Manager   |
| Trucks entering the site unannounced and traffic control not ready causing unsafe maneuvering around the site. | 3               | 4           | Medium      | <p>When the driver of vehicles, trucks, and plant turn up unannounced to site and are required to reverse into the site across public footpaths and walkways the following is to be implemented:</p> <ul style="list-style-type: none"> <li>Erect a sign at the entry gates 'directing drivers to contact the site manager/foreman before entering the site - provide contact details;</li> <li>Email suppliers and sub-contractors to advise them of the health and safety protocol to be followed for plant and vehicle movements onto the site;</li> <li>Where there are members of the public using the footpath or walkways provide a stop/go person or a spotter or mechanical aids to control their movements during the reversing process; and</li> <li>The reversing vehicle, plant and trucks from the street to be controlled as per the traffic management plan for the project.</li> </ul> | 2                        | 4                    | Medium               | Site Manager   |

**Construction Traffic Management Plan and Public Safety Management Plan**  
 J7453A - 119-123 Jonson Street, Byron Bay  
 Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



| Hazards and Risks   | Risk Assessment |             |             | Recommended Risk Control Measures   | Residual Risk Assessment |                      |                      | Responsible for monitoring and reviewing this activity to ensure safety and compliance |
|---|-----------------|-------------|-------------|---|--------------------------|----------------------|----------------------|--|
|   | Likelihood      | Consequence | Risk Rating |   | Residual Likelihood      | Residual Consequence | Residual Risk Rating |  |
| Partial road closure causing delays to the public.                                | 2               | 2           | low         | Written approval will be obtained from relevant authority to close adjoining areas when excluding members of the public. Any controls imposed by the authority will be fully implemented and monitored during the project | 2                        | 1                    | Negligible           | Site Manager   |
| Individuals entering the construction site resulting in injuries and site damage. | 2               | 4           | Medium      | "Construction Site - No Un-authorized entry" signs and fencing will be used to clearly identify areas on and around the perimeter of the sites as being a place of work.  | 2                        | 4                    | Medium               | Site manager/Foreman   |

Construction Traffic Management Plan and Public Safety Management Plan  
 J7453A - 119-123 Jonson Street, Byron Bay  
 Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



## 7 Environmental Control Measures

### 7.1 Noise and Vibration

Throughout the duration of the proposed construction stages, all practicable efforts shall be made to mitigate the noise and vibration generated from the construction activities. A project specific noise and vibration management plan has been prepared by ENV. All noise and vibration measures noted within this plan will be implemented on the project and will include mitigation measures to satisfy the following DA conditions:

- DA Condition 33: Noise and Vibration Management Plan;
- DA Condition 10: Environmental Management Plan;
- DA Condition 68: Construction Times; and
- DA Condition 69: Construction Noise.

Performance Criteria:

- Construction works are undertaken within scheduled work hours in accordance with the EPA and BSC guidelines; and
- No noise complaints from project related works to be lodged to the contractor or relevant authorities.

Refer to Table 5 for the proposed management/mitigation strategies:

Table 5: Construction Mitigation Strategies

| Hazard              | Control   | Timing                        | Responsibility                        |
|---------------------|---|-------------------------------|---------------------------------------|
| Noise Pollution     | <b>Normal Construction hours:</b> <ul style="list-style-type: none"> <li>Monday to Friday 7:00am-6:00pm</li> <li>Saturday 8:00am – 1:00pm</li> <li>No work on Sundays or public Holidays</li> </ul> | Construction                  | Project Manager                       |
| Noise Pollution     | Demolition and excavation works are to be restricted to only operate within hours from 8:00am – 5:00pm Monday to Friday.  | Construction                  | Project Manager                       |
| Noise Pollution     | All personnel will be informed during inductions of the potential negative affect noise pollution could have on local residents and the surrounding area.   | Construction                  | Supervisor/Environmental Officer      |
| Noise Pollution     | Residents in the general vicinity will be informed of the proposed works and consultation with affected landowners will be maintained throughout the construction period.                           | Pre-construction/Construction | Project Manager/Environmental Officer |
| Noise Pollution     | During Construction hours, any proposed noise generating construction activity shall not result in the emission of offensive noise.   | Construction                  | Project Manager/Environmental Officer |
| Excessive Vibration | Vibration from works is to be undertaken in accordance with industry best practice, to ensure excessive levels of vibration do not occur.   | Construction                  | Project Manager/Environmental         |

**Construction Traffic Management Plan and Public Safety Management Plan**  
 J7453A - 119-123 Jonson Street, Byron Bay  
 Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



## 7.2 Dust Mitigation

To mitigate the impact of dust, on-site watering of equipment will be on hand to dampen any dust activities. A project specific Construction Environmental Management Plan has been compiled by ENV, this plan will be implemented. Monitoring of dust will include visual inspection of the quantity of dust on surrounding vegetation and particles suspended within the air. This shall provide an indication of the effectiveness of dust control measures. The results will be documented in a monthly report, including inspections, non-conformances detected and corrective actions.

In the case of an non-conformances (e.g., a complaint from residents or if the site manager or relevant authorities believes there is excessive dust being generated), dust deposition gauges are required to be installed to quantify dust movement/generation on the property boundary of the closest resident downwind of the development site.

The values obtained from the dust deposition gauge will be compared with the National Environment Protection Council (NEPC) standards shown in Table 6.

Table 6: NEPC Standards

| Pollutant | Averaging Period | Maximum Concentration |
|-----------|------------------|-----------------------|
| PM10      | 1-day            | 50 µg/m <sup>3</sup>  |
| PM10      | 1-year           | 25 µg/m <sup>3</sup>  |
| PM2.5     | 1-day            | 25 µg/m <sup>3</sup>  |
| PM2.5     | 1-year           | 8 µg/m <sup>3</sup>   |

Should dust onsite exceed NEPC standards, immediate actions shall be taken to mitigate the quantity of dust being generated. This will be achieved by the proposed mitigation strategies shown in Table 7.

Table 7: Air Quality Management Actions

| Hazard | Control  | Timing       | Responsibility   |
|--------|--|--------------|--|
| Dust   | In winds exceeding 35km/hr, works shall cease unless further dust controls are implemented and approved by the superintendent. | Construction | Project Manager/<br>Environmental<br>Officer               |
| Dust   | Water carts shall be used regularly to dampen stockpiles, stripped areas and other exposed surfaces.                           | Construction | Project<br>Manager/Supervisor/<br>Environmental<br>Officer |
| Dust   | The site access is to ensure it is designed for the traffic volume and appropriately stabilised with gravel.                   | Construction | Project Manager/<br>Environmental<br>Officer               |
| Dust   | Loads are to be securely covered on both internal and public roads whereby there is a risk of release.                         | Construction | Supervisor/<br>Environmental<br>officer                    |
| Dust   | Erosion and Sediment controls are to be maintained to ensure devices do not increase dust generation.                          | Construction | Supervisor/<br>Environmental<br>officer                    |
| Dust   | Disturbed areas are to be stabilised as soon as practical to minimised exposure earth.   | Construction | Supervisor/<br>Environmental<br>officer                    |
| Dust   | Excavated topsoil shall be isolated for reuse during the restoration process.  | Construction | Supervisor/<br>Environmental<br>Officer                    |
| Dust   | Excavated material shall be stockpiled in an appropriate area.   | Construction | Supervisor/<br>Environmental<br>Officer                    |
| Dust   | Contaminated soil shall be disposed of at an approved facility.  | Construction | Project Manager/<br>Environmental<br>Officer               |

**Construction Traffic Management Plan and Public Safety Management Plan**  
 J7453A - 119-123 Jonson Street, Byron Bay  
 Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



| Hazard             | Control  | Timing                                | Responsibility                               |
|--------------------|--|---------------------------------------|--|
| Dust               | Roads/haul roads shall be clearly defined and marked out to reduce the area of traffic loading.              | Construction                          | Supervisor/<br>Environmental<br>Officer      |
| Dust               | All vehicles entering/egressing the site shall use the designated stabilised access location.                | Construction                          | Supervisor/<br>Environmental<br>Officer      |
| Dust               | Transportation of soil material onto pavement surfaces is to be cleaned and removed.                         | Construction                          | Supervisor/<br>Environmental<br>Officer      |
| Dust               | Material Spilled from trucks onto any roads is to be removed and cleaned immediately.                        | Construction                          | Project Manager/<br>Environmental<br>Officer |
| Dust               | Loads are to be securely covered on both internal and public roads whereby there is a risk of release.       | Construction                          | Project Manager/<br>Environmental<br>Officer |
| Dust<br>Complaints | Ensure public concerns can be heard by making contact numbers available.                                     | Construction                          | Project Manager/<br>Environmental<br>Officer |
| Dust<br>Complaints | Make appropriate corrective action when a complaint is made.   | Construction                          | Project Manager/<br>Environmental<br>Officer |
| Air Quality        | Prepare monitoring program and report on observations, non-conformances, and corrective actions.             | Pre-<br>construction<br>/construction | Project Manager/<br>Environmental<br>Officer |
| Fumes              | No burning or incinerating of waste to take place on-site.   | Construction                          | Supervisor/<br>Environmental<br>Officer      |
| Fumes              | Construction Plants, Vehicles, and equipment shall be well maintained, ensuring an efficient exhaust system. | Construction                          | Supervisor                                   |

Construction Traffic Management Plan and Public Safety Management Plan  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



---

## 8 Monitoring and Evaluation

---

### 8.1 Inspections and Monitoring

In addition to traffic control safety Inspections, formal and documented daily and weekly inspections shall be undertaken at work sites by individuals holding the prepare Work Zone Traffic Management Plan qualification.

It should be noted any incidents (e.g., 'Close calls') must be recorded and documented. This shall be reviewed as part of any inspection and action on immediately.

### 8.2 What the Works Supervisor Shall Do

For all long-term work sites, the works supervisor who is appropriately qualified shall:

- Inspect the traffic control layout on the day before the works begin and at least once per week during the duration of the work;
- Inspect the traffic control layout between shifts at least once during the first week and at least once every two months for the duration of work;
- Review the reported 'close calls', 'near miss incidents', and any incidents around the subject site;
- Provide after-hours contact to local police for the duration of the work period;
- Inspect the site on the final day to ensure that un-necessary signs and devices are removed;
- Record results of these inspections noting date, time, deficiencies and any corrective action taken or specified; and
- Ensure that any specified corrective action is taken.

### 8.3 What The Leader Should Do

For all works, the team leader (or site supervisor) shall:

- Keep a record of the TCP that was implemented;
- Have a copy of the TCP used on site;
- Record start and finish times and location of the works;
- Record near misses;
- Carry out inspections before work starts, during the works and pre-closedown of the site using the nominated checklist, noting:
  - Date and time of inspection;
  - Deficiencies identified and corrective action taken; and
  - Changes or modifications made to the site.
- Periodically check that all signs and devices are satisfactory and in their correct position; and
- Make these records readily available to authorised staff.

### 8.4 What The Project Manager Shall Do

The project manager shall:

- Ensure that a traffic control safety inspection is carried out at least once per month by a person qualified in Prepare Work Zone Traffic Management Plans and that the date, time and deficiencies are recorded;
- Ensure that a Traffic Control safety inspection or road safety audit is carried out prior to the implementation of any changes in traffic control or a TCP;
- Ensure that near miss incidents are being reported and recorded then reviewed; and

**Construction Traffic Management Plan and Public Safety Management Plan**  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



- 
- Ensure that any corrective action specified is taken and recorded.

### **8.5 Legal Procedures**

In the case of accidents, either witnessed or reported, involving the public or from which legal proceedings might arise, the following is to be recorded:

- Actual type, size, and location of signs;
- Devices in use at the time of the accident should be recorded and the sign arrangement photographed for subsequent reporting;
- The actual travelled path width and condition;
- Weather conditions should also be recorded;
- Personal injury;
- Extent of vehicle damage; and
- Vehicle details (e.g., rego).

**Construction Traffic Management Plan and Public Safety Management Plan**  
J7453A - 119-123 Jonson Street, Byron Bay  
Cobild  
[www.planitconsulting.com.au](http://www.planitconsulting.com.au)



---

## **9 Service Bay Occasional Use**

---

Once the project reaches its operation phase, the service bay is proposed to be used for both waste collection and the loading/unloading of goods.

To minimise the risk of clashes between waste collection and the unloading/loading of goods, scheduling shall be implemented organising unloading/loading of good outside typical waste collection hours where possible.

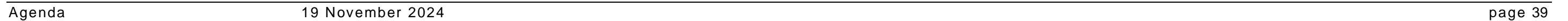
As these activities will be occurring onsite, traffic control measures should not be required during these operations. In addition, commentary from Solo waste has been obtained stating that waste collection from the Jonson Ln development (is currently in operation and is a similar size and has similar waste collection requirements), has not had any issues with waste collection.

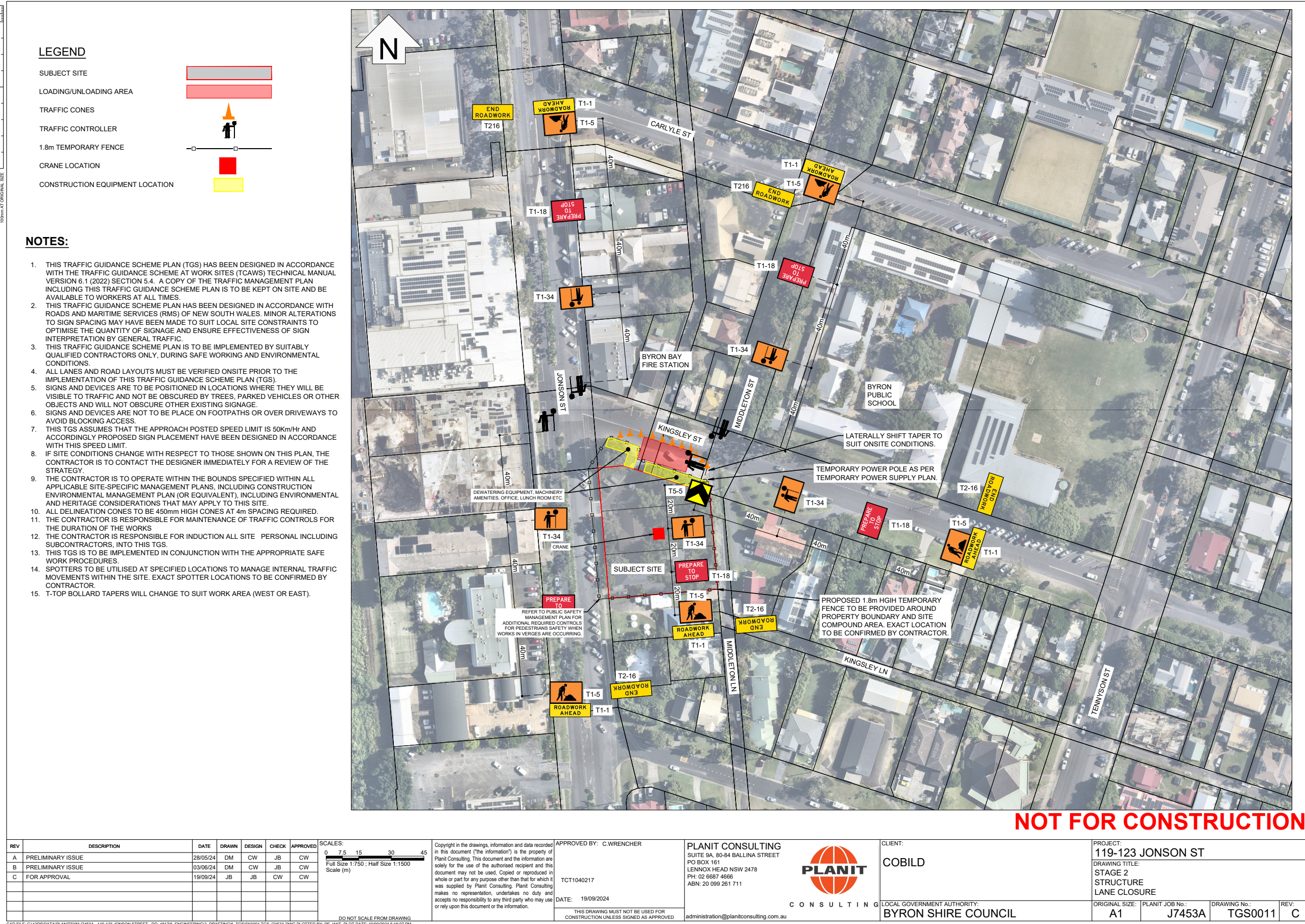


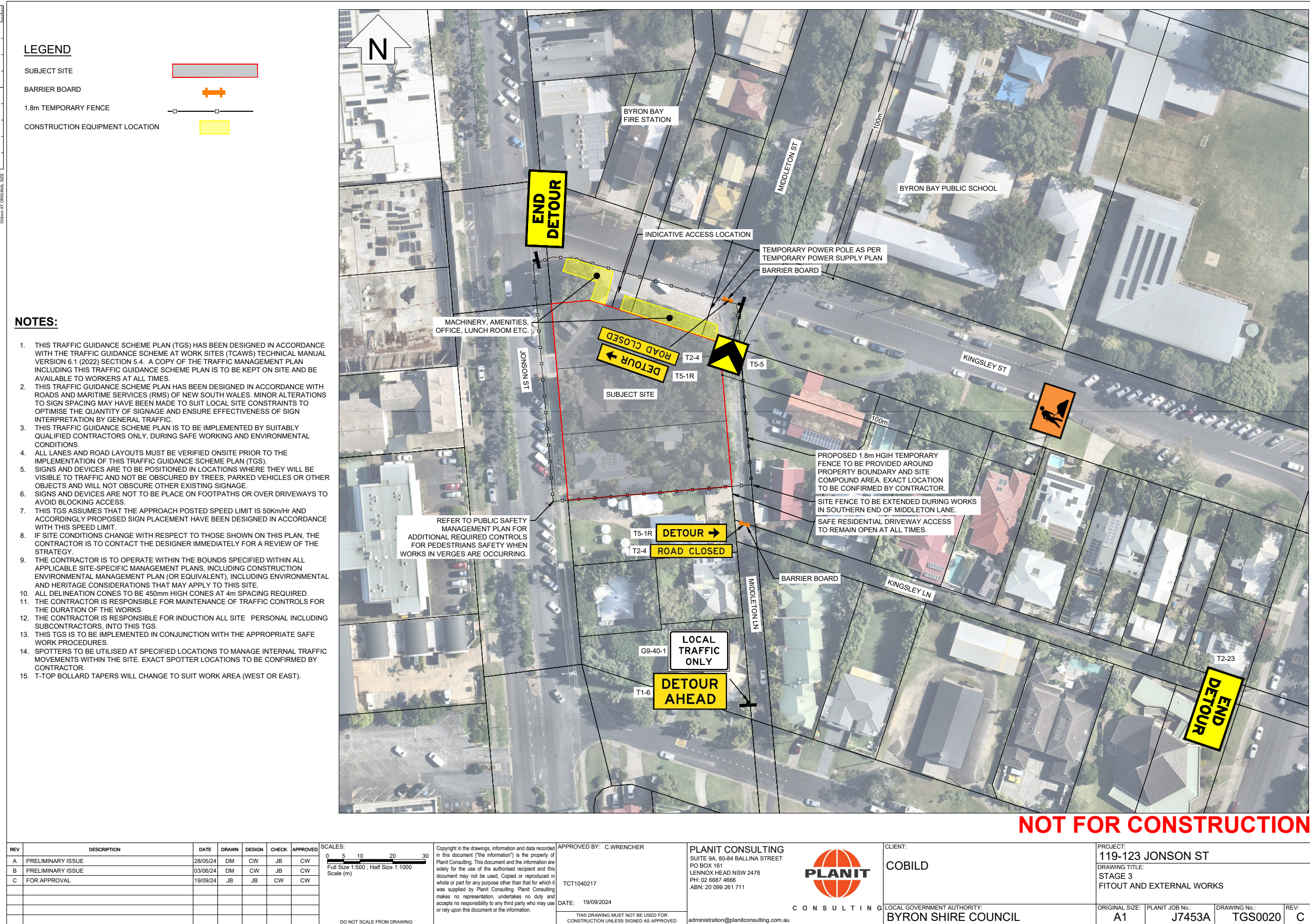
# Appendix A – Traffic Guidance Schemes (TGS)

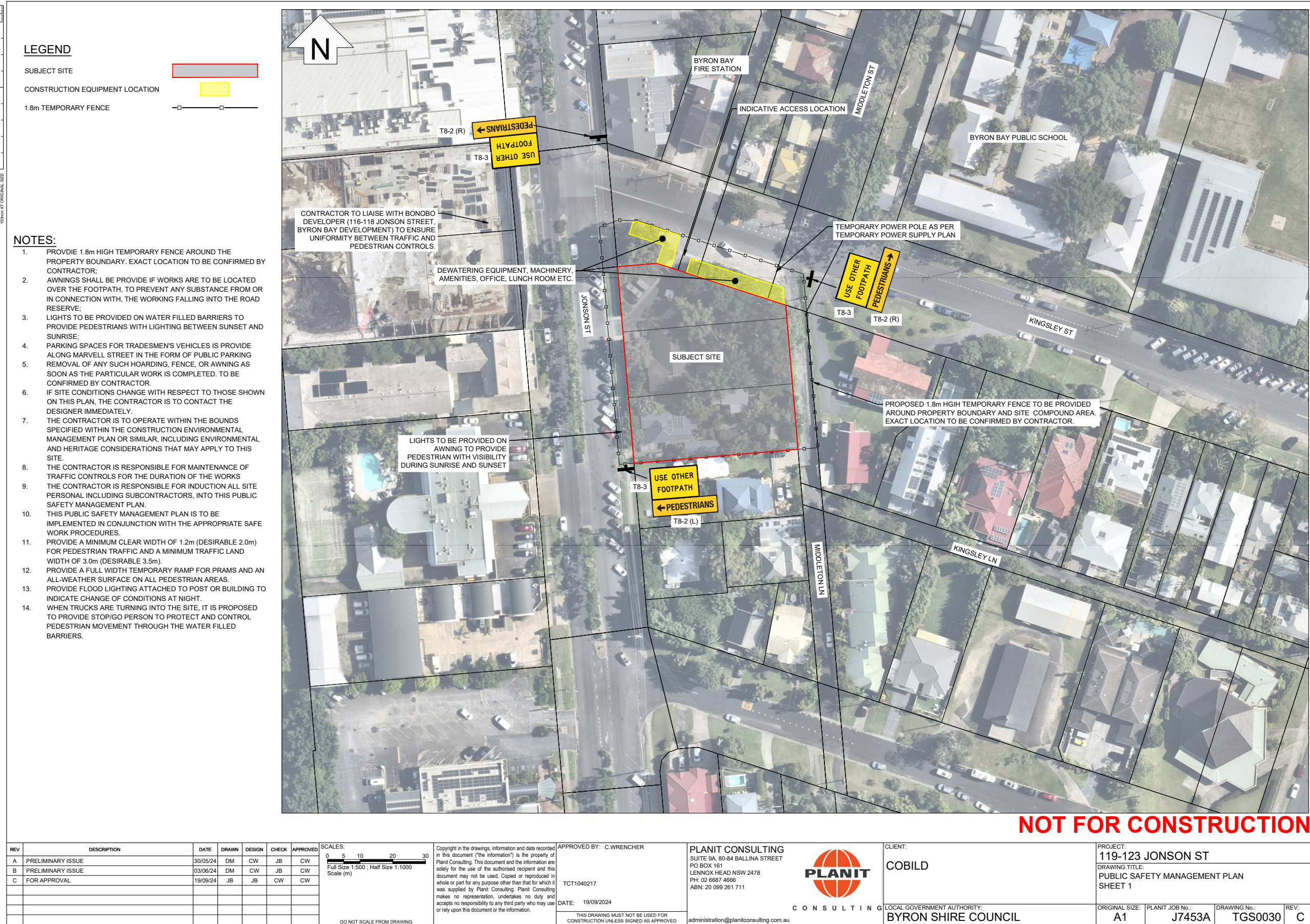
Page 31 of 31

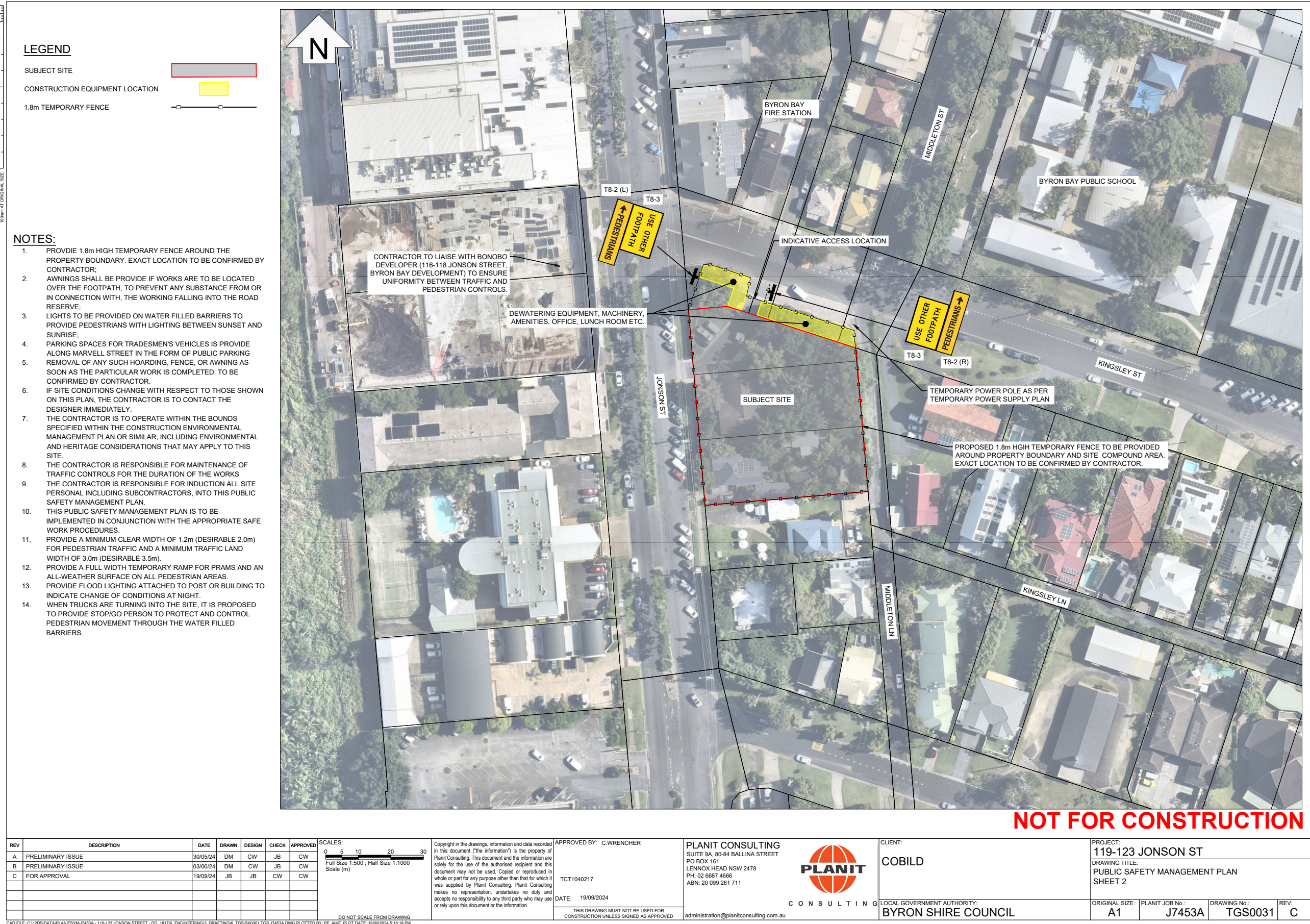


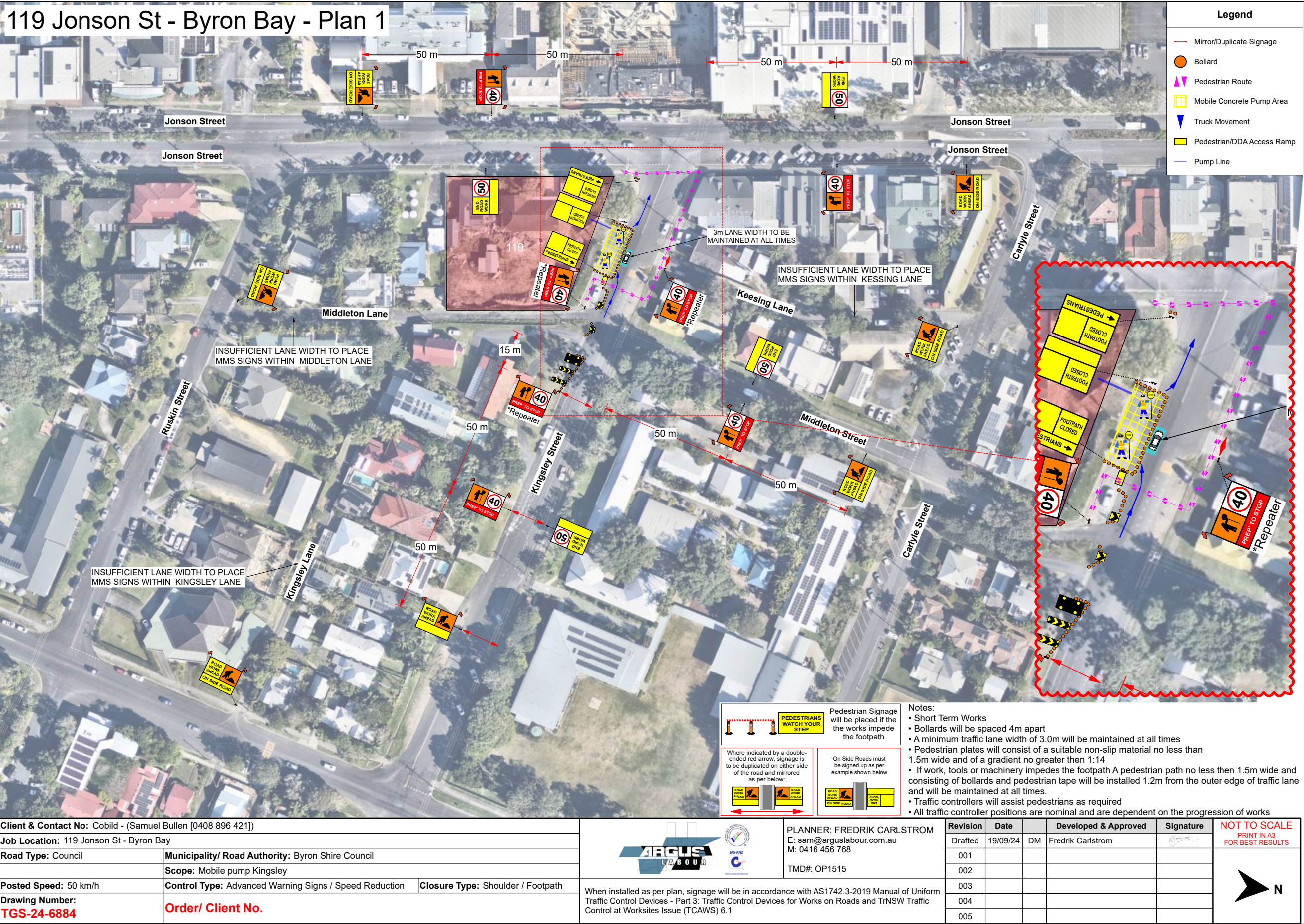




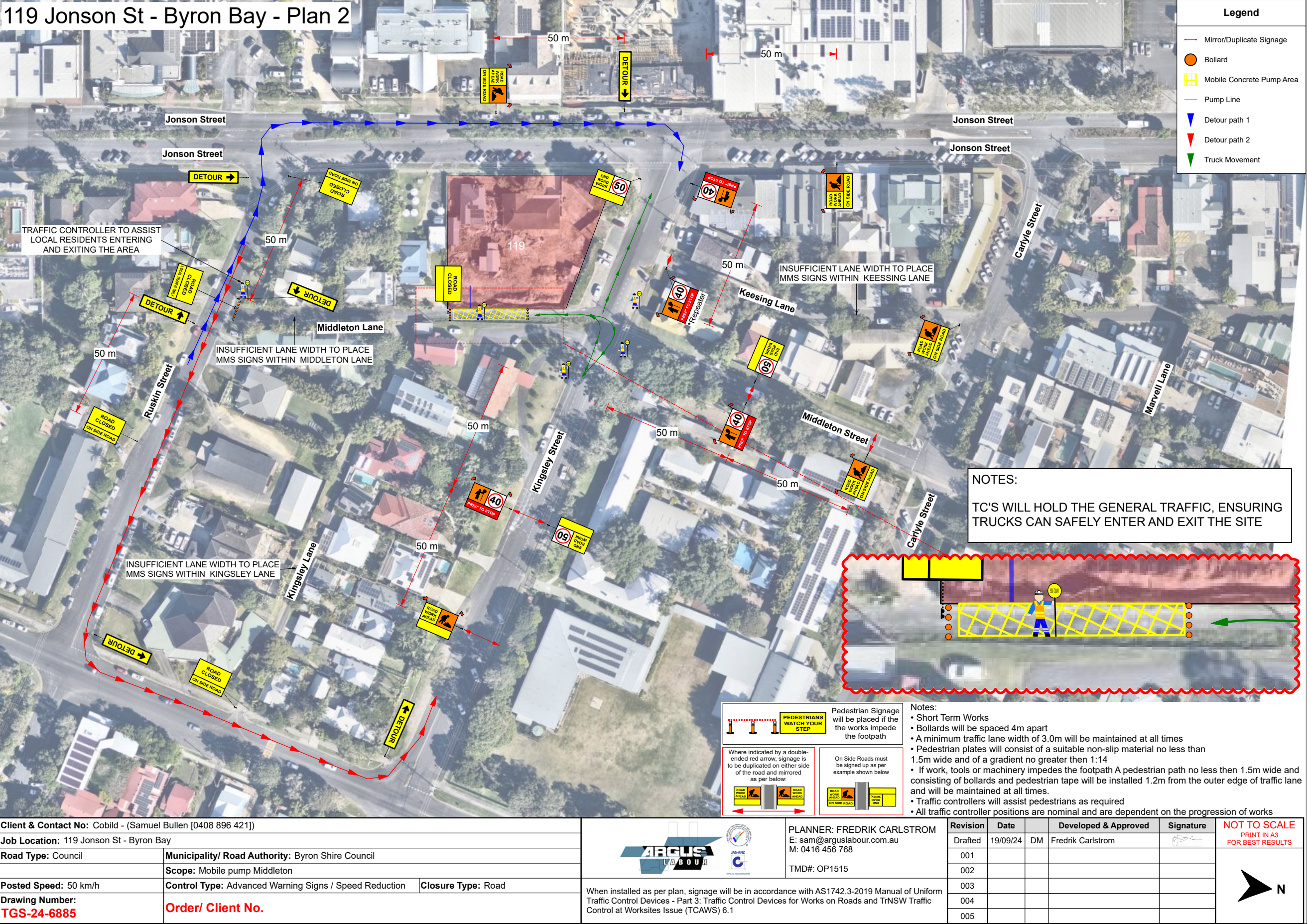










119 Jonson St - Byron Bay - Plan 2



|  |  |                    |
|--|--|--------------------|
| Client & Contact No: Cobild - (Samuel Bullen [0408 896 421]) |  |                    |
| Job Location: 119 Jonson St - Byron Bay                      |  |                    |
| Road Type: Council   | Municipality/ Road Authority: Byron Shire Council      |                    |
|  | Scope: Mobile pump Middleton                           |                    |
| Posted Speed: 50 km/h  | Control Type: Advanced Warning Signs / Speed Reduction | Closure Type: Road |
| Drawing Number: TGS-24-6885                                  | Order/ Client No.                                      |                    |

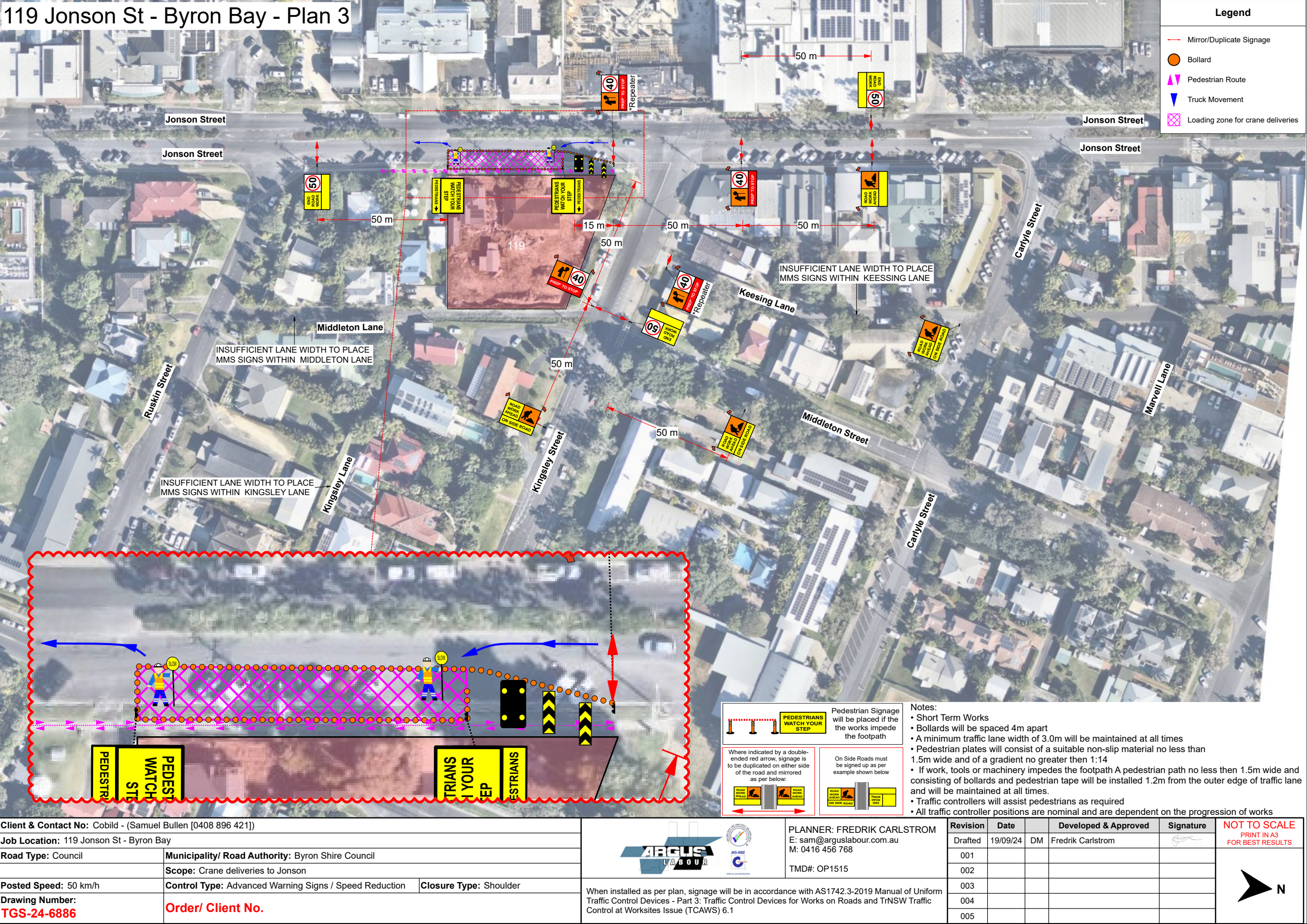


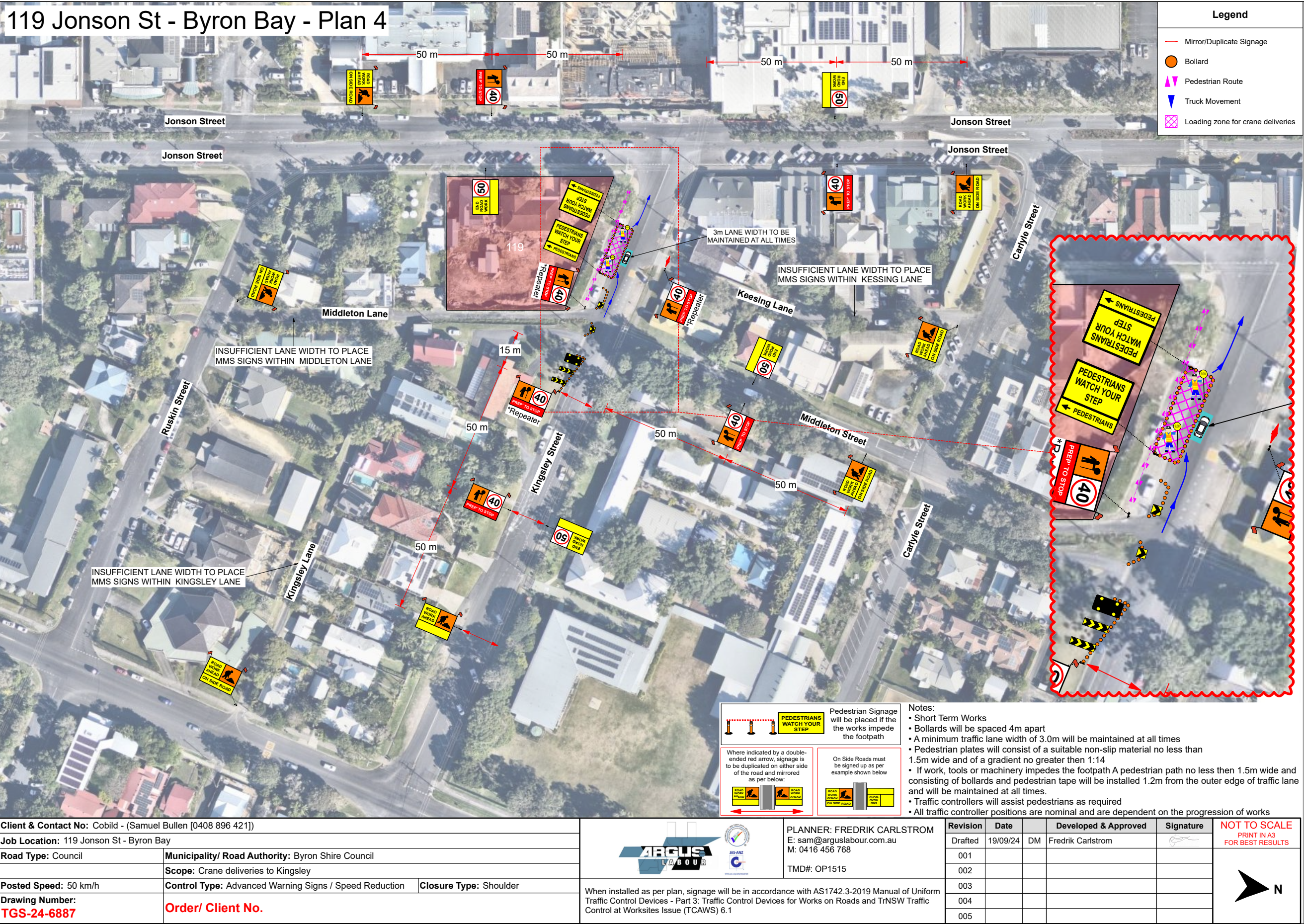
PLANNER: FREDRIK CARLSTROM  
E: sam@arguslabour.com.au  
M: 0416 456 768  
TMD#: OP1515

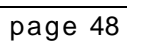
When installed as per plan, signage will be in accordance with AS1742.3-2019 Manual of Uniform Traffic Control Devices - Part 3: Traffic Control Devices for Works on Roads and TrNSW Traffic Control at Worksites Issue (TCAWS) 6.1

| Revision | Date     | Developed & Approved | Signature         | NOT TO SCALE<br>PRINT IN A3<br>FOR BEST RESULTS                                       |
|----------|----------|----------------------|-------------------|---|
| Drafted  | 19/09/24 | DM                   | Fredrik Carlstrom |  |
| 001      |          |                      |                   |   |
| 002      |          |                      |                   |   |
| 003      |          |                      |                   |   |
| 004      |          |                      |                   |   |
| 005      |          |                      |                   |   |

119 Jonson St - Byron Bay - Plan 3









**Report No. 6.2      Slessor Lane, Brunswick Heads - No Stopping area**

**File No:** I2024/1373

**5 Purpose**

This report proposes new parking signage and line marking on Slessor Lane to prevent driver confusion and avoid conflicts between vehicles and pedestrians. The proposed changes also aim to improve the safety of residents, businesses, workers, emergency services, Council Staff, and other road users.

- 10 This is in response to the Notice of Motion raised by Cr Michael Lyon during the Council Planning meeting held on 1 August 2024 and resolved under Council resolution 24-364, which states that Council:

- 15 *1. Investigates, through the Local Traffic Committee, changes to Slessor Lane in Brunswick Heads, where recent development activity on the corner of Tweed Street is causing conflicts between patrons and residents, including investigating changing it to one-way out to Tweed Street.*
- 2. Investigates other laneways in Brunswick Heads that may benefit from changes to traffic conditions.*

Note – Part 2 of the above resolution will be dealt with separately.

**20 Information/background**

- 25 Three tenancies have been created in the building where the former mechanic used to operate on the corner of Tweed Street and Slessor Lane in Brunswick Heads. These include a retail store, a café and a proposed gym which is under construction. These tenancies have all been operated independently and all relied on provisions relating to existing use under a SEPP. Staff have done a great job of attempting to bring this into compliance and a DA is expected to be lodged shortly to deal with the change of uses and the increase in parking that is needed to service them.

- 30 Council has recently received numerous requests from the community for investigations of traffic impacts which have been caused by changes in land use, typically approved as a new development application with council, or utilising existing development consent. Traffic impacts on Slessor Lane associated with the development of 38 Tweed Street are displayed in Figure 1 below.



*Figure 1 – Construction traffic using Slessor Lane for parking*

Vehicles are currently parking in Slessor Lane to access the tenancies within the development at 38 Tweed Street. This is creating confusion and safety hazards between residents, patrons and pedestrians.

- 5 Vehicles parking on Slessor Lane also increases the difficulty of refuse collection and servicing, given the narrow roadway.

Council staff recommend installation of yellow line marking and 'No Stopping' signs on both sides of Slessor Lane adjacent to the 38 Tweed Street development. See Figure 2 below for locations.



Figure 2: Map showing no parking area on Slessor Lane, Brunswick Heads

5 Installing new signs and new line marking will reduce the likelihood of vehicles parking at the corner of the laneway and conflicts between patrons and residents, while a permanent solution including one-way laneways is investigated for the wider Brunswick Heads area.

It is anticipated that Council enforcement officers and Police officers would use their discretion when enforcing this signage.

See Figure 3 for an example of the signage type.

The proposed signage would replace existing in the same location.



10

Figure 3: Example of No Stopping sign

**RECOMMENDATION:**

**That the Local Traffic Committee support installation of yellow line marking and “No Stopping” signage on Slessor Lane where it meets Tweed Street, Brunswick Heads.**

5

## Report No. 6.3 Mullumbimby Road Upgrade

File No: I2024/1439

5 The purpose of this report is to gain Local Traffic Committee and Council support for the proposed changes to Mullumbimby Road.

The proposed works will improve road widths, turning lanes, drainage and the poor condition road surface, creating a safer road environment.

10 Pavement repair works will be undertaken from Mullumbimby to Gulgarn Road while pavement widening and drainage improvements will be focused around the stretch of Mullumbimby Road from McAuleys Lane to Gulgarn Road.

15 The funding for this project is divided into two Stages. Stage 1A is Blackspot, Council and R2R funding for improvements at McAuleys Lane. Stage 1B is Betterment and EPAR funding for the pavement repairs to the full length of the road and drainage improvements centred around Gulgarn Road. Draft plans for both Stages have been attached to this report (Attachment 1 E2024/130657 and Attachment 2).

Design is currently being finalised on the project, however, lines and signs will generally be in accordance with the attached plans. Approval is sought in principle and should any significant changes occur to the proposed signs and linemarking then the plans will be resubmitted to LTC for approval.

20 The main changes to the road layout are as follows:

- Road widening to maintain minimum lane widths of 3.5m and standard shoulder widths of 1.75m and an absolute minimum width of 0.5m where highly constrained.
- Introduction of a left turn deceleration lane for McAuleys Lane and a protected right turn lane.
- 25 • The Saddle Road and the residential driveway located between The Saddle Road and McAuleys Lane will formally signed and linemarked as left in left out only. This is due to the dangerous and inadequate stopping sight distance, manoeuvre sight distance, minimum gap sight distance, and safe intersection sight distance at these accesses.

30 A meeting was held with Transport to discuss the possibility of lowering the speed limit on Mullumbimby Road to address the sight distance issues at the Saddle Road and McAuleys Lane intersections. At this time Transport was not supportive of lowering the speed limit from 80km/hr in this location.

35

---

### RECOMMENDATION:

# BYRON SHIRE COUNCIL

## LOCAL TRAFFIC COMMITTEE MEETING

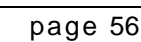
6.3

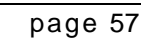
**That Council supports the signage, line markings and traffic control devices associated with the Mullumbimby Road works generally in accordance with Attachment 1 (E2024/130657) and Attachment 2 (E2024/130677)**

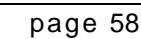
### **Attachments:**

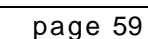
5

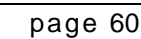
- 1 Mullumbimby Road McAuleys Design Plans, E2024/130657 , page 56 [↓](#) 
- 2 Mullumbimby Road Betterment Design Plans LTC, E2024/130677 , page 59 [↓](#) 

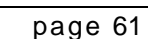












EXISTING SIGNAGE SCHEDULE

FOR INFORMATION

| SIGNAGE_ID | DRG NUMBER | REMAIN OR RELOCATE | REFERENCE | DESCRIPTION                          | EASTING    | NORTHING    | ROTATION (CW FROM NORTH) | WIDTH (mm) | HEIGHT (mm) | MOUNTING HEIGHT (mm) | NO. OF POSTS | POST SIZE | POST SPACING (mm) | FOOTING REQUIRED? | FOOTING DEPTH (mm) | FOOTING DIAMETER (mm) | COMMENTS     |
|------------|------------|--------------------|-----------|--------------------------------------|------------|-------------|--------------------------|------------|-------------|----------------------|--------------|-----------|-------------------|-------------------|--------------------|-----------------------|--------------|
| ES-001     | FN-PL-01   | REMAIN             | R5-400(L) | NO STOPPING (LEFT SYMBOLIC)          | 552063.779 | 6839185.227 | 45                       | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     |              |
| ES-002     | FN-PL-01   | REMAIN             | W5-56     | WILDLIFE SIGN                        | 552064.523 | 6839188.455 | 128                      | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     |              |
| ES-003     | FN-PL-01   | REMAIN             | W2-3      | T-JUNCTION (SYMBOLIC)                | 552063.306 | 6839210.877 | 36                       | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     |              |
| ES-004     | FN-PL-01   | REMAIN             | -         | FLOOD SIGN                           | 552046.907 | 6839205.934 | 123                      | -          | -           | -                    | 2            | -         | -                 | -                 | -                  | -                     |              |
| ES-005     | FN-PL-01   | REMAIN             | G1-1      | ADVANCE DIRECTION SIGN               | 552035.064 | 6839229.9   | 290                      | -          | -           | -                    | 2            | -         | -                 | -                 | -                  | -                     |              |
| ES-006     | FN-PL-01   | REMAIN             | G4-1      | REASSURANCE DIRECTION SIGN           | 551984.785 | 6839236.755 | 110                      | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     |              |
| ES-007     | FN-PL-01   | REMAIN             | R4-1      | SPEED LIMIT 80 km/h (SYMBOLIC)       | 551974.228 | 6839243.13  | 101                      | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     |              |
| ES-008     | FN-PL-01   | RELOCATE           | W1-3(R)   | CURVE (RIGHT SYMBOLIC)               | 551839.817 | 6839297.192 | 108                      | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     | SHARED POSTS |
| ES-009     | FN-PL-01   |                    | W8-2      | 65 km/h ADVISORY SPEED               |            |             |                          | -          | -           | -                    |              | -         | -                 | -                 | -                  | -                     |              |
| ES-010     | FN-PL-02   | REMAIN             | -         | NON-STANDARD SIGN (FRUIT & VEG)      | 551764.212 | 6839352.828 | 110                      | -          | -           | -                    | 2            | -         | -                 | -                 | -                  | -                     |              |
| ES-011     | FN-PL-02   | RELOCATE           | W1-3(L)   | CURVE (LEFT SYMBOLIC)                | 551627.466 | 6839472.797 | 318                      | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     | SHARED POSTS |
| ES-012     | FN-PL-02   |                    | W8-2      | 75 km/h ADVISORY SPEED               |            |             |                          | -          | -           | -                    |              | -         | -                 | -                 | -                  | -                     |              |
| ES-013     | FN-PL-03   | REMAIN             | G1-207    | ADVANCE RURAL ROAD SIGN WITH W2-4(R) | 551580.781 | 6839514.947 | 138                      | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     |              |
| ES-014     | FN-PL-03   | REMAIN             | W6-7      | BICYCLE (SYMBOLIC)                   | 551524.054 | 6839585.856 | 138                      | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     | SHARED POSTS |
| ES-015     | FN-PL-03   | REMAIN             | -         | CYCLISTS (SYMBOLIC) BE SAFE BE SEEN  |            |             | 138                      | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     |              |
| ES-016     | FN-PL-03   | RELOCATE           | -         | FLOOD SIGN                           | 551525.483 | 6839602.786 | 321                      | -          | -           | -                    | 2            | -         | -                 | -                 | -                  | -                     |              |
| ES-017     | FN-PL-03   | RELOCATE           | R4-1      | SPEED LIMIT 80 km/h (SYMBOLIC)       | 551475.506 | 6839664.385 | 318                      | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     |              |
| ES-018     | FN-PL-03   | REMAIN             | W5-56     | WILDLIFE AHEAD                       | 551440.548 | 6839709.715 | 318                      | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     |              |
| ES-019     | FN-PL-03   | REMAIN             | W6-7      | BICYCLE (SYMBOLIC)                   | 551424.17  | 6839732.135 | 317                      | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     | SHARED POSTS |
| ES-020     | FN-PL-03   | REMAIN             | -         | CYCLISTS (SYMBOLIC) BE SAFE BE SEEN  |            |             | 317                      | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     |              |
| ES-021     | FN-PL-03   | REMAIN             | G3-5      | RURAL ROAD NAME (SADDLE ROAD)        | 551407.127 | 6839726.615 | 321                      | -          | -           | -                    | 3            | -         | -                 | -                 | -                  | -                     | SHARED POSTS |
| ES-022     | FN-PL-03   | REMAIN             | D4-4      | SIGHTING SCREEN                      |            |             | 51                       | -          | -           | -                    |              | -         | -                 | -                 | -                  | -                     |              |
| ES-023     | FN-PL-03   | REMAIN             | R1-1      | STOP SIGN                            | 551422.032 | 6839739.582 | 53                       | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     |              |
| ES-024     | FN-PL-03   | REMAIN             | R1-1      | STOP SIGN                            | 551411.995 | 6839751.671 | 53                       | -          | -           | -                    | 1            | -         | -                 | -                 | -                  | -                     |              |

NOT FOR CONSTRUCTION

DRAWING FILE LOCATION / NAME  
BKCD185-2000-5200-FN-SCH-01

EXTERNAL REFERENCE FILES

REV  
01

DATE  
05.11.24

AMENDMENT / REVISION DESCRIPTION  
ISSUED FOR 50% DESIGN

APPROVAL

DESIGN MODEL FILE(S) USED FOR DOCUMENTATION OF THIS DRAWING

SCALES ON A1 SIZE DRAWING

DRAWINGS / DESIGN PREPARED BY  
BRIDGE KNOWLEDGE PTY LTD

CO-ORDINATE SYSTEM

HEIGHT DATUM

PLOT DATE / TIME  
5/11/2024

PLOT BY  
----

TITLE

NAME

DATE  
2.7.24

DRAWN  
DJK


DRG CHECK  
GM

DESIGN  
SKN

DESIGN CHECK  
GM

DESIGN MNGR

PROJECT MNGR

CLIENT  
BYRON SHIRE COUNCIL

PREPARED FOR  
BYRON SHIRE COUNCIL

SHIRE OF BYRON BAY  
MULLUMBIMBY ROAD – BETTERMENT PROJECT  
SIGNS, LINES AND ROAD FURNITURE  
EXISTING SIGNAGE SCHEDULE

BK PROJECT No.  
BKCD185

LCC PROJECT No.

PART

DRG No.  
BKCD185-2000-5200-FN-SCH-01

SHEET No.  
46 OF 46

ISSUE  
1

© BRIDGE KNOWLEDGE PTY LTD

A1

## Report No. 6.4 Lighthouse Road - Shared path - 'Get NSW Active' Grant Application

File No: I2024/1459

- 5 The purpose of this report is to gain Local Traffic Committee and Council support for the proposed changes to Lighthouse Road, Byron Bay.

The project entails construction of a footpath commencing at the intersection of Brooke Road, extending toward the lighthouse, for 852m. Inclusive of three new crossings, new signage, and line marking, as per attachment 1.

- 10 The width of the footpath varies along the alignment of Lighthouse Road, primarily due to the steep topography of the land and limited available space. The following describes the footpath alignment along the road:

CH0 - 33 = 1.8m path | Footpath Type 1, Approach to intersection crossing with Brooke Road, also have a width limitation due to an existing bus stop that will be maintained.

- 15 There is a 2.5m buffer zone which is used as the width for the bus to pull over. Road speed limit of 50km/hr – likely to be reduced to 30km/h, or at a maximum to 40km/h, as per TfNSW speed limit review of Lighthouse Road - awaiting TfNSW formal support letter.

- 20 CH33 - 210 = 2.5m = Shared path | Footpath Type 2, Raised boardwalk due to steep embankment for the length adjacent to Lighthouse Road. There is a 0.5m buffer zone with either bollards or a cycle friendly railing. Road speed limit of 50km/hr - likely to be reduced to 30km/h, or at a maximum to 40km/h as per TfNSW speed limit review of Lighthouse Road - awaiting TfNSW formal support letter.

- 25 CH210 - 842 = 1.5m = pedestrian path (uphill cyclists directed to continue along Lighthouse Rd, downhill cyclists to use one-way vehicle lane) | Footpath Type 1, buffer zone of 0.25m with guide-posts adjacent to one way traffic with a posted 10km/hr shared zone speed limit.

Staff is now working on the 'Get NSW Active' FY2025/25 Grant application to fund construction.

- 30 Staff has applied for the same Grant FY2024/25 which was unsuccessful. As a result of that, staff has worked to improve the application based on TfNSW feedback.


- 35 Based on the project specific feedback, staff has consulted with TfNSW regarding the speed limit of the road and it happened to be at the same time, TfNSW was already undertaking a speed limit review for Lighthouse Road.  
The speed limit is being proposed to reduce from 50km/h to 30km/h, and the 10km/h shared space to continue.

- 40 A Review of Environmental Factors and a Safety in Design Report have been completed for the proposed work.

**RECOMMENDATION:**

**That the Local Traffic Committee endorses the detail design of the Lighthouse Road footpath extension project, as per drawings (E2024/126445) in attachment 1.**

**5 Attachments:**

1 100%\_SET\_LIGHTHOUSE RD - shared path 23.10.2024, E2024/126445 , page 65 [↓](#) 

10

Locality Map



Shared Path

PM-22-1386

Lighthouse Road, Byron Bay, NSW

For: Byron Shire Council



**ARDILL PAYNE**  
& P a r t n e r s  
ENGINEERS PLANNERS SURVEYORS  
ENVIRONMENTAL PROJECT MANAGEMENT

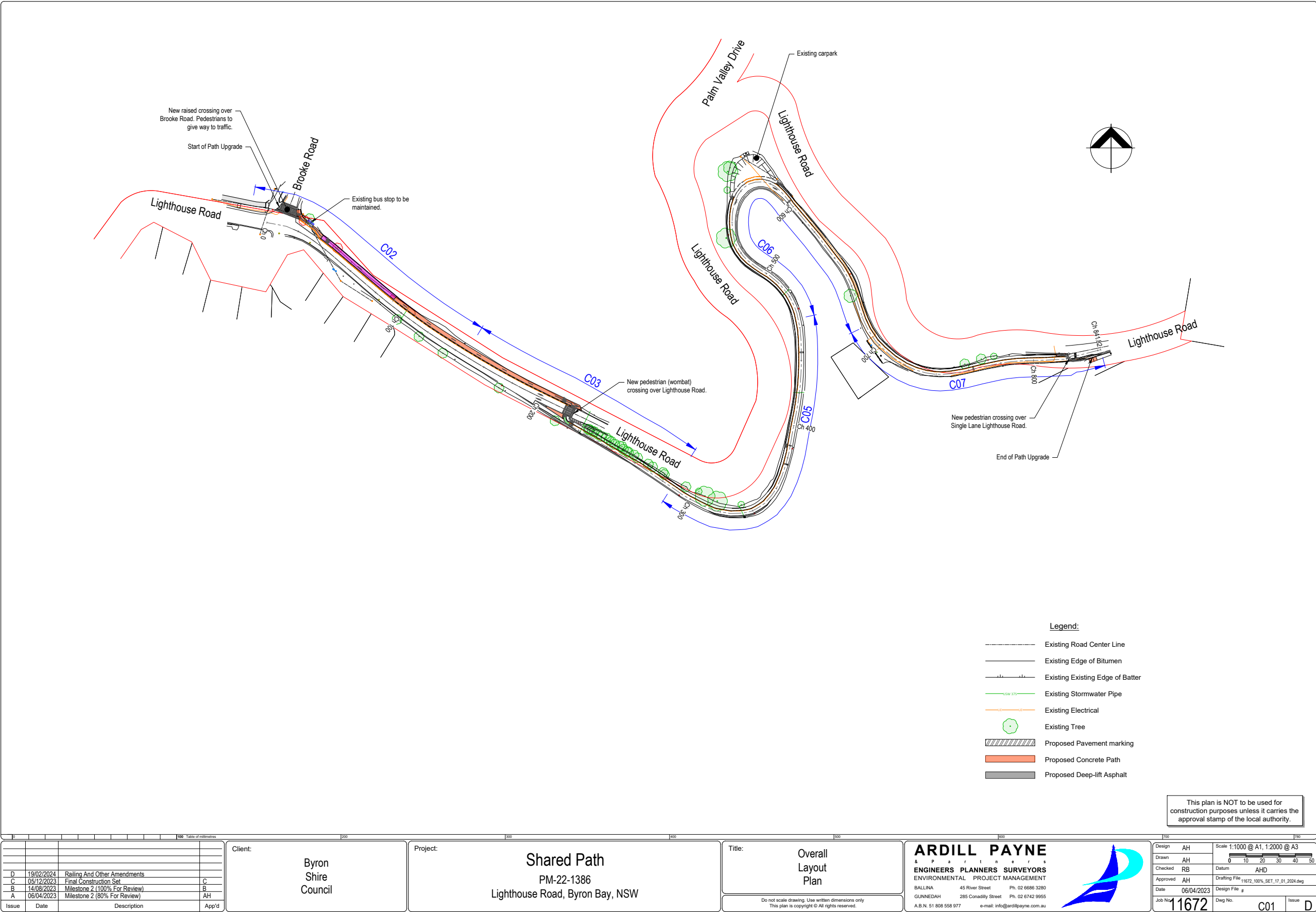
BALLINA 45 River Street Ph. 02 6686 3280  
GUNNDAH 285 Conadilly Street Ph. 02 6742 9955  
A.B.N. 51 808 558 977 e-mail: info@ardillpayne.com.au

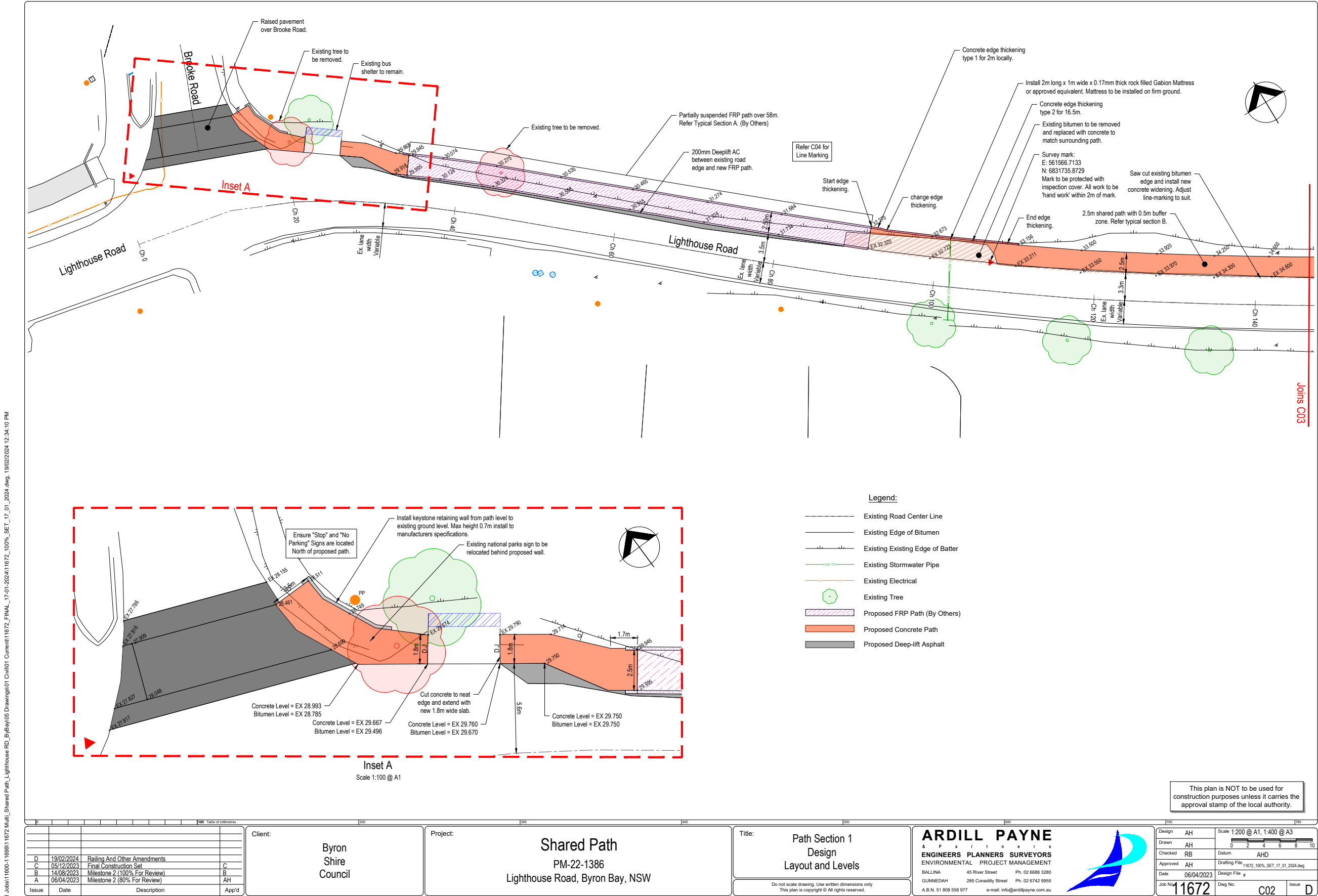
Drawing Schedule

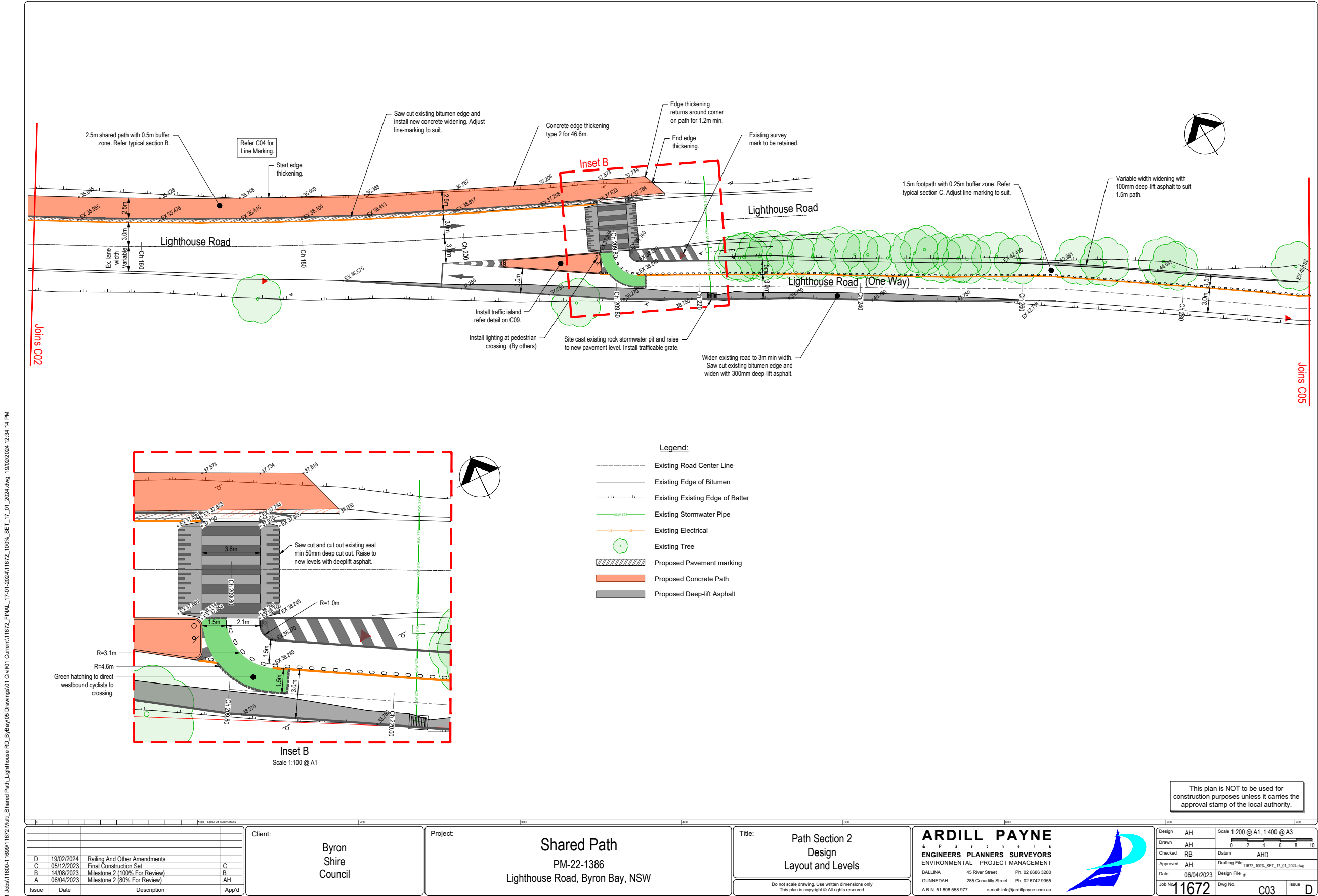
| Drawing    | Sheet    | Description   |
|------------|----------|---|
| 11672 -C01 | 1 of 10  | Overall Layout Plan                                       |
| 11672 -C02 | 2 of 10  | Path Section 1 Design Layout and Levels                   |
| 11672 -C03 | 3 of 10  | Path Section 2 Design Layout and Levels                   |
| 11672 -C04 | 4 of 10  | Path Section 1 & 2 Design Signage Linemarking Layout Plan |
| 11672 -C05 | 5 of 10  | Path Section 3 Design Layout and Levels                   |
| 11672 -C06 | 6 of 10  | Path Section 4 Design Layout and Levels                   |
| 11672 -C07 | 7 of 10  | Path Section 5 Design Layout and Levels                   |
| 11672 -C08 | 8 of 10  | Construction Notes and Details Sheet 1                    |
| 11672 -C09 | 9 of 10  | Construction Notes and Details Sheet 2                    |
| 11672 -C10 | 10 of 10 | Swept Path Assessment 8.8m Long MRV                       |

S:\01 Jobs\11600-11699\11672 Multi Shared Path\_Lighthouse RD\_ByBay\05 Drawings\01 Civil\01 Current\11672\_FINAL\_17-01-2024\11672\_100%\_SET\_17\_01\_2024.dwg, 19/02/2024 12:34:06 PM

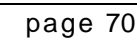
S:\101\_Jones\11672\_Multi\11672\_Multi\_Shared Path\_Lighthouse RD\_ByBay\15 Drawings\01 Civil\01 Current\11672\_FINAL\_17-01-2024\11672\_100%\_SET\_17\_01\_2024.dwg, 19/02/2024 12:34:07 PM

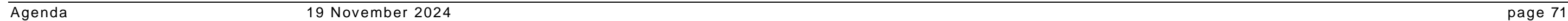


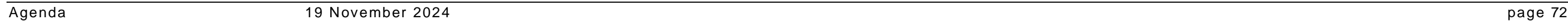




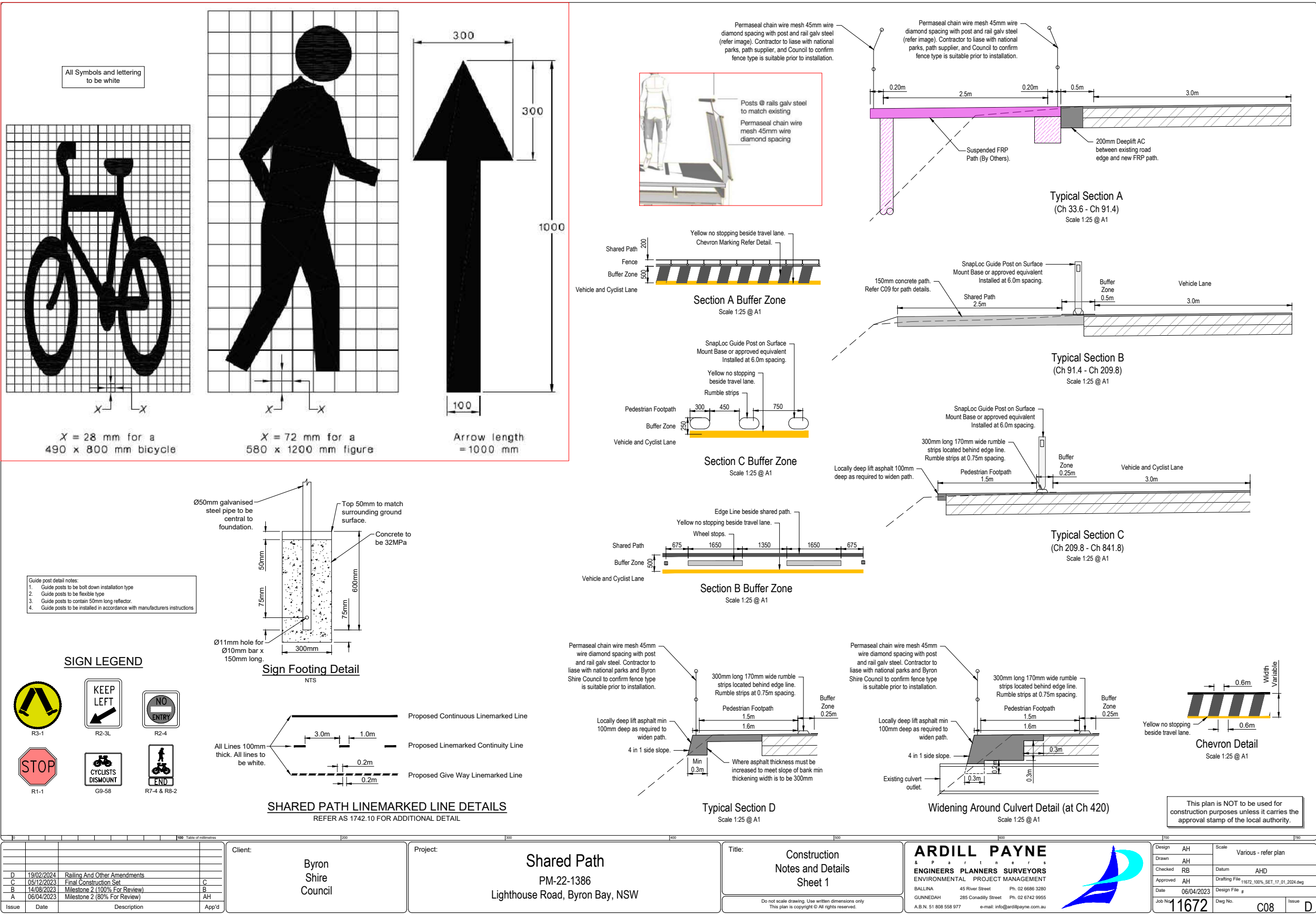


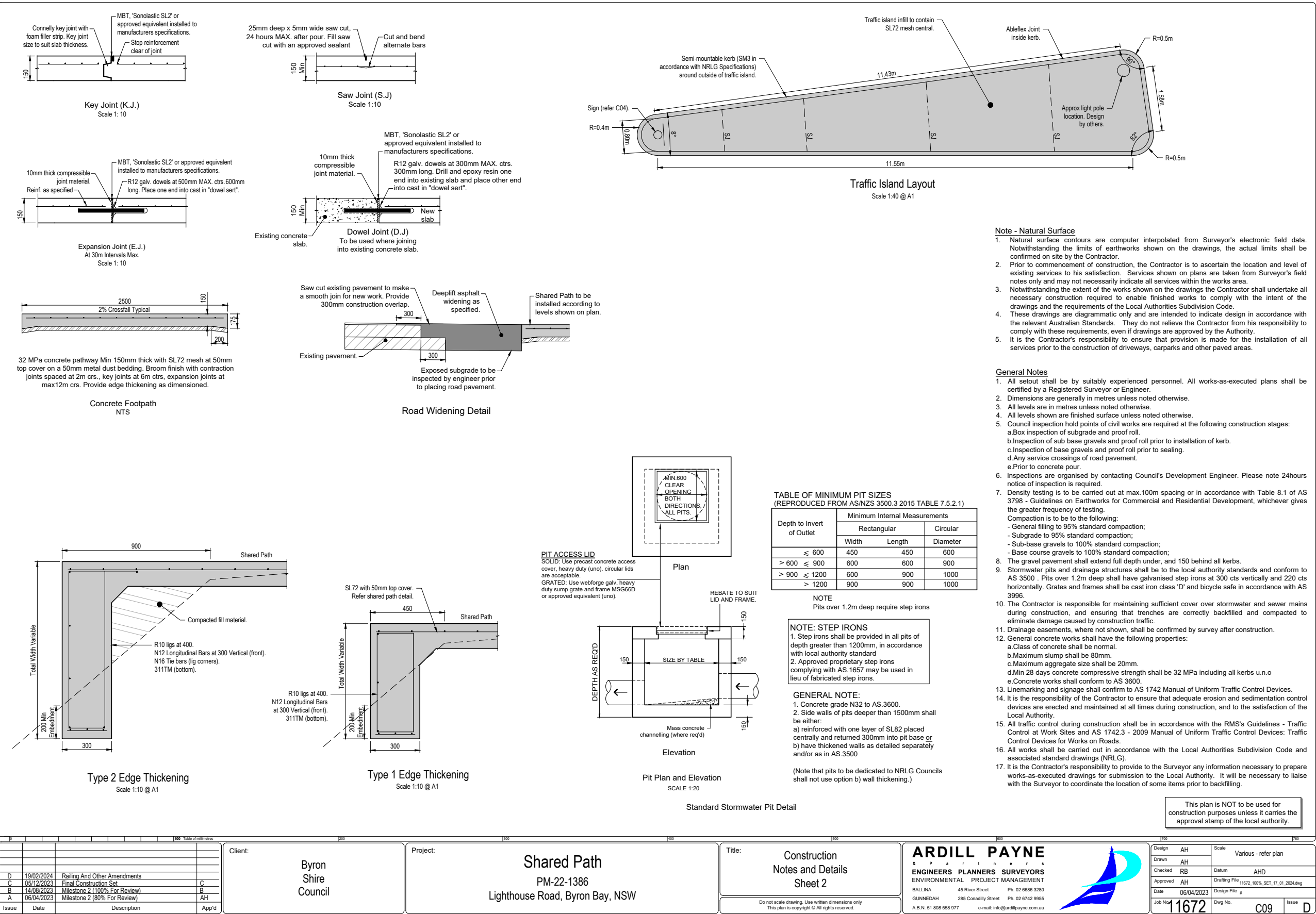


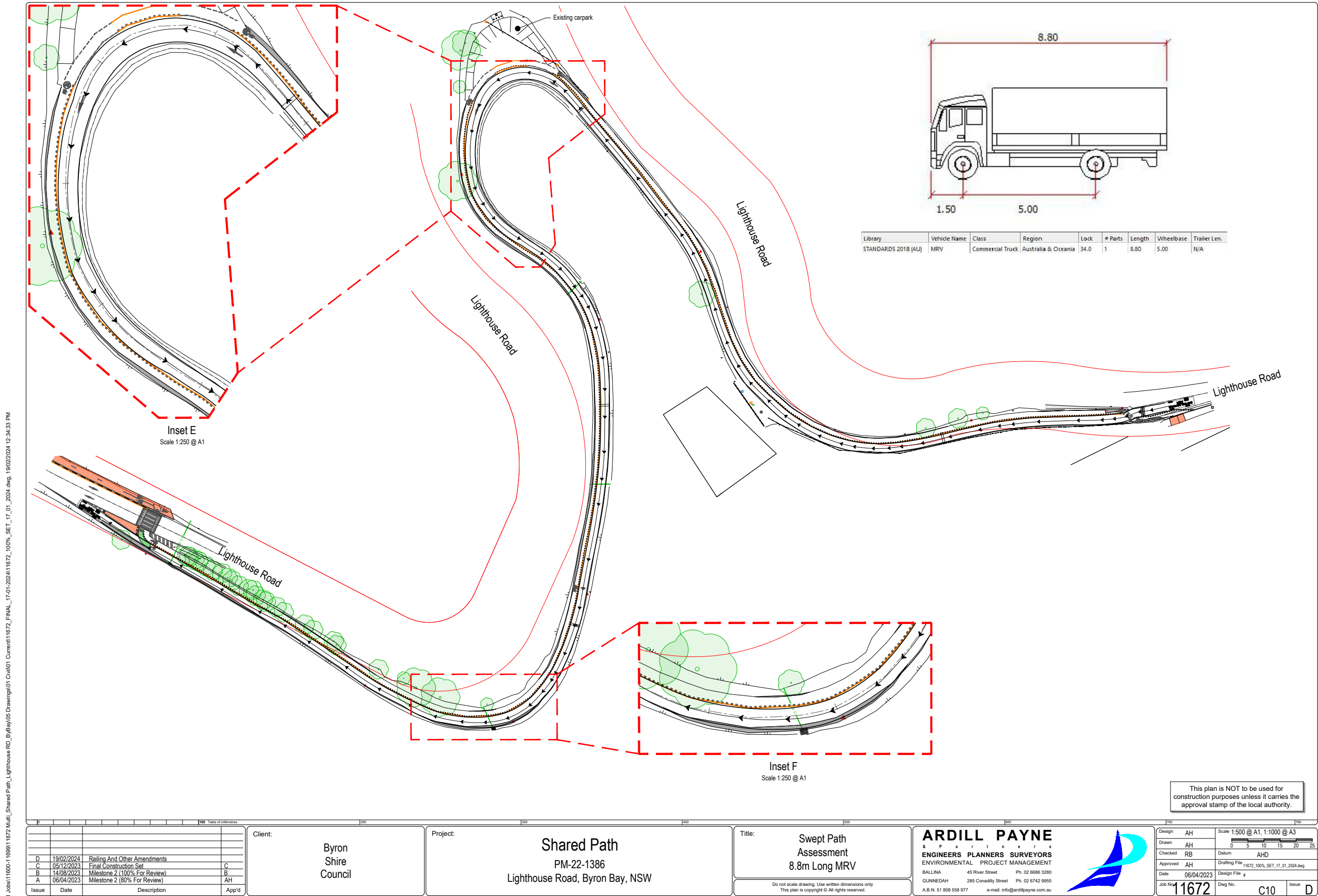


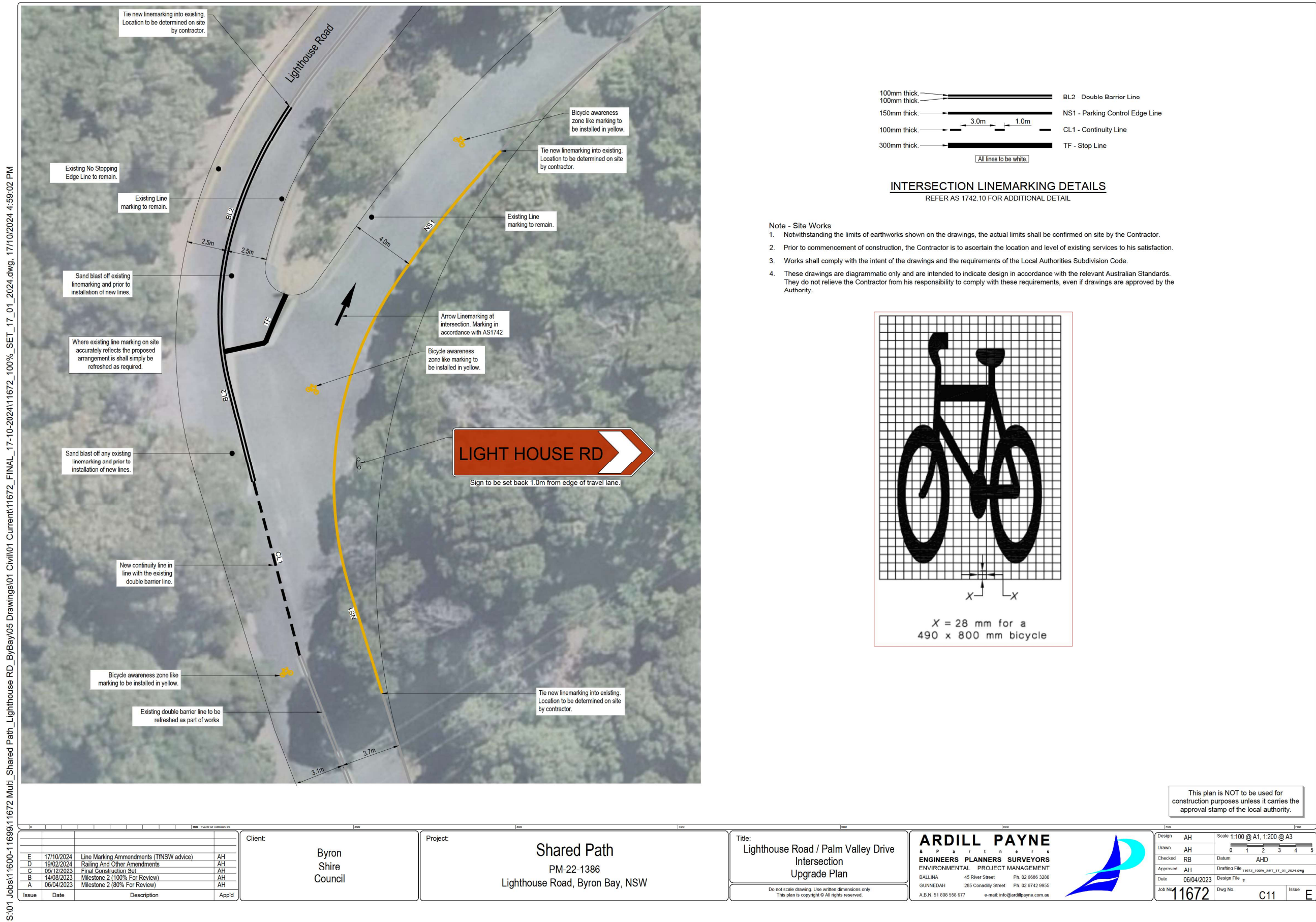


S:\101 Jobs\11600-11699\11672 Multi\_Shared Path\_Lighthouse RD\_ByBay\05 Drawings\01 Civil\01 Current\11672\_FINAL\_17-01-2024\11672\_100%\_SET\_17\_01\_2024.dwg, 19/02/2024 12:34:31 PM









**Report No. 6.5      Byron Street shared path – Bangalow - 'Get NSW Active' Grant Application**

**File No:** I2024/1499

- 5      The purpose of this report is to gain Local Traffic Committee and Council support for the proposed changes to Byron Street, Bangalow.

10      Works will include the construction of a 2.5m wide concrete path with associated drainage, landscaping, crossings, signs, lines and retaining walls. The existing handrail on the pedestrian bridge over Byron Creek will be replaced with a bike safe full barrier fence, as per plan in attachment 1.

Staff is now working on the 'Get NSW Active' FY2025/26 Grant application to fund construction.

Staff has applied for the same Grant FY2024/25 which was unsuccessful. As a result of that, staff has worked to improve the application based on TfNSW feedback, attachment 2.



- 15      Review of Environmental Factors and a Risk Register Report have been completed for the proposed work.

---

**RECOMMENDATION:**

- 20      **That the Local Traffic Committee endorses the detail design of the Byron Street footpath extension project, as per drawings (E2024/129370) in attachment 1.**

**Attachments:**

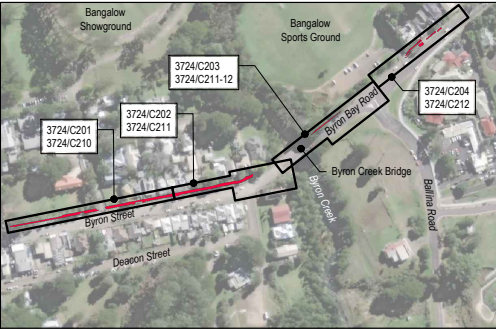
- 25      1      IFC 3724-C200 Set - Byron Street shared Path - Bangalow, E2024/129370 , page 78  

Byron Street, Bangalow  
Shared Path - Stage 2  
Detailed Design

prepared for



| Drawing Register |  |
|------------------|--|
| 3724/C200        | Cover Sheet and Locality Plan                |
| 3724/C201        | Existing Site Layout - Sheet 1 of 4          |
| 3724/C202        | Existing Site Layout - Sheet 2 of 4          |
| 3724/C203        | Existing Site Layout - Sheet 3 of 4          |
| 3724/C204        | Existing Site Layout - Sheet 4 of 4          |
| 3724/C210        | Design Layout Plan - Sheet 1 of 3            |
| 3724/C211        | Design Layout Plan - Sheet 2 of 3            |
| 3724/C212        | Design Layout Plan - Sheet 3 of 3            |
| 3724/C220        | Shared Path Longitudinal Sections            |
| 3724/C221        | Driveway Longitudinal Sections               |
| 3724/C230        | Cross Sections: Station Ln. to Market Street |
| 3724/C231        | Cross Sections: Market Street to Bridge      |
| 3724/C240        | Typical Details                              |
| 3724/C241        | Notes and Specifications                     |



Key Plan - Byron Street and Byron Bay Road



Locality Plan - Bangalow

| Survey Marks |            |             |            |
|--------------|------------|-------------|------------|
| RL           | Position X | Position Y  | Position Z |
| BOLT11       | 551641.666 | 6826730.910 | 44.368     |
| DH01         | 551262.465 | 6826657.003 | 47.148     |
| DH02         | 551258.774 | 6826652.671 | 47.564     |
| DH03         | 551265.552 | 6826639.792 | 47.145     |
| DH04         | 551293.061 | 6826596.990 | 43.612     |
| DH05         | 551559.277 | 6826657.376 | 44.459     |
| DHW01        | 551920.262 | 6826903.769 | 53.462     |
| DHW02        | 551926.844 | 6826908.786 | 53.499     |
| DHW03        | 551902.122 | 6826927.609 | 51.437     |
| DHW04        | 551906.542 | 6826935.739 | 51.442     |
| DHW05        | 551878.459 | 6826942.441 | 51.417     |
| DHW06        | 551668.051 | 6826761.085 | 47.229     |
| DHW07        | 551581.554 | 6826699.672 | 46.716     |
| DHW08        | 551521.867 | 6826705.511 | 46.656     |
| DHW09        | 551521.830 | 6826705.501 | 46.646     |
| DHW10        | 551277.873 | 6826656.972 | 46.501     |
| DHW11        | 551343.471 | 6826604.827 | 43.685     |
| DHW12        | 551372.469 | 6826610.263 | 43.785     |
| DHW13        | 551604.571 | 6826662.235 | 44.789     |
| DHW14        | 551612.725 | 6826621.235 | 44.347     |
| GIN01        | 551276.739 | 6826657.302 | 46.541     |
| NIC01        | 551277.379 | 6826642.661 | 46.531     |
| NIC02        | 551271.038 | 6826598.598 | 44.978     |
| NIC03        | 551319.580 | 6826602.067 | 43.336     |
| NIC04        | 551359.021 | 6826607.737 | 43.807     |
| NIC05        | 551356.544 | 6826617.582 | 43.660     |
| NIC06        | 551443.055 | 6826634.604 | 42.281     |
| NIC07        | 551521.914 | 6826650.044 | 43.877     |
| NIC08        | 551584.996 | 6826701.256 | 46.139     |
| NIC09        | 551588.622 | 6826719.234 | 46.618     |
| NIC10        | 551604.895 | 6826711.858 | 46.455     |
| NIC12        | 551677.801 | 6826767.023 | 47.021     |
| NIC13        | 551675.983 | 6826781.229 | 47.315     |
| NIC14        | 551712.314 | 6826772.781 | 45.556     |
| NIC15        | 551761.469 | 6826783.995 | 46.774     |
| NIC16        | 551732.252 | 6826829.145 | 46.907     |
| NIC17        | 551814.807 | 6826874.882 | 48.078     |
| NIC18        | 551491.692 | 6826698.825 | 46.591     |
| NIC19        | 551452.034 | 6826691.693 | 46.086     |
| NIC20        | 551404.530 | 6826668.027 | 44.900     |
| NIC21        | 551331.491 | 6826666.954 | 44.818     |
| PEG01        | 551348.765 | 6826602.420 | 43.656     |
| PM39044      | 551604.702 | 6826664.071 | 44.646     |
| PM39061      | 551258.577 | 6826655.428 | 47.420     |
| SSM126073    | 551930.813 | 6826891.543 | 54.503     |
| SSM54514     | 551432.004 | 6826687.213 | 45.309     |
| SSM8102      | 551582.049 | 6826702.595 | 45.968     |
| SSM81635     | 551388.252 | 6826623.824 | 43.513     |

ISSUE FOR  
CONSTRUCTION

|     |             |      |     |     |     |
|-----|-------------|------|-----|-----|-----|
| Rev | Description | Date | Des | App | Chk |
|     |             |      |     |     |     |

Amendments

© GeoLINK

This drawing must not be relied upon for any purpose other than that for which it was prepared or by any person or corporation other than the referred client.

Lennox Head Office  
PO Box 119  
Lennox Head NSW 2478  
Tel: 02 6687 7666

Coffs Harbour Office  
PO Box 1446  
Coffs Harbour NSW 2450  
Tel: 02 6651 7666

Armidale Office  
PO Box 1207  
Armidale NSW 2350  
Tel: 02 6772 0454

Lismore Office  
PO Box 229  
Lismore NSW 2480  
Tel: 02 6621 6677

www.geolink.net.au

Email: info@geolink.net.au

quality solutions sustainable future

Project Title

Bangalow Shared Path: Stage 2

Client

Byron Shire Council

|          |     |       |           |         |
|----------|-----|-------|-----------|---------|
| Designed | MVE | Drawn | MVE       | Checked |
| Approved | JLC | Date  | 9/09/2022 | JLC     |

XREFs

Scale

metres

Not To Scale

Drawing Title

Cover Sheet, Locality Plan  
and Drawing Schedule

3724-1035

3724/C200

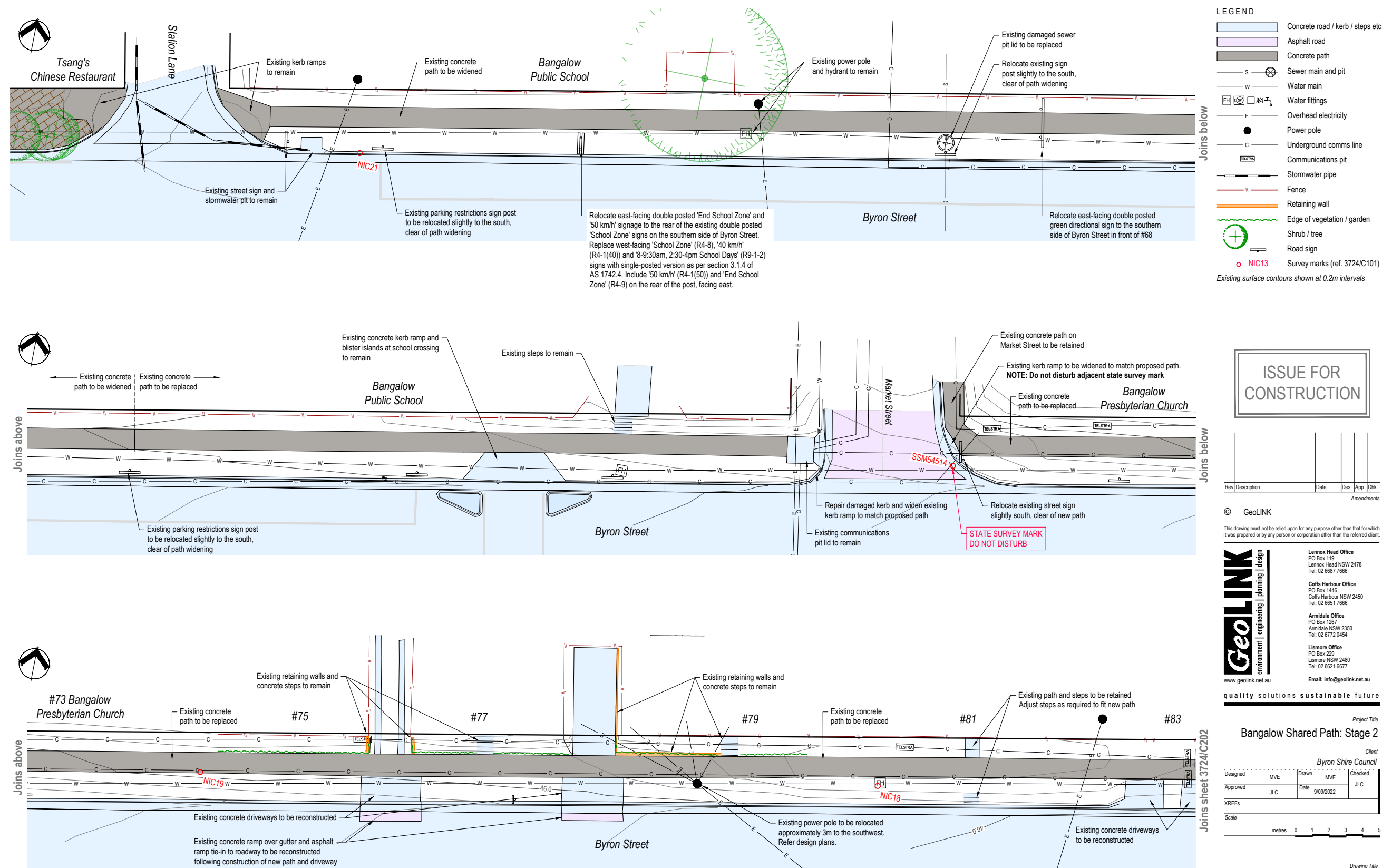
Drawing Number

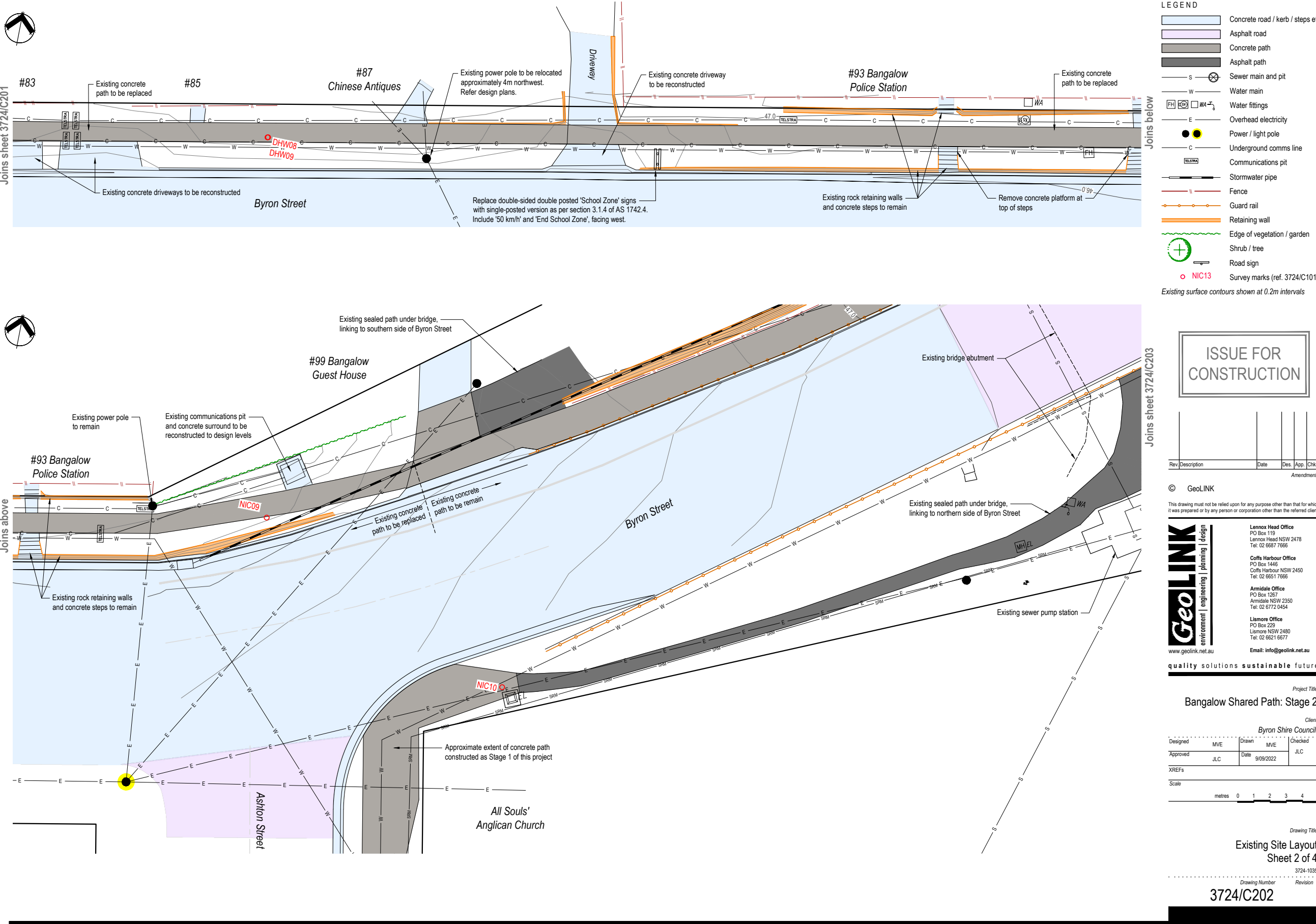
Revision

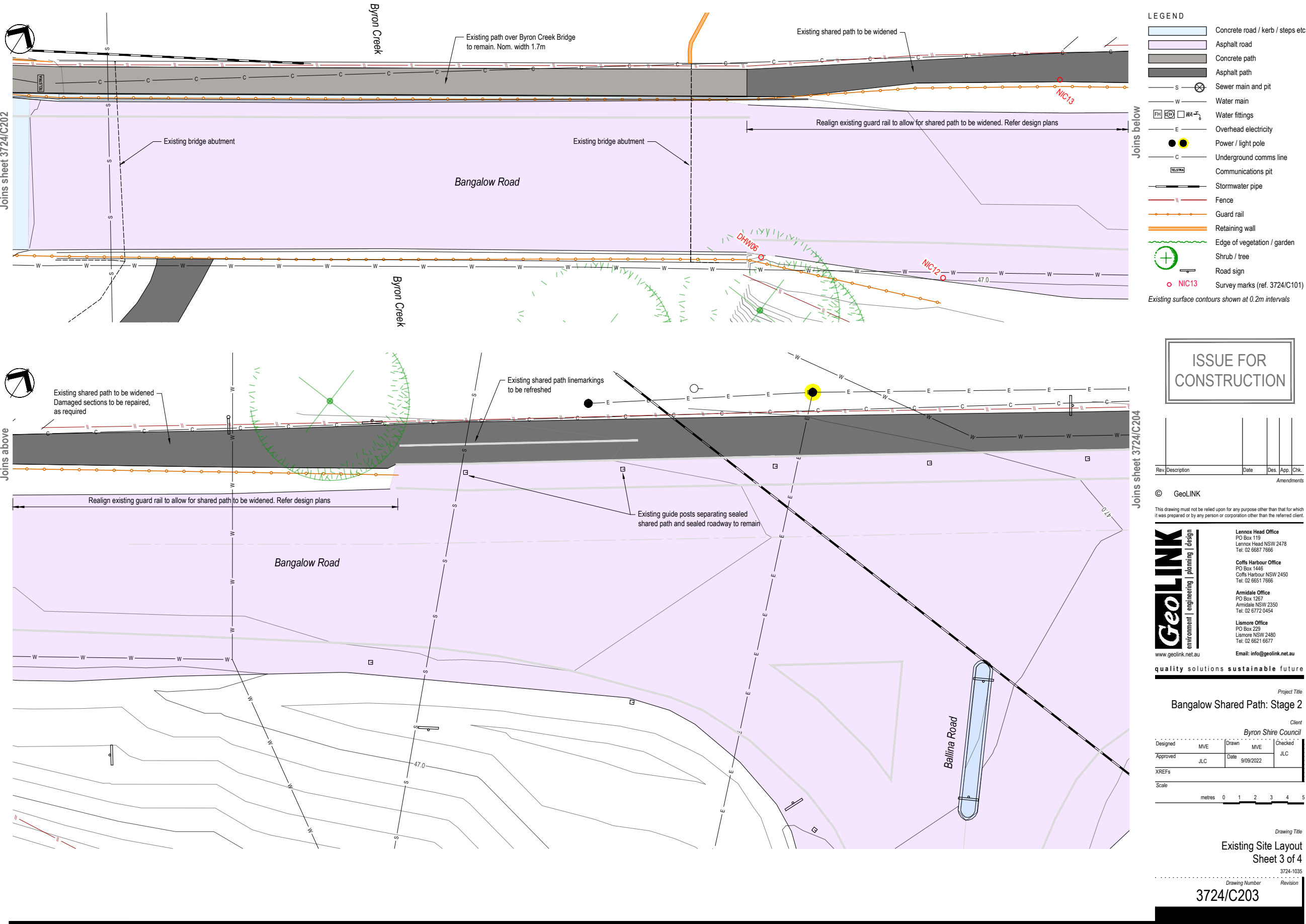
Agenda

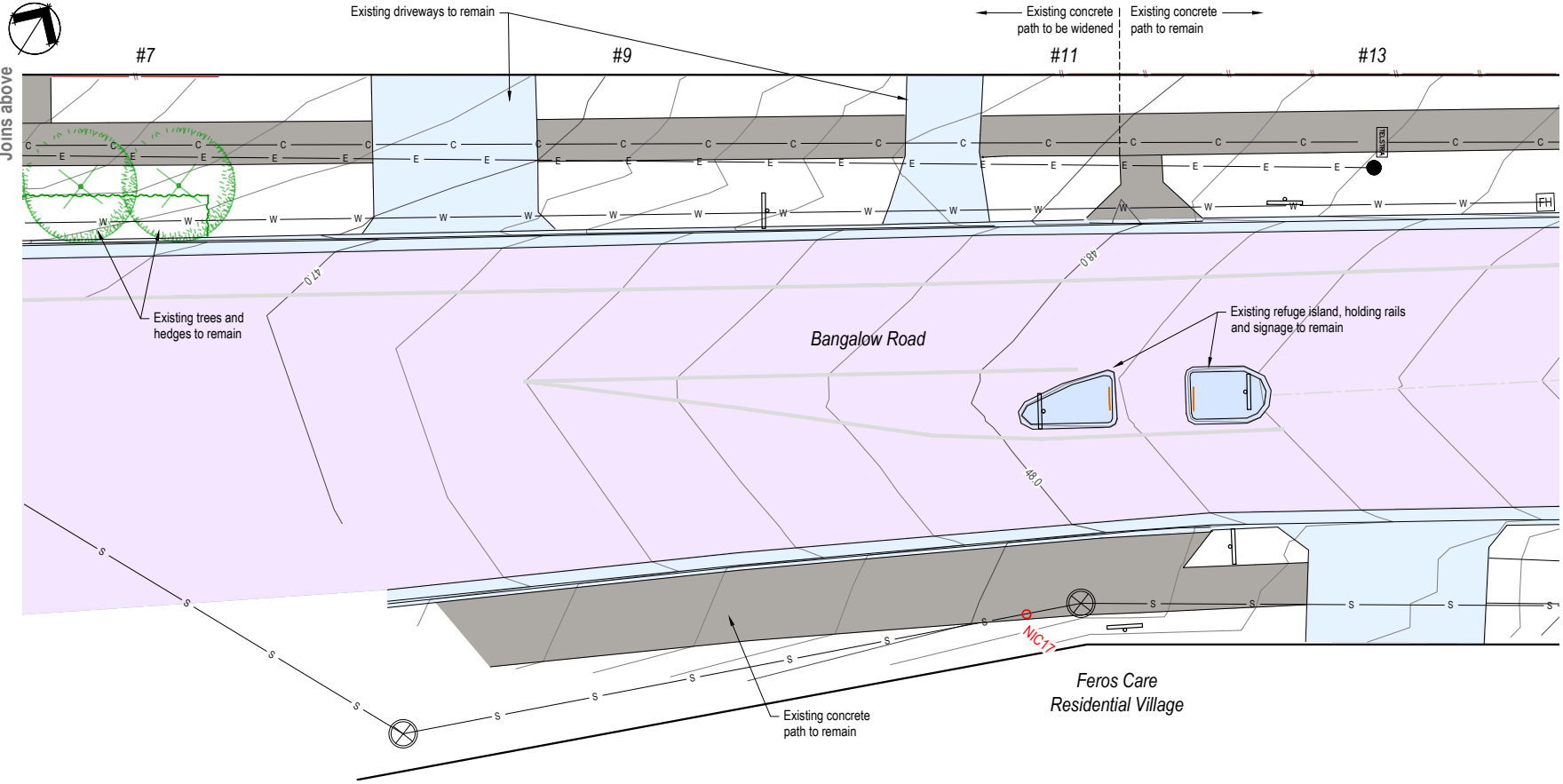
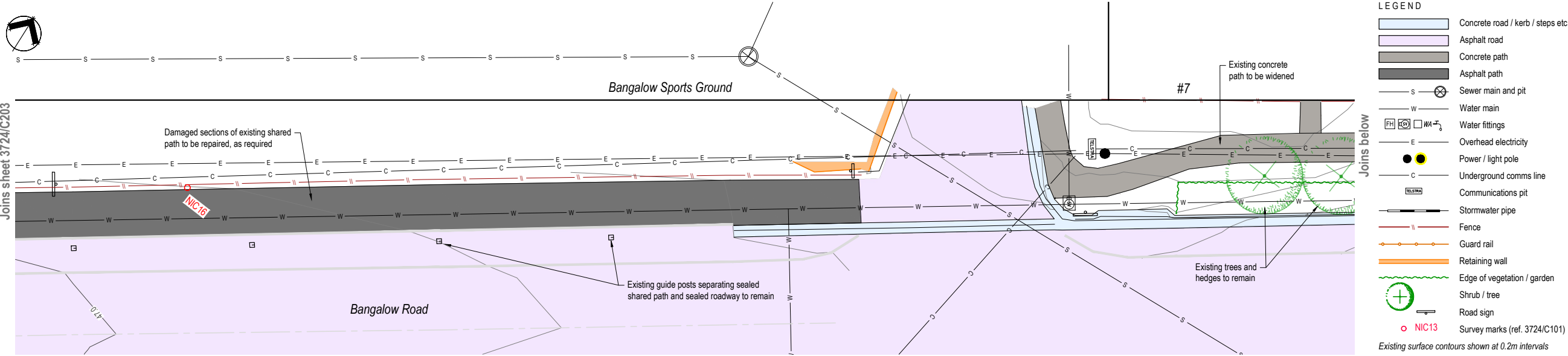
19 November 2024

page 78









ISSUE FOR  
CONSTRUCTION

| Rev. | Description | Date | Des. | App. | Chk. |
|------|-------------|------|------|------|------|
|      |             |      |      |      |      |

© GeoLINK

This drawing must not be relied upon for any purpose other than that for which it was prepared or by any person or corporation other than the referred client.

**GeoLINK**  
environment | engineering | planning | design

**Lennox Head Office**  
PO Box 119  
Lennox Head NSW 2478  
Tel: 02 6687 7666

**Coffs Harbour Office**  
PO Box 1446  
Coffs Harbour NSW 2450  
Tel: 02 6651 7666

**Armidale Office**  
PO Box 1207  
Armidale NSW 2350  
Tel: 02 6772 0454

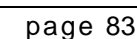
**Lismore Office**  
PO Box 229  
Lismore NSW 2480  
Tel: 02 6621 6677

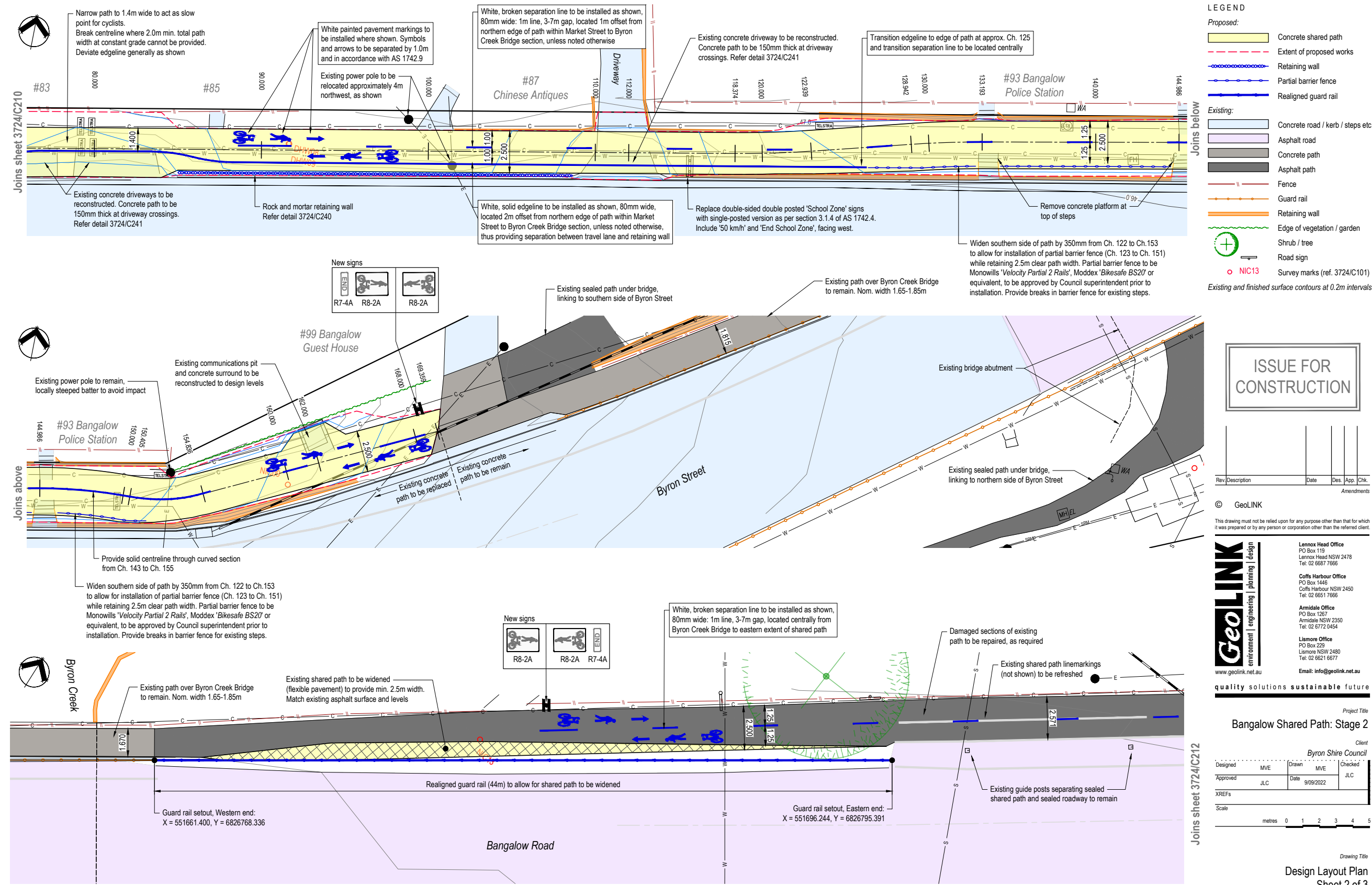
www.geolink.net.au Email: info@geolink.net.au

quality solutions sustainable future

|                               |     |       |           |         |  |
|-------------------------------|-----|-------|-----------|---------|--|
| Project Title                 |     |       |           |         |  |
| Bangalow Shared Path: Stage 2 |     |       |           |         |  |
| Client                        |     |       |           |         |  |
| Byron Shire Council           |     |       |           |         |  |
| Designed                      | MVE | Drawn | MVE       | Checked |  |
| Approved                      | JLC | Date  | 9/09/2022 | JLC     |  |
| XREFs                         |     |       |           |         |  |
| Scale                         |     |       |           |         |  |
| metres 0 1 2 3 4 5            |     |       |           |         |  |

|                      |          |
|----------------------|----------|
| Drawing Title        |          |
| Existing Site Layout |          |
| Sheet 4 of 4         |          |
| 3724-1035            |          |
| Drawing Number       | Revision |
| 3724/C204            |          |





**LEGEND**

**Proposed:**

- Concrete shared path
- Extent of proposed works
- Retaining wall
- Partial barrier fence
- Realigned guard rail

**Existing:**

- Concrete road / kerb / steps etc.
- Asphalt road
- Concrete path
- Asphalt path
- Fence
- Guard rail
- Retaining wall
- Edge of vegetation / garden
- Shrub / tree
- Road sign
- Survey marks (ref. 3724/C101)

**Other:**

- NIC13

Existing and finished surface contours at 0.2m intervals

**ISSUE FOR CONSTRUCTION**

| Rev. | Description | Date | Des. | App. | Chk. |
|------|-------------|------|------|------|------|
|      |             |      |      |      |      |

© GeoLINK

This drawing must not be relied upon for any purpose other than that for which it was prepared or by any person or corporation other than the referred client.

**GeoLINK**  
environment | engineering | planning | design  
www.geolink.net.au

**Lennox Head Office**  
PO Box 119  
Lennox Head NSW 2478  
Tel: 02 6687 7666

**Coffs Harbour Office**  
PO Box 1446  
Coffs Harbour NSW 2450  
Tel: 02 6651 7666

**Armidale Office**  
PO Box 1207  
Armidale NSW 2350  
Tel: 02 6772 0454

**Lismore Office**  
PO Box 229  
Lismore NSW 2480  
Tel: 02 6621 6677

Email: info@geolink.net.au

**quality solutions sustainable future**

**Project Title**  
Bangalow Shared Path: Stage 2

**Client**  
Byron Shire Council

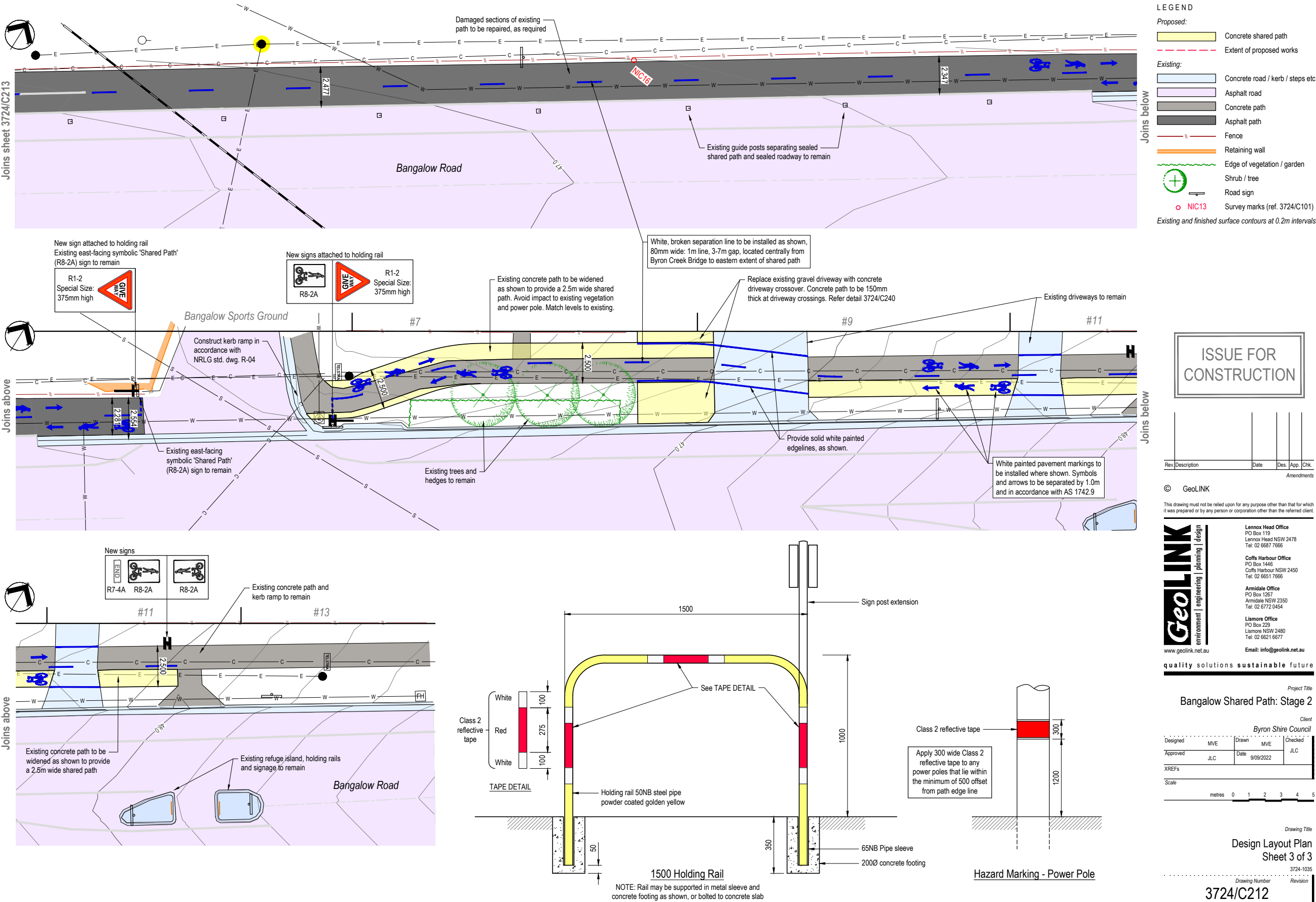
| Designed | MVE | Drawn | MVE | Checked |
|----------|-----|-------|-----|---------|
| JLC      | JLC | JLC   | JLC | JLC     |

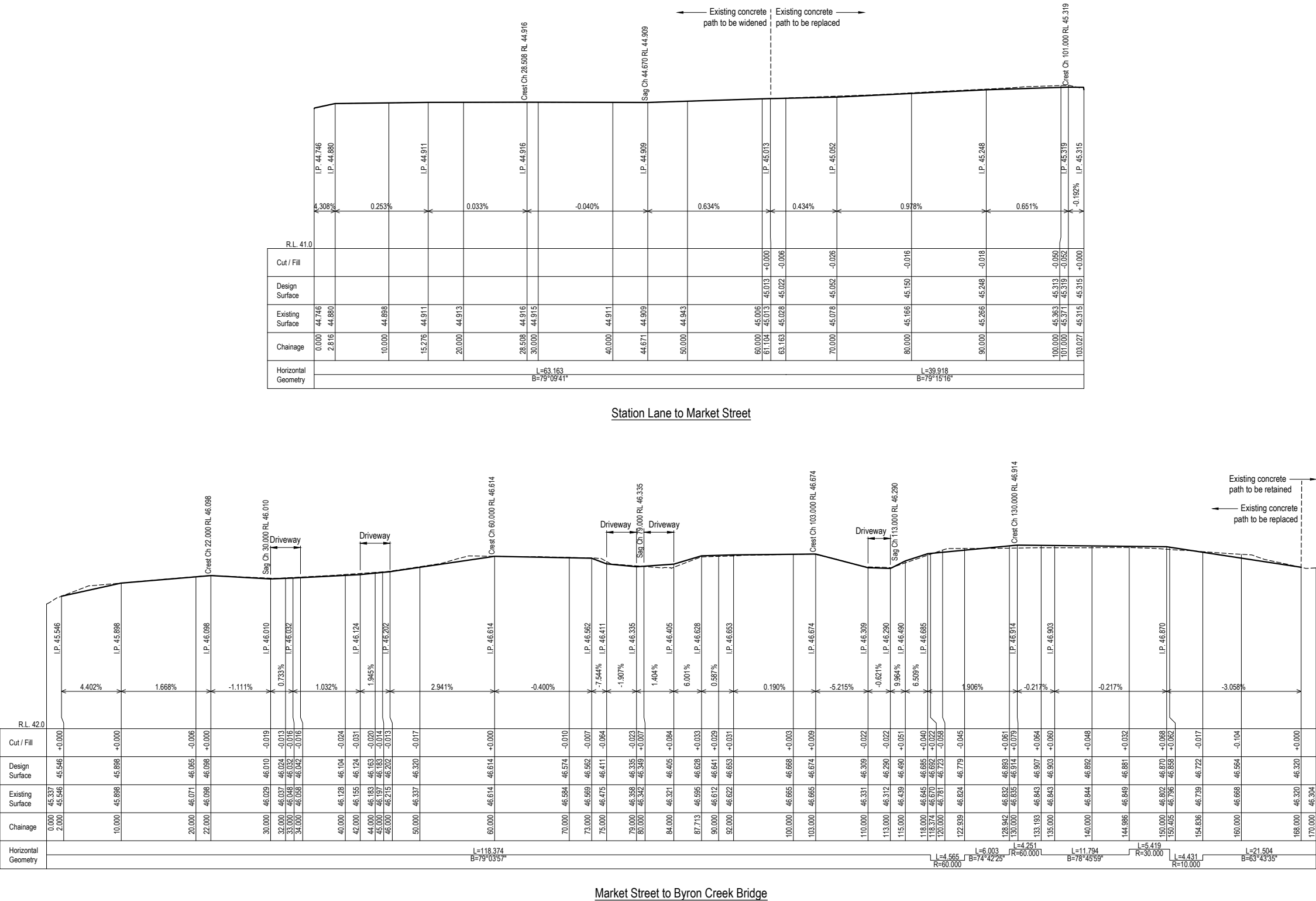
**Scale**  
metres 0 1 2 3 4 5

**Drawing Title**  
Design Layout Plan  
Sheet 2 of 3

**Drawing Number**  
3724/C211

**Revision**  
3724-1035





ISSUE FOR CONSTRUCTION

| Rev | Description | Date | Des. | App. | Chk. |
|-----|-------------|------|------|------|------|
|     |             |      |      |      |      |

© GeoLINK

GeoLINK

environment | engineering | planning | design

Lennox Head Office

PO Box 119

Lennox Head NSW 2478

Tel: 02 6687 7666

Coffs Harbour Office

PO Box 1446

Coffs Harbour NSW 2450

Tel: 02 6651 7666

Armidale Office

PO Box 1207

Armidale NSW 2350

Tel: 02 6772 0454

Lismore Office

PO Box 229

Lismore NSW 2480

Tel: 02 6621 6677

www.geolink.net.au

Email: info@geolink.net.au

quality solutions sustainable future

Project Title

Bangalow Shared Path: Stage 2

Client

Byron Shire Council

Designed

MVE

Drawn

MVE

Checked

JLC

Approved

JLC

Date

01/09/2022

XREFs

Scale

Vertical

metres

0

0.5

1.0

1.5

2.0

2.5

Horizontal

0

2.5

5

7.5

10

12.5

Drawing Title

Shared Path

Longitudinal Sections

3724-1035

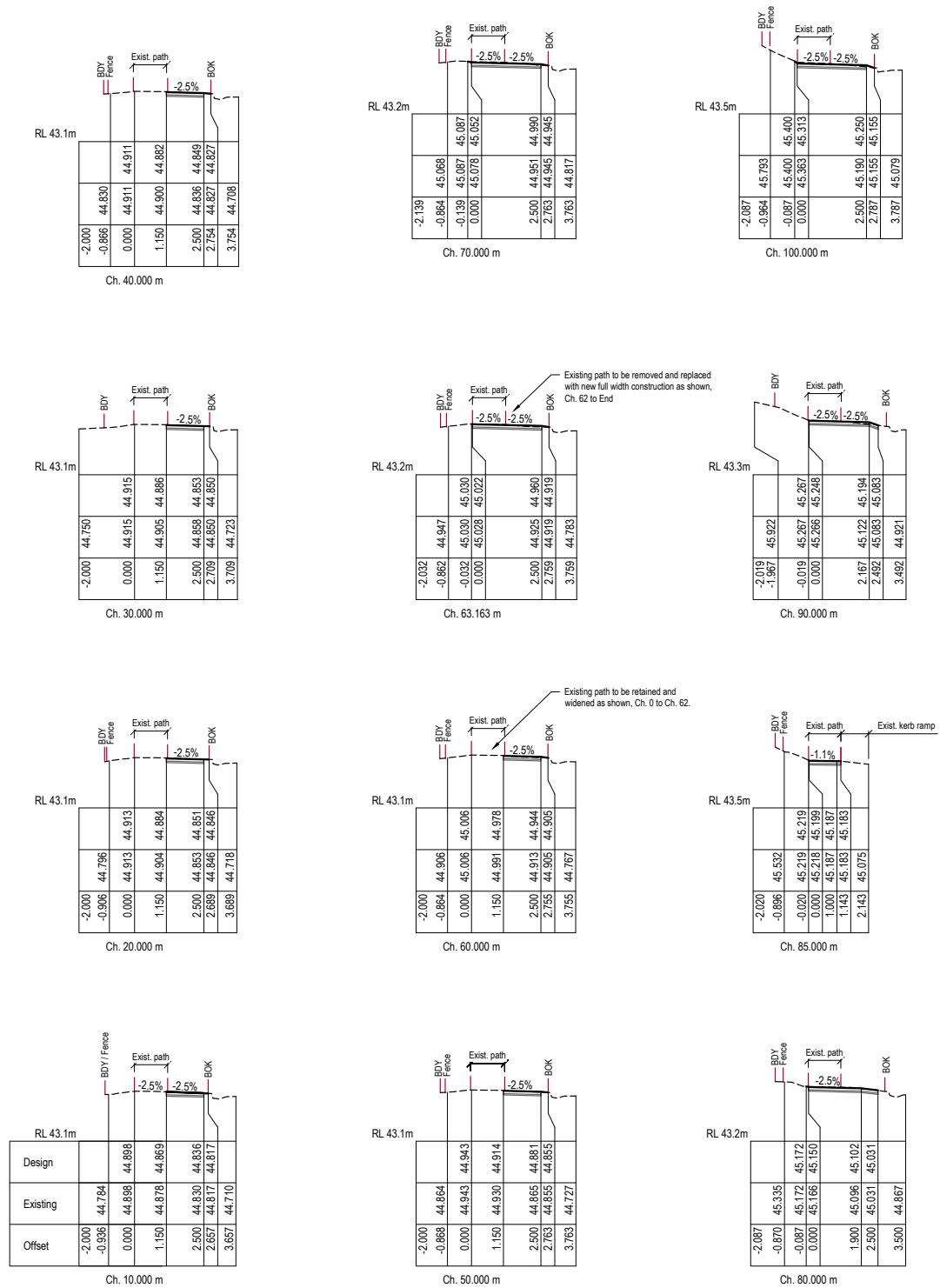
Drawing Number

Revision

3724/C220

Driveway at Ch. 111.500  
(Market Street to Bridge)

3724/C221



ISSUE FOR  
CONSTRUCTION

| Rev        | Description | Date | Des. | App. | Chk. |
|------------|-------------|------|------|------|------|
| Amendments |             |      |      |      |      |

© GeoLINK

This drawing must not be relied upon for any purpose other than that for which it was prepared or by any person or corporation other than the referred client.



www.geolink.net.au

quality solutions sustainable future

Project Title

Bangalow Shared Path: Stage 2

Client

Byron Shire Council

|          |     |       |           |         |     |
|----------|-----|-------|-----------|---------|-----|
| Designed | MVE | Drawn | MVE       | Checked | JLC |
| Approved | JLC | Date  | 9/09/2022 |         |     |
| XREFs    |     |       |           |         |     |

Scale

metres 0 1 2 3 4 5

Drawing Title

Cross Sections  
Station Lane to Market Street

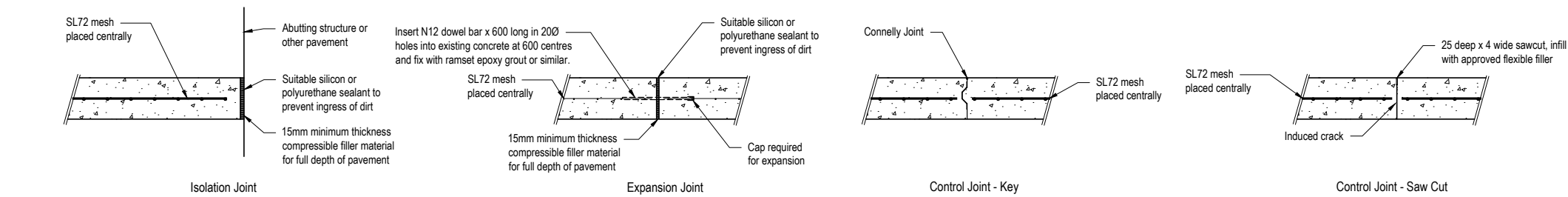
3724-1035

Drawing Number

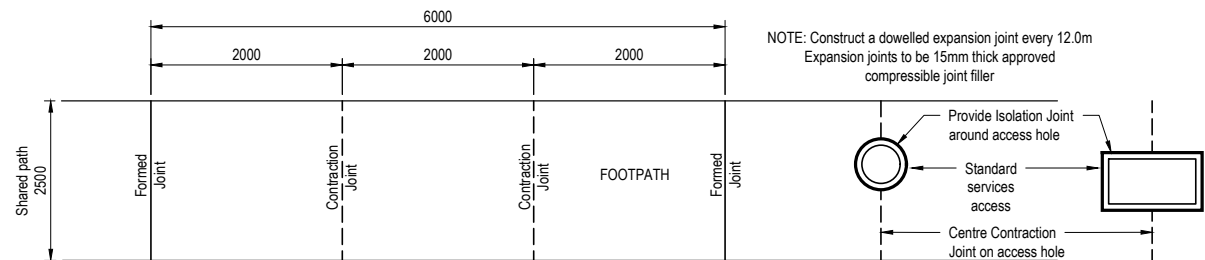
Revision

3724/C230

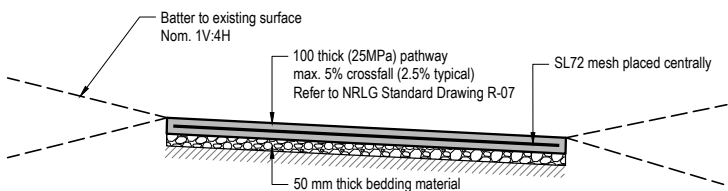




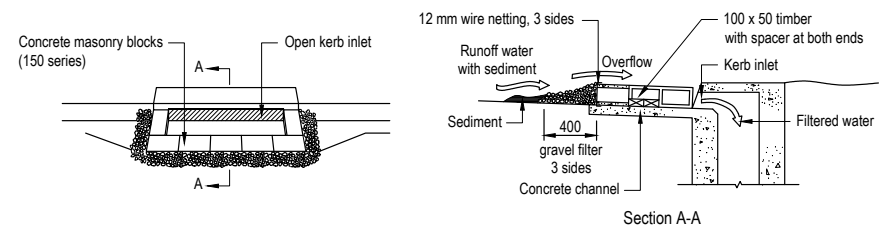
Typical Concrete Joint Details



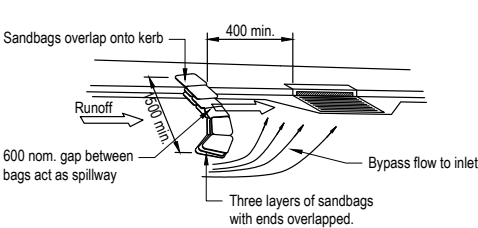
Typical Concrete Joint Constructions



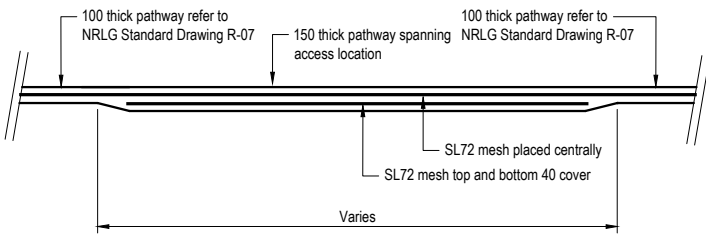
2.5 m Typical Shared Path Section



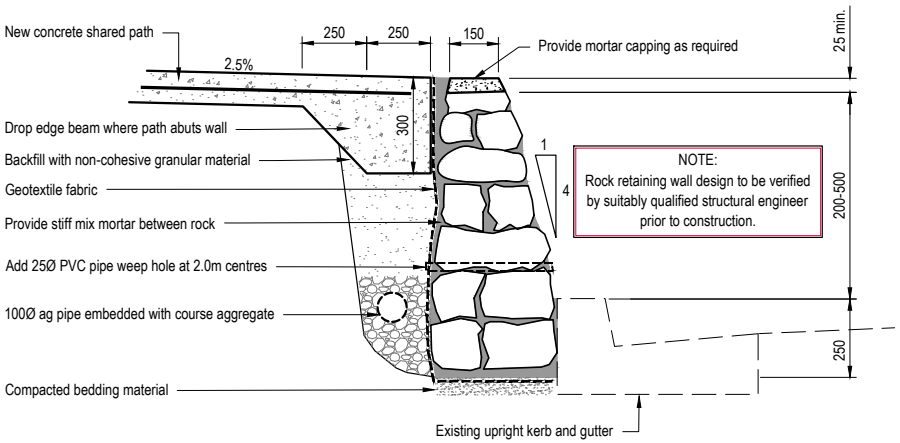
Sag Inlet Sediment Trap



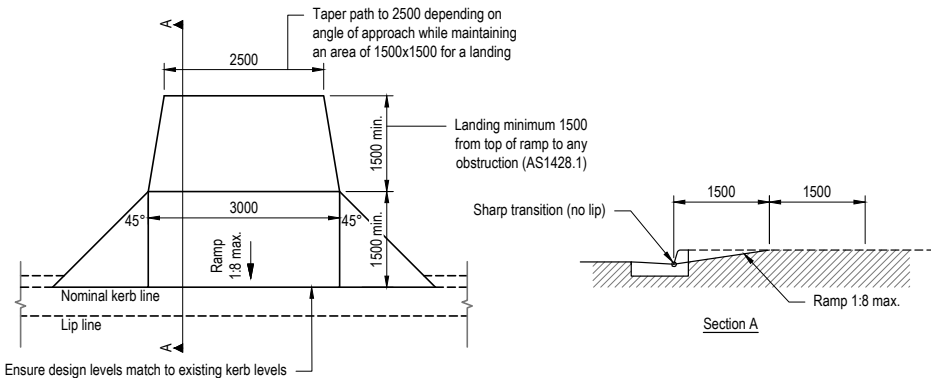
On-Grade Kerb Inlet Sediment Trap



Shared Path Thickening - Driveway Crossing



Rock and Concrete Mortar Retaining Wall  
To be constructed Ch. 5.0 to 30, Ch. 44 to 74, and Ch. 85 to 108.5  
Note: Rock to match existing rock walls on Byron Street Ch. 115 to 160.



Typical Kerb Ramp Detail  
Note: Refer to NRLG Std Dwg R-04

ISSUE FOR CONSTRUCTION

| Rev | Description | Date | Des. | App. | Chk. |
|-----|-------------|------|------|------|------|
|     |             |      |      |      |      |

© GeoLINK

This drawing must not be relied upon for any purpose other than that for which it was prepared or by any person or corporation other than the referred client.

**GeoLINK**  
environment | engineering | planning | design  
www.geolink.net.au

**Lennox Head Office**  
PO Box 119  
Lennox Head NSW 2478  
Tel: 02 6687 7666

**Coffs Harbour Office**  
PO Box 1446  
Coffs Harbour NSW 2450  
Tel: 02 6651 7666

**Armidale Office**  
PO Box 1207  
Armidale NSW 2350  
Tel: 02 6772 0454

**Lismore Office**  
PO Box 229  
Lismore NSW 2480  
Tel: 02 6621 6677

Email: info@geolink.net.au

quality solutions sustainable future

Project Title  
Bangalow Shared Path: Stage 2

Client  
Byron Shire Council

|          |     |       |           |         |     |
|----------|-----|-------|-----------|---------|-----|
| Designed | MVE | Drawn | MVE       | Checked | JLC |
| Approved | JLC | Date  | 9/09/2022 |         |     |

XREFs  
Scale  
metres Not To Scale

Drawing Title  
Typical Details

3724-1035  
Drawing Number Revision  
3724/C240

These notes shall be read in conjunction with other specified documents. Where a discrepancy occurs between any documents such discrepancy shall be referred to the superintendent before proceeding with construction.

### General

- These drawings shall be read with full consideration of all the specifications, acts, regulations, standards, codes of practice and guidelines, both specifically referred to herein and implied, and with other such written instructions as may be issued during the course of the contract. The contractor is responsible for ensuring they, and all site workers as necessary, have access to the relevant specifications, acts, regulations, standards, codes of practice and guidelines, including the NRLG Specifications and are aware of the requirements and obligations set out therein. Any discrepancies shall be referred to the Superintendent before proceeding with the work.
- All materials and workmanship shall be in accordance with the relevant and current Australian Standards, by-laws and ordinances of the relevant building authorities, and local government specifications.
- All dimensions shown shall be verified by the Contractor on site. The drawings shall not be scaled for dimensions.
- Unless otherwise noted all levels are in metres, and dimensions shown in millimetres.
- The Contractor must ensure they are satisfied as to the site conditions and the requirements of the work. No claim whatsoever will be considered as a consequence of the failure of the Contractor to have made a full and comprehensive investigation of the site and assessment of the conditions likely to affect the works.
- The Contractor shall notify all relevant Local and Public Authorities of any works carried out that may affect those authorities. The Contractor must ensure they are fully aware of the requirements of all relevant authorities such as hold points, testing and analysis, approvals, and inspections of work in progress and meet the requirements of all relevant authorities.
- The contractor shall maintain a full set of the approved Drawings on site throughout the construction period, protect them from damage and make them available whenever required.
- Existing underground facilities may not be shown on the drawings or may be in locations or levels different from those shown on the drawings. The Contractor shall be responsible for ascertaining the exact location of each underground facility prior to undertaking any work that may damage such facility.
- When trade or propriety names, brands, catalogue or reference numbers are nominated, the Contractor shall use such equipment, services unless written approval is obtained from the Superintendent for the Contractor to supply a substitute material or equipment of identical characteristics of type, quality, appearance, finish, method of construction, and/or performance.
- Explosives of any form are NOT permitted on or near this site.
- Plant and equipment stored on site shall be the sole responsibility of the Contractor. A compound for plant and equipment can be established on the property beyond the construction site area subject to the approval of the Superintendent having regard to environmental and operational issues. Upon completion of the works the Contractor shall restore the site compound to a condition acceptable to the Superintendent.
- Unless otherwise agreed or stated in the Specification or Contract (where applicable), the Contractor shall give one clear working day's notice to the Superintendent, in writing, of the construction operations which require examination, inspection or testing under the Contract.
- Unless otherwise stated, all materials, plant, equipment, fixtures and other items salvaged from the Site of the works shall be the property of the Principal and shall not be removed from the Site without the prior approval of the Superintendent.
- Other contractors may be working on the site during the works period. It shall be the contractor's responsibility to liaise with all contractors and ensure that any interference between adjacent works are kept to a minimum, and that all contractors are informed when works (that may affect other contracts) are proposed to be implemented.

### Glossary of Terms

- AS - Australian Standard
- Contractor - is defined to include or be synonymous with 'builder' or 'contractor'
- NRLG - Northern Rivers Local Government Construction Manual
- Superintendent - is defined to include or be synonymous with 'engineer', 'supervisor', or 'nominated overseer'

### Safety Requirements

#### General

- The Contractor shall ensure that all the relevant requirements of the Work Health and Safety Act - 2011 (WHSa) and Work Health and Safety Regulation - 2011 (WHSR), and those codes of practice, statutory requirements and Australian Standards pertaining to workplace safety, are implemented and maintained throughout the works site and for the entire duration of the works.
- The Contractor shall appoint a Representative or Committee as requested in accordance with the WHSA and WHSR, and provide the name(s), address(es) and contact phone number(s) of the Representative or Committee to the Superintendent in writing upon their appointment.

### Local Amenity

#### Construction Times

- Construction work must not unreasonably interfere with the amenity of the neighbourhood. Hours of work involving construction noise audible on adjoining premises shall be restricted to:
  - Monday to Friday, from 7 am to 6 pm;
  - Saturday, from 8 am to 1 pm; and
  - No construction work to take place on Sundays or Public Holidays.

#### Construction Noise

- Construction noise is to be limited as follows:
  - For construction periods of four (4) weeks and under, the L10 noise level measured over a period of not less than fifteen (15) minutes when the construction site is in operation must not exceed the background level by more than 20 dB(A).
  - For construction periods greater than four (4) weeks and not exceeding twenty-six (26) weeks, the L10 noise level measured over a period of not less than fifteen (15) minutes when the construction site is in operation must not exceed the background level by more than 10 dB(A).

### Survey Setout

- The Contractor shall at their own expense set out the Works correctly in accordance with the Contract and the drawings and shall provide all instruments and materials necessary for that purpose.
- If at any time during the progress of the work, any error is discovered in the position, level, dimensions or alignment of any part thereof, the Contractor shall immediately notify the Superintendent and shall, unless the Superintendent otherwise directs, rectify the error in accordance with the General Conditions of Contract.
- The Contractor shall preserve and maintain in their true position all State Survey Marks (SSM) and Permanent Marks (PM) whether or not the marks are to be used for the purpose of setting out, checking or measuring the work under the Contract.
- Should any SSM or PM be disturbed or obliterated, the Contractor shall immediately notify the Superintendent and shall arrange for a Registered Surveyor to rectify such disturbance or obliteration. Unless the disturbance or obliteration has been caused by the principle, his employees or agents, the cost of rectification shall be borne by the Contractor. Such notice shall include the requirements of the Local Council for undertaking inspection of the work in progress. The Contractor shall be fully responsible for making himself aware of Council's requirements in this regard and for making all necessary provisions for such Council inspections.

### Traffic Management

- Where works occur in public areas, road reserves or private developments it shall be the Contractor's responsibility to ensure the safety of pedestrian and vehicular traffic within the works area.
- The Contractor shall ensure that the approved Traffic Management Plans prepared by an accredited traffic consultant, is strictly adhered to during the construction period.

### Environmental Management

#### General

- All work shall be undertaken so as to avoid damage to the environment, including erosion, contamination and sedimentation of the site, surrounding country, watercourses and streams.
- The Contractor shall comply with the requirements of environmental legislation enacted by Federal, State and Local governments
- Necessary vegetation clearing and grubbing shall be in accordance relevant specifications, requirements of authorities and the direction of the Superintendent.

#### Toxic Substances and Contaminants

- Herbicides and other toxic chemicals shall NOT be used without prior written permission of the Superintendent.
- The Contractor shall give written notice to the Superintendent 14 days prior to any toxic substances entering the Site, comply with statutory requirements and take proper precautions to keep poison and other injurious substances in places secured against access by unauthorised persons, children and animals.
- The Contractor shall dispose of all solid, liquid and gaseous contaminants in accordance with all statutory requirements and to locations off Site.
- The Contractor shall service and maintain vehicles and machinery at locations approved by the Superintendent.

#### Sediment and Erosion Control

- All erosion and sediment controls are to be in accordance with the *Soils and Construction Volume 1 - Managing Urban Stormwater 4th Ed.* (the 'Blue Book').
- All sub contractors are to be informed of their responsibilities in relation to erosion and sedimentation.
- Accidentally disturbed areas would be stabilised and/or re-vegetated or secured with geofabric as soon as practical after the damage has occurred.
- Works to cease if pollution problems are suspected or detected.
- Safety issues must be considered at all times; incorporate traffic control devices to the satisfaction of the superintendent.
- The weather is to be monitored during the works. Works shall be scheduled outside of forecasted significant rain events and postponed during unforeseen rain events.
- Pavement formations and any excavation works are to be scheduled outside of average high rainfall periods.
- Works shall cease and all sediment control measures checked and repaired/re-installed (if required) if heavy rainfall forecast.
- Sediment control features would be checked as soon as practical (within 24 hours) after significant rainfall events.

### Earthworks

#### General

- Earthworks includes the following works:
  - Removal of topsoil
  - All activities and quality requirements associated with site regrading, excavation, haulage of material and filling
  - Removal and replacement of any unsuitable material from below the work defined for general earthworks; and
  - Any associated spoil or borrow activities.
- The Contractor shall undertake all inspection and testing necessary to demonstrate that the quality requirements of this specification have been achieved. All earthworks are to be carried out in accordance with NRLG Specification C213.

#### Nature of Earthworks Materials

- The Contractor shall be responsible for any assumptions made in relation to the nature and types of materials encountered in excavation and the bulking and compaction characteristics of the materials incorporated in filling operations.

#### Protection of Earthworks

- The Contractor's responsibility for care of the Works shall include the protection of earthworks.
- Prior to commencement of earthworks operations, the Contractor shall install and maintain effective erosion and sedimentation control measures to the approval of the superintendent.
- Adequate drainage of all working areas shall be maintained throughout the period of construction to ensure run-off of water without ponding, except where ponding forms part of a planned erosion and sediment control system.
- When rain is likely or when work is not proposed to continue in a working area on the following day, precautions shall be taken to minimise ingress of any excess water into earthworks material. Ripped material remaining in cuts and material placed as fill shall be sealed off with a smooth roller.
- Should in situ or stockpiled material become over-wet as a result of the Contractor not providing adequate protection of earthworks, the Contractor shall be responsible for replacing and/or drying out the material and for any consequent delays.

### Path Construction

- Construct shared path and ancillary works to details shown.
- All materials and workmanship shall be in accordance with the current Australian Standards, NRLG Guidelines and Standard Drawings and Austroads Specifications and Guidelines
- Excavate natural surface where required, trim and compact subgrade and construct concrete footpath.
- Earthworks:
  - Fill material shall be clean, well graded (ALD >50mm) with limited plasticity (PI<20), and free of organic matter.
  - Fill shall be placed in layers not exceeding 200mm loose thickness and compacted to 95% standard.
  - Side batter slopes shall not be greater than 4H : 1V uno.
  - The top 100mm of any fill area outside of the footpath shall be topsoiled and turfed.
- All concrete footpaths to be constructed in accordance with NRLG std. dwg. R-07 (current version), unless noted otherwise.
- Concrete footpath to be 25MPa at 28 days.
- Concrete footpath to have lightly broomed finished perpendicular to pedestrian traffic.
- Crossfall of footpaths is to be consistent along the path as much as practicable. A maximum crossfall of 2.5% is not to be exceeded except where specifically indicated.
- Footpath to be provided with contraction joints as per detail to induce controlled cracking. The joint shall be formed in the freshly placed concrete perpendicularly across the footpath at intervals shown.
- Match new path to existing concrete driveways and levels unless otherwise stated. Dowel to existing as per detail provided and NRLG Standard Drawing R-07.
- Trim all trees with overhanging limbs 2.4m above footpath to provide adequate headroom clearance and clear edge of footpath by min. 0.5m.
- Construct path to height of adjacent pits and services where levels permit. Adjust Telstra Pits, Water Valve Covers and Boxes, Sewer Manholes, and Stormwater Pit Covers to match new path level as required. All services to be provided with isolation joints as per detail.
- All signage and pavement markings are to be in accordance with AS 1742.9. All permanent signs are to be supplied by Byron Shire Council and installed by contractor.
- Once completed, the path is to be fully inspected to ensure no trip hazards are present, and all edges have been backfilled.
- Restoration:
  - All distributed areas are to be reinstated to their original line and matching in to new construction.
  - The restored surface shall be free of all stones (exceeding 20mm).
  - All distributed surfaces or areas of fill are to be sown with an even distribution of seed and fertilizer at the rates specified in NRLG Annexure C273A.

### Holding Rails and Signage

- Holding rails to be placed on the left hand side of the path, 600 from the kerb line and 300 from the path
- Installation of signs and line marking to comply with AS1742
- Signs which are to be mounted adjacent to bicycle paths or footpaths separate from vehicular roadways should be located as follows:
  - Lateral Placement - Signs should be at least 600mm clear of the outer edge of the facility
  - Mounting height - Signs should be mounted a minimum of 2m above the ground surface
  - Overhead signs - Signs may be placed above bicycle paths, provided the minimum vertical clearance is 2.5m and the supporting structure conforms with the Lateral Placement (above)

### Concrete

- All workmanship and materials shall be in accordance with AS 3600 and AS 3610 current editions.
- Concrete quality:
  - strength grade 25 MPa
  - nominal aggregate size 20mm
  - slump 80mm
- All cement shall be sulphate resistant
- No admixtures shall be used in concrete unless approved in writing.
- Clear concrete cover to all reinforcement shall be as follows unless shown otherwise (mm)
  - slabs, top cover to mesh. .... 40
  - elsewhere ..... 50
  - surfaces in contact with sewerage or groundwater. ... 65
- All concrete shall be compacted using mechanical vibrators.
- Construction joints where not shown shall be located to the approval of the Superintendent.
- All concrete surfaces to be cured by an approved method for 7 days.
- SL denotes grade 500 SL hard drawn wire reinforcing fabric to AS 1304.
- Except as otherwise required, exposed concrete corners and edges shall have 20 mm chamfers

### Acceptance Testing of Works

#### General

- The Contractor shall arrange for an approved testing authority to undertake the testing of path subgrades, pavements and concrete slabs, and shall comply with the testing methods and frequencies specified in NRLG Specification CQC.

#### Testing Authority

- All acceptance testing shall be carried out by a testing authority certified by the National Association of Testing Authorities (NATA) to conduct the specified tests.
- Test results shall be endorsed by the NATA certified authority which conducts the tests.
- The authority conducting the test shall submit a copy of the test result(s) and/or report direct to the Superintendent with the original sent to the Contractor.
- The Contractor shall supply full details of the testing requirements in the Contract to the testing authority, prior to commencement of work under the Contract.
- The Contractor shall supply a copy of the Contractor's letter commissioning the testing authority to the Superintendent within seven days of the start of the commission.

#### Testing

- The Contractor shall liaise with the testing authority and arrange for all acceptance testing.
- The Contractor shall, unless agreed or stated otherwise in the Specification give one clear working days written notification to the Superintendent before carrying out acceptance testing.
- Tests and/or samples shall be representative of the work or material which is being tested for quality and/or acceptance.
- The Contractor shall arrange for retesting the work when the Superintendent considers that the test and/or samples are not representative of the material or work.
- The Contractor shall arrange to carry out testing progressively as the work proceeds.
- The locations of all tests in plan and elevation shall be shown on test certificates and test reports and locate tests relative to the job datum.

#### Test Results

- The Contractor shall submit copies of the test results and not proceed with the tested portion of the work until the results are accepted by the Superintendent.
- The Contractor shall keep on Site an up-to-date record of test results for inspection by the Superintendent.

### Completion of Works

- Prior to the issue of a Certificate of Practical Completion, all surplus material and rubbish shall be removed and the whole of the site left clean and neat in appearance. Similarly, the whole work area shall be left in a neat and tidy condition at the end of the Defects Liability Period.
- Any road pavement or footpath disturbed or destroyed during construction shall be reinstated to a condition at least equal to that existing before commencement of works.
- The Contractor shall ensure that deleterious material deposited as a result of the work is removed from roads.
- Any damage to external areas of the site as a result of the works shall be reinstated by the Contractor as directed by the Superintendent.
- Any clearing, cleaning or reinstatement by the Contractor shall be carried out at no cost to the Principal.

### Maintenance / Defects Liability Period

- The Contractor shall maintain the work under the contract for the whole of the defects liability period.
- Maintenance shall include, but not be limited to:
  - Reinstatement of any subsidence in fills, trenches and excavations;
  - Repair of any erosion or other damage caused to any item of the work by the flow or passage of water on, over or through the Site; and
  - Reconstruction of any portion or item of the works which has deteriorated or no longer conforms to relevant specifications, the design, or the intent of the works.

ISSUE FOR  
CONSTRUCTION

| Rev        | Description | Date | Des. | App. | Chk. |
|------------|-------------|------|------|------|------|
|            |             |      |      |      |      |
| Amendments |             |      |      |      |      |

© GeoLINK

This drawing must not be relied upon for any purpose other than that for which it was prepared or by any person or corporation other than the referred client.

**GeoLINK**  
environment | engineering | planning | design

www.geolink.net.au

quality solutions sustainable future

Project Title

Bangalow Shared Path: Stage 2

Client

Byron Shire Council

|          |     |       |            |         |
|----------|-----|-------|------------|---------|
| Designed | MVE | Drawn | MVE        | Checked |
| Approved | JLC | Date  | 01/09/2022 | JLC     |
| XREFs    |     |       |            |         |

Scale

metres

Drawing Title

Notes and Specifications

3724-1035

Drawing Number Revision

3724/C241

FOR INFORMATION ONLY

**Report No. 7.1 Brunswick Heads Pay Parking - Notification of Completion**

5 **Directorate:** Infrastructure Services  
**Report Author:** Shelley Currie, Road Safety Officer  
**File No:** I2024/1325

10

**Purpose:**

This report is in accordance with the Transport for NSW Traffic Management and Pedestrian Works Temporary Delegation to Councils No. 2 and the requirement to notify of works completed under this delegation.


15 Attached is a record of parking signage and line marking works completed in Brunswick Heads in relation to the introduction of paid parking.

**Communicate to:**

- Transport for NSW
- NSW Police

20

**Attachments:**

1 Memo to GM - Brunswick Heads Parking Signage approval, E2024/96072 , page 93 

25

# BYRON SHIRE COUNCIL

FOR INFORMATION ONLY

7.1 - ATTACHMENT 1

Page 1 of 2

## Byron Shire Council - Memorandum

**MEMO TO:** General Manager  
**COPY TO:** Director Infrastructure Services  
**MEMO FROM:** Road Safety Officer  
**SUBJECT:** Approval to complete Brunswick Heads parking signage installation  
**DATE:** 26 September 2024  
**RECORD NO:** E2024/96072

Dear Mark,

In accordance with the Transport for NSW Traffic Management and Pedestrian Works Temporary Delegation to Councils No.2, I am seeking your approval for the installation of new parking signage and line marking to support the implementation of pay parking in Brunswick Heads, as per Council resolution 24-131 shown below.

|               |  |   |
|---------------|--|---|
|               | <b>Report No. 13.12</b><br><b>File No:</b>   | <b>Brunswick Heads Pay Parking - Implementation</b><br><b>I2024/373</b> |
| <b>24-131</b> | <b>Resolved that Council:</b> <ol style="list-style-type: none"><li>Endorses for inclusion in the draft 2024/25 Fees and Charges, a fee of \$5 per hour for the Brunswick Heads Pay Parking scheme and reiterates its earlier commitment (Resolution 23-652) to reducing the resident permit fee from \$55 to \$0 when the Brunswick Heads scheme becomes operational.</li><li>Supports Brunswick Heads Pay Parking scheme using a combination of existing cash meters and Pay by Plate cashless parking meters and other options such as phone apps</li><li>Receives further reports:<ol style="list-style-type: none"><li>discussing how the funds generated from the Brunswick Heads Pay Parking scheme could be distributed in Brunswick Heads and the Shire.</li><li>advising of different software options to improve the usability and efficiency of the permit application process</li></ol></li><li>Receives a further report on discussions with relevant Brunswick Heads community groups on:<ol style="list-style-type: none"><li>impacts on of likely overflow from avoidance of parking charges</li><li>impacts on possible revenue from various options on permits and exemptions (Hunter/Lyon)</li><li>unsealed parking areas</li></ol></li></ol> <p><i>The motion was put to the vote and carried.</i><br/><i>Crrs Ndiaye, Swivel, and Coorey voted against the motion.</i></p> |   |

The following documentation is attached for your review:

1. Bitzios Brunswick Heads Pay Parking Signage Install Plans
2. Byron Shire Council Proposed Linemarking and Signage, Brunswick Heads

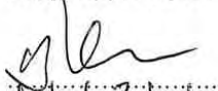
Please note these drawings have been reviewed by Ben Taylor (Council's Traffic & Transport Engineer), Jason Brook and Aaron Stockwell (Bitzios Consulting).



.....  
**Shelley Currie**

**Road Safety Officer**

  
Supported / ~~Not Supported~~

  
.....  
**Phil Holloway**

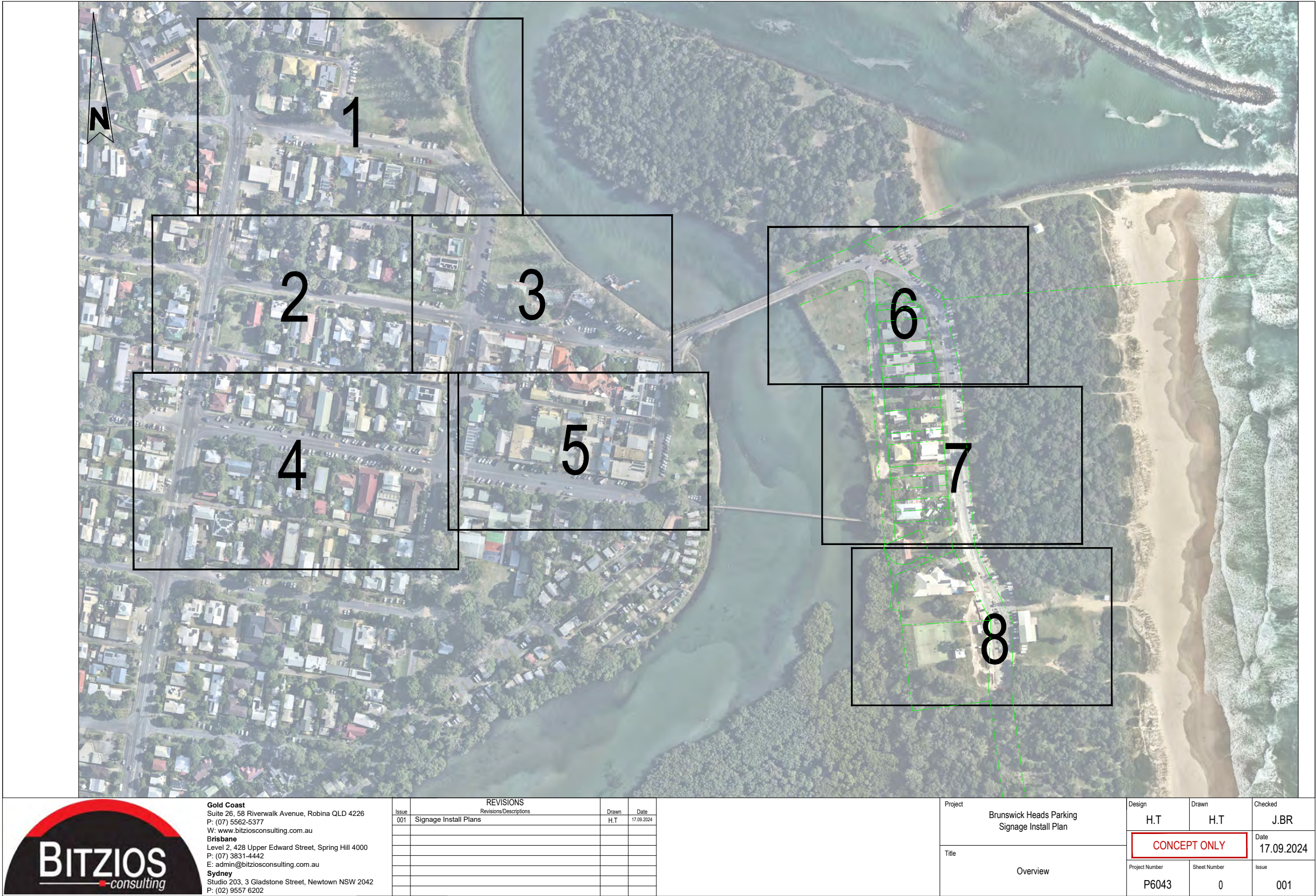
**Director Infrastructure Services**

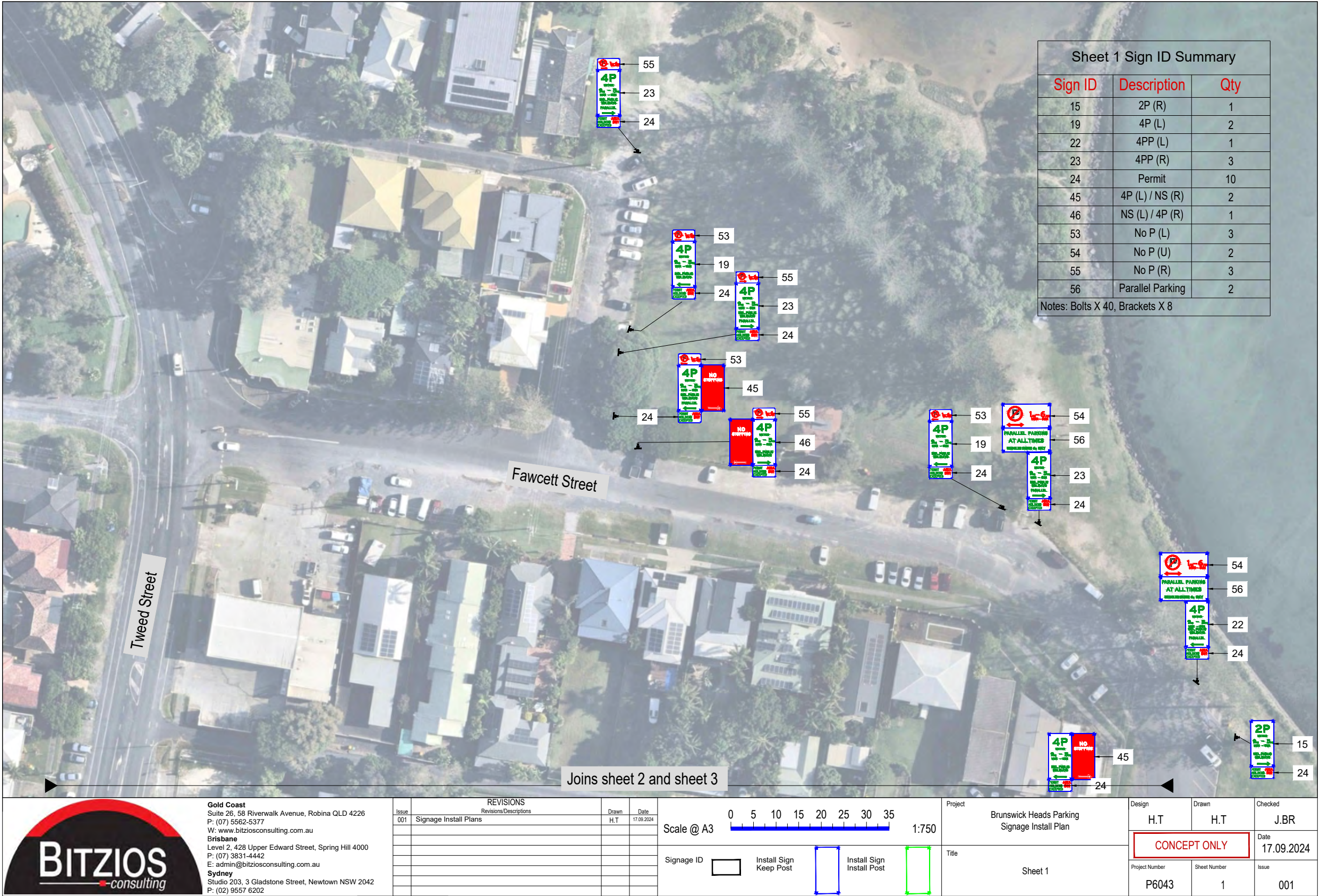
Approved / ~~Not Approved~~

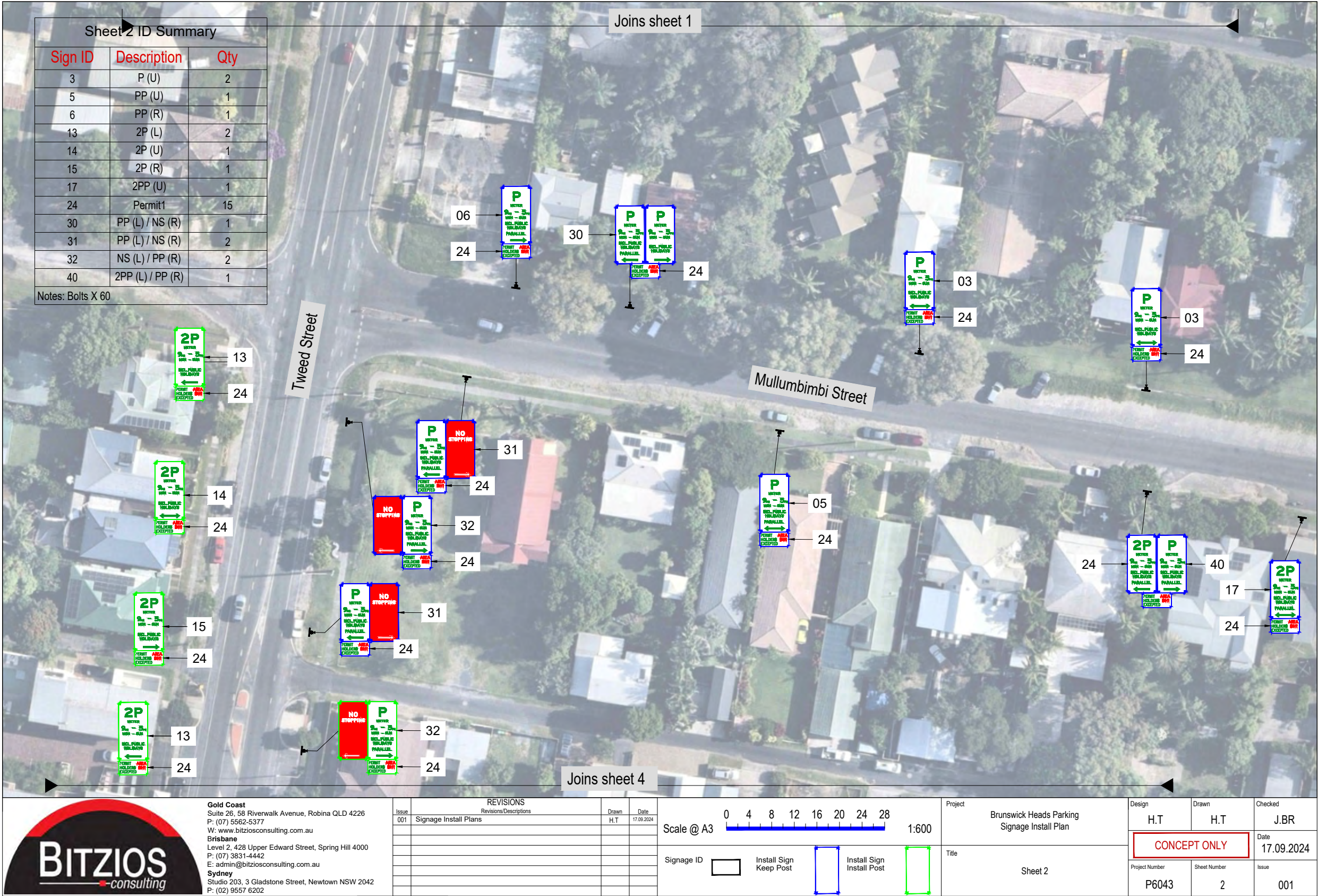
  
.....  
**Mark Arnold**

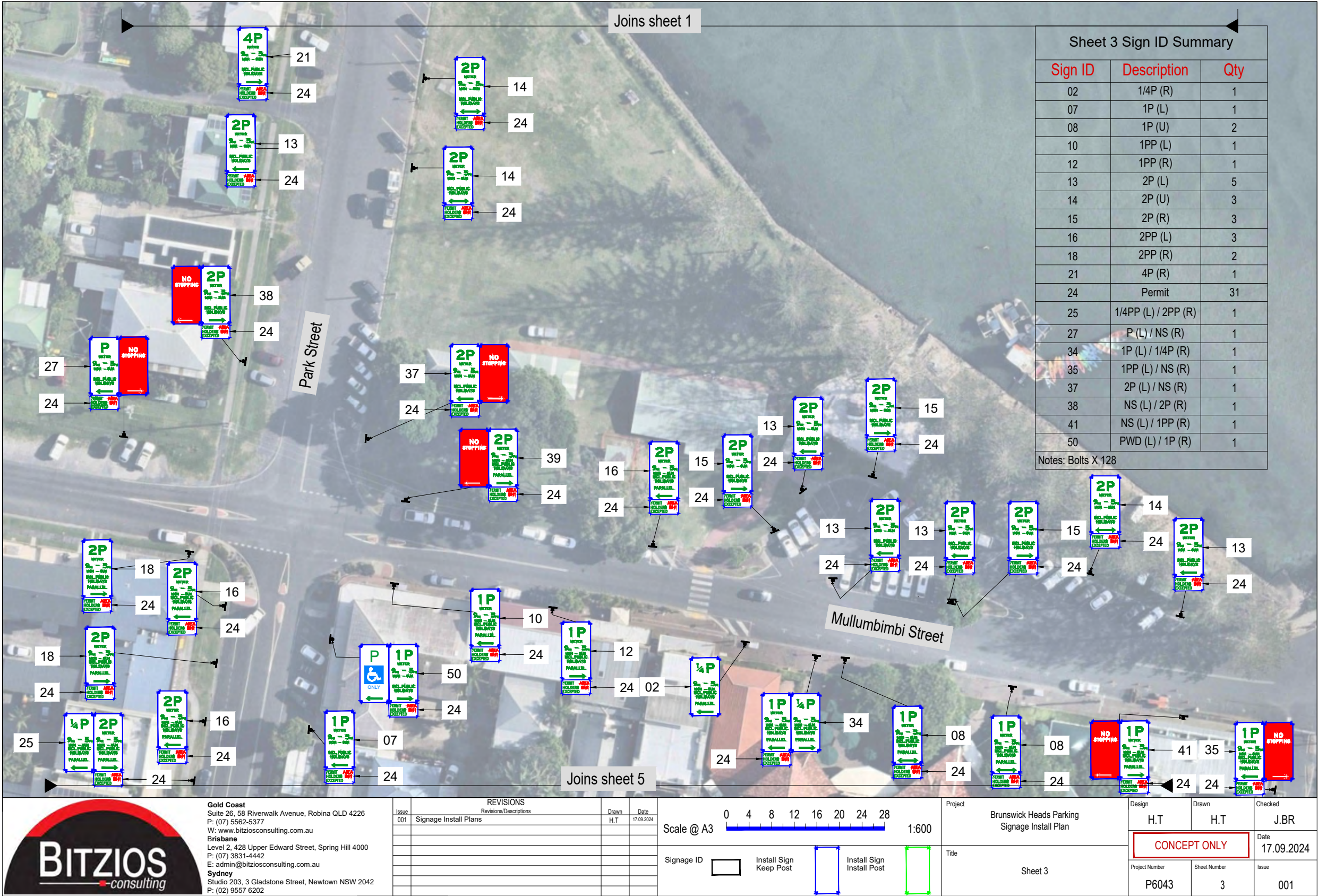
**General Manager**

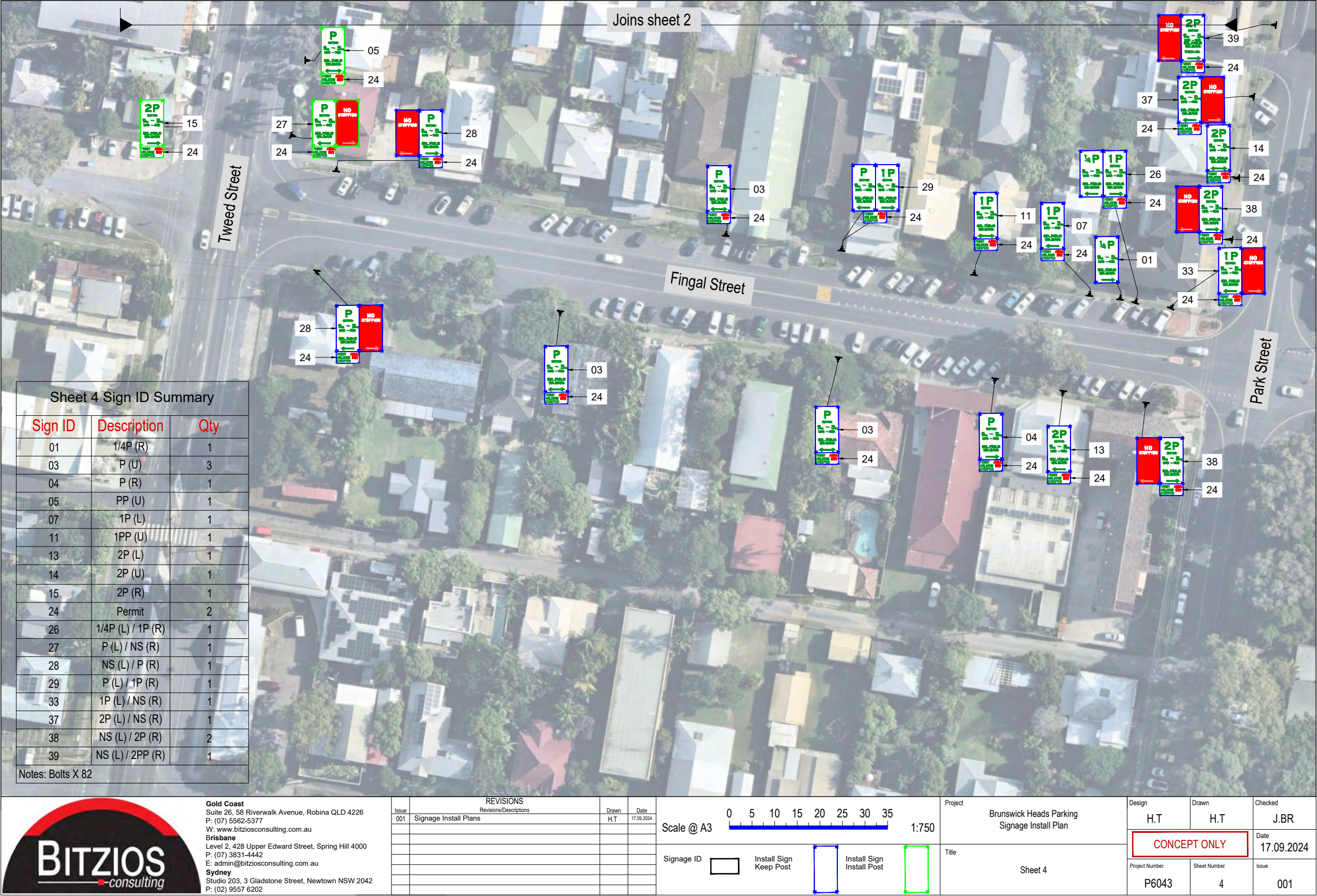
26/9/24

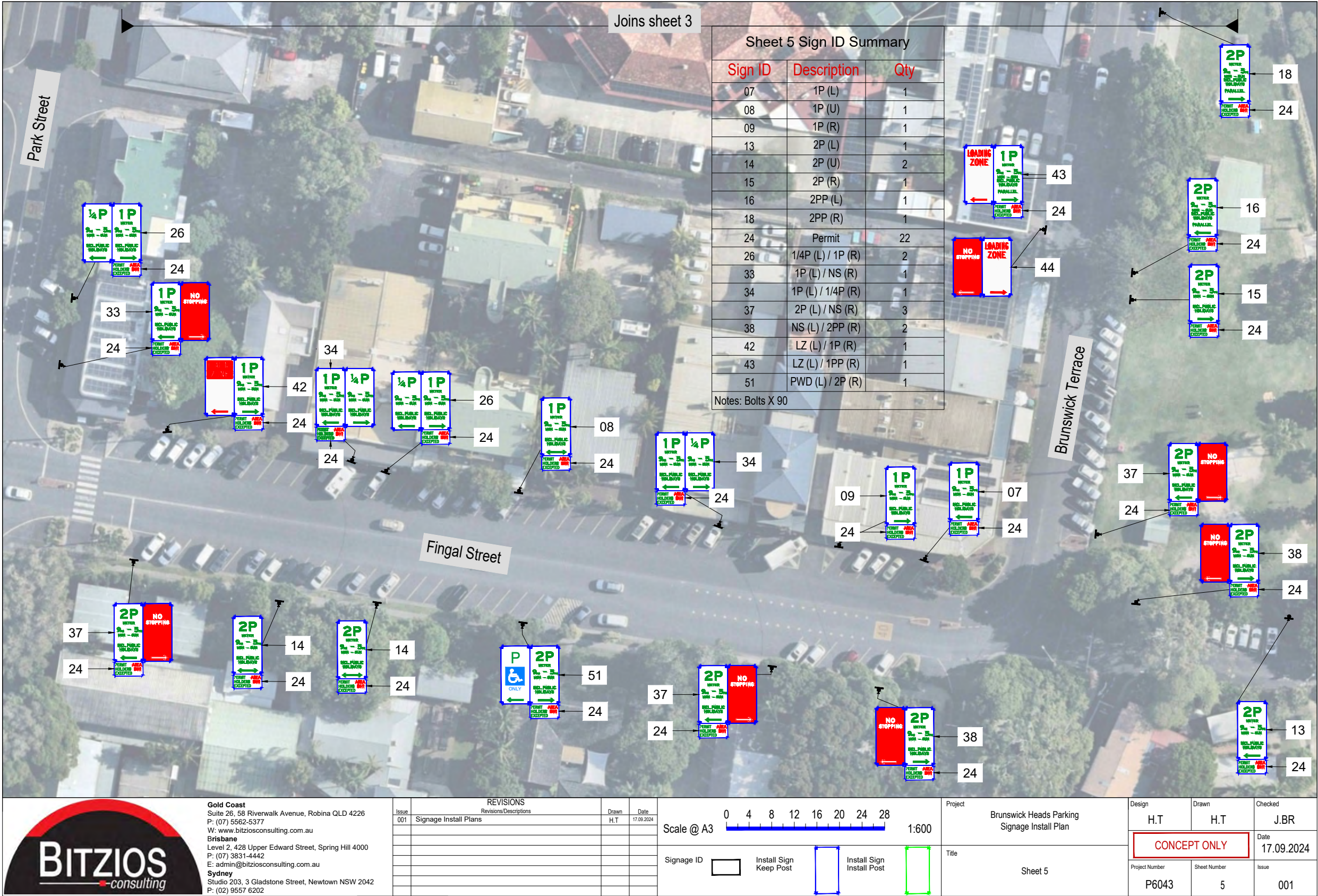


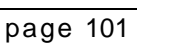


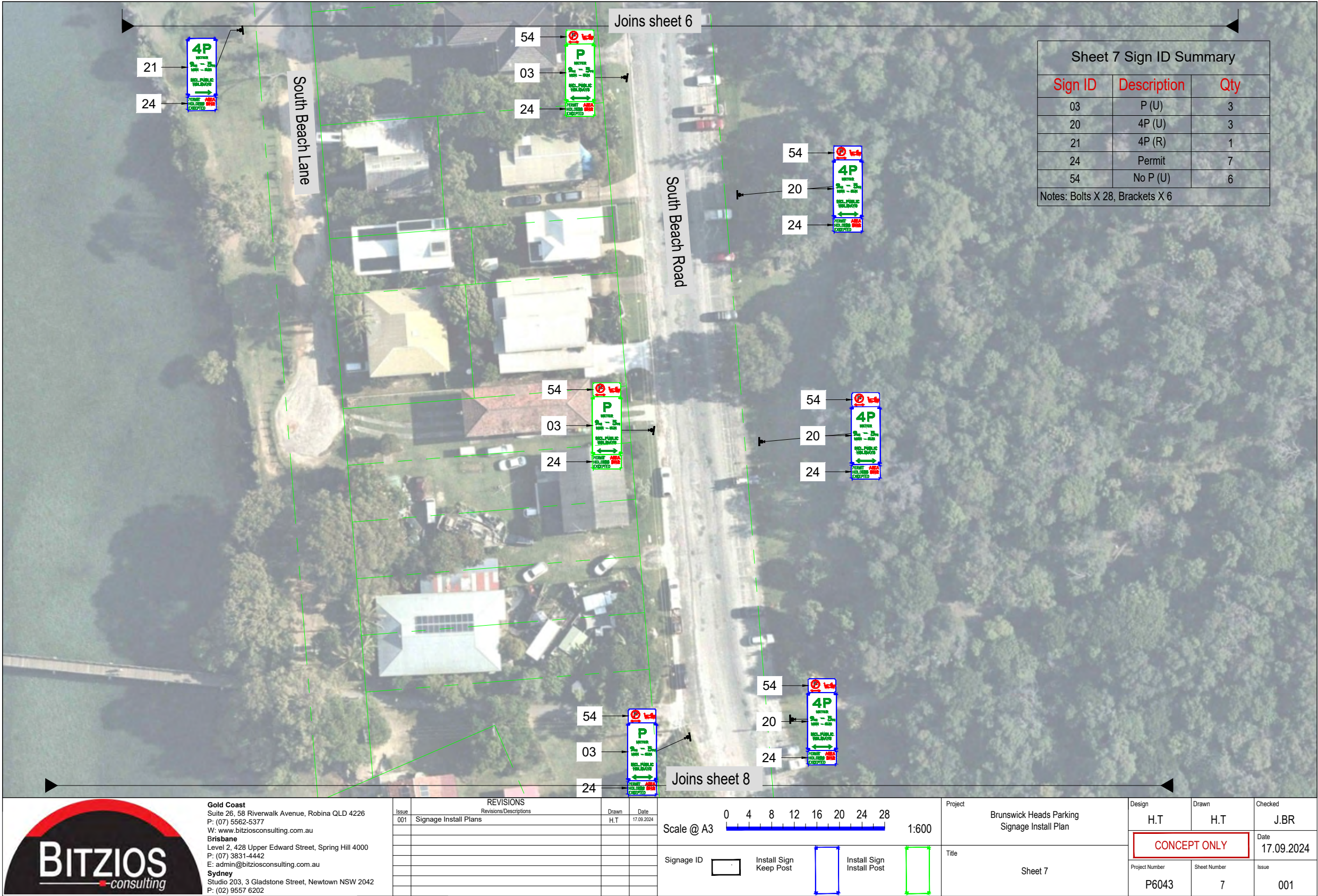


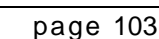




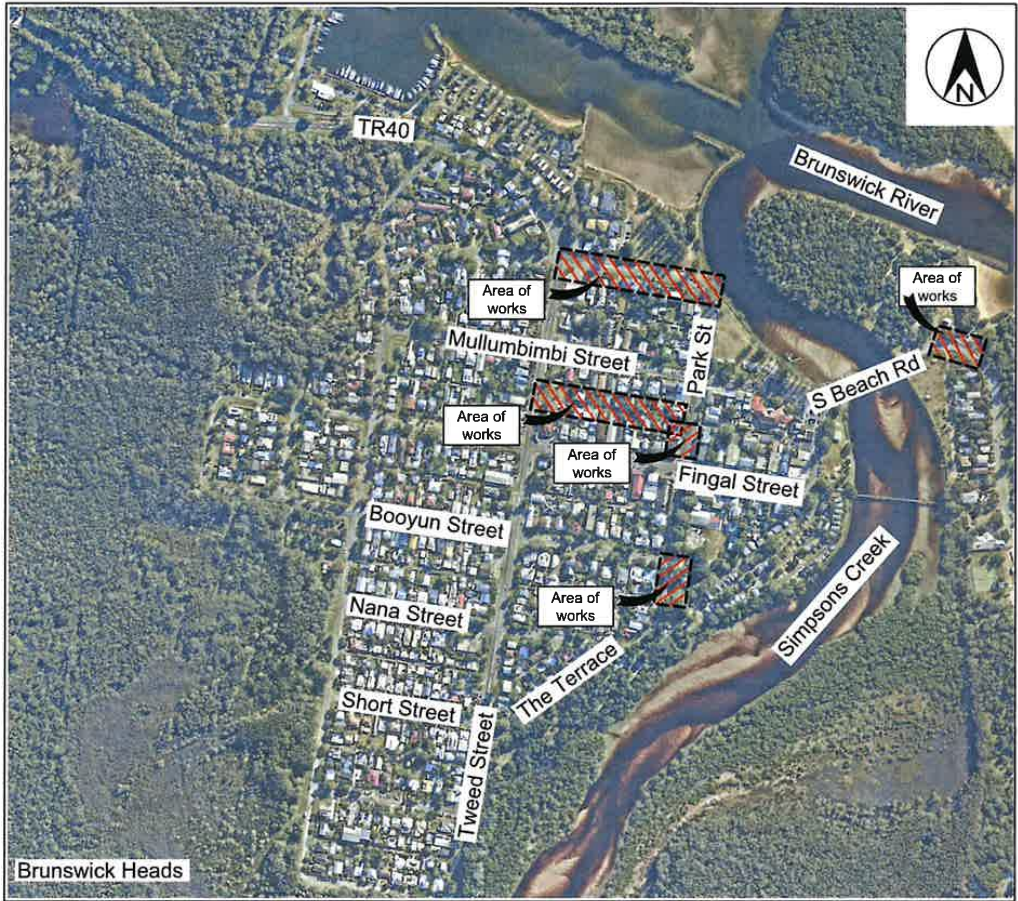








Proposed Line Marking and Signage,  
Brunswick Heads



Locality sketch

| Index                                      |         |       |
|--|---------|-------|
| Description                                | DWG No. | Issue |
| Index and Locality Sketch                  | 3091-01 | 1     |
| General Notes                              | 3091-02 | 1     |
| Sheet Layout Plan                          | 3091-03 | 1     |
| Signage Plan - Fawcett and Park Street     | 3091-04 | 1     |
| Line Marking Plan - Park Street            | 3091-05 | 1     |
| Line Marking and Signage Plan - Balun Lane | 3091-06 | 1     |
| Line Marking and Signage Plan - Balun Lane | 3091-07 | 1     |
| Signage Plan - Park Street                 | 3091-08 | 1     |
| Line Marking Plan - South Beach Road       | 3091-09 | 1     |
| Quantities                                 | 3091-10 | 1     |
|  |         |       |
|  |         |       |
|  |         |       |

**Legend**  
Issue A, B, C, etc. = Preliminary approvals / tender drawings (NOT FOR CONSTRUCTION)  
Issue 1, 2, 3, etc. = Construction issue drawings

ISSUED FOR CONSTRUCTION  
DATE 23/09/24

☐ Project has been constructed in accordance with these plans

OR

☐ Project has been constructed with departures from these plans as shown marked in red

.....

Construction Engineer

Date: .....

Approval

on behalf of the General Manager

Director Infrastructure Services

Date: 23.09.24

|                               |       |
|-------------------------------|-------|
| Project Pulse Number:         |       |
| Plan Register Number:<br>3091 |       |
| Drawing number                | Issue |
| 3091-01                       | 1     |

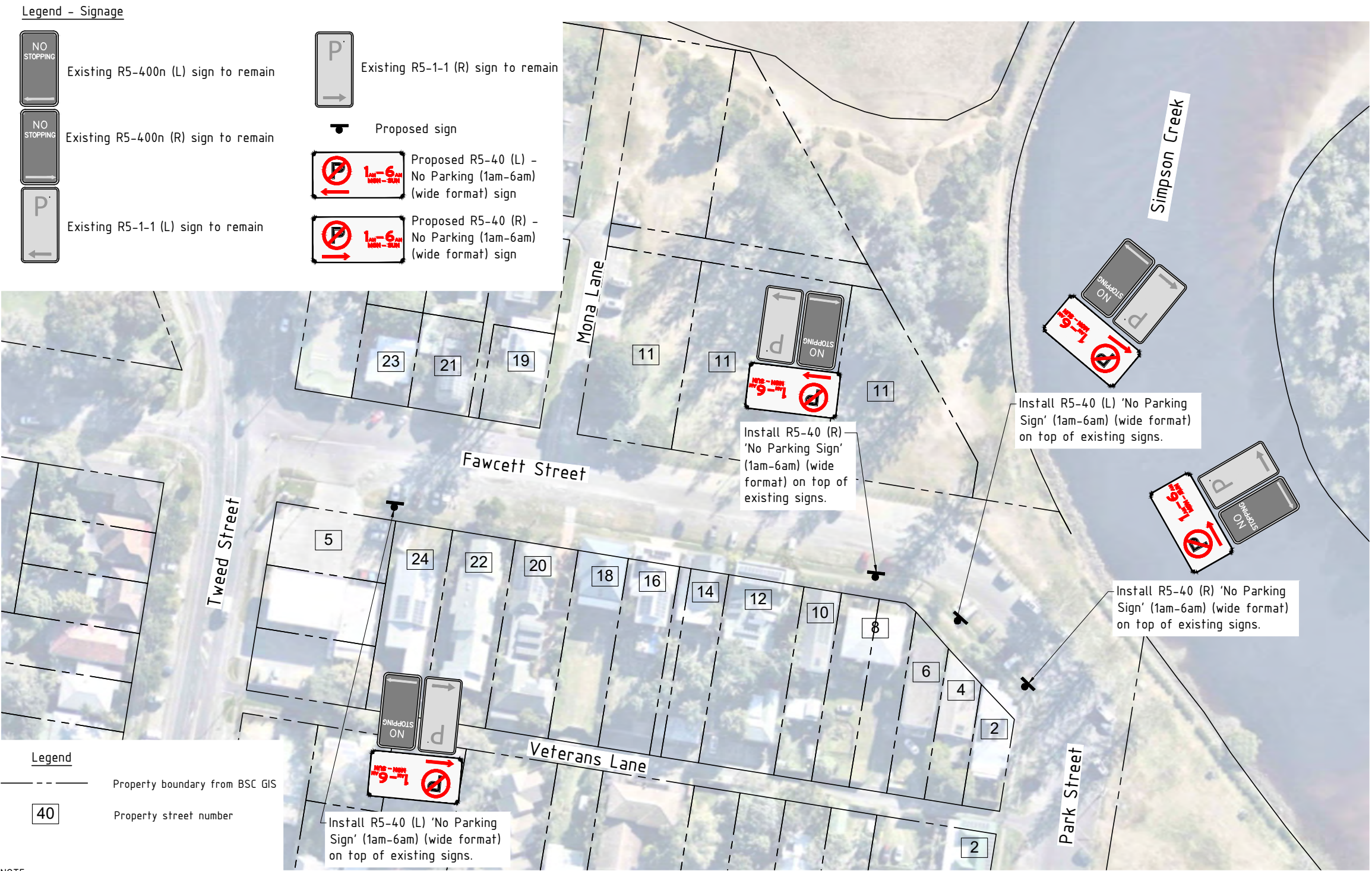
ACAD FILE No: G:\Engineer\CAD\3000-3999\3091 Brunswick Linemarking and Signage\Civil Design\DWG\CONSTRUCTION\3091 CONSTRUCTION Brunswick L&S Plan.dwg

Agenda 19 November 2024 page 105

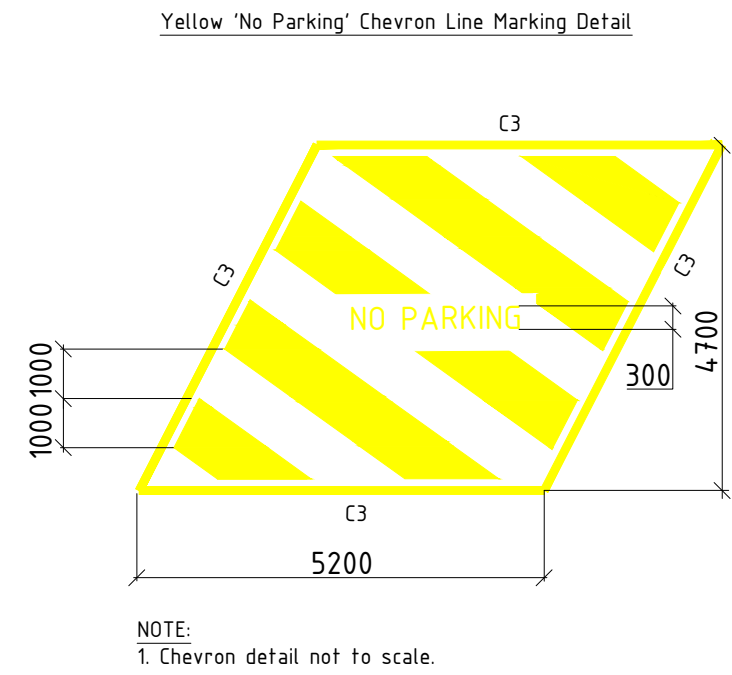


|  |  |  |  |   |  |   |  |   |  |  |  |  |  |                                      |  |
|--|--|--|--|---|--|---|--|---|--|--|--|--|--|--------------------------------------|--|
|  |  |  |  | SCALE: A1 SHEET 1:2000, A3 SHEET 1:4000 |  | <b>Infrastructure Services</b><br>Council offices<br>70-90 Station Street,<br>Mullumbimby NSW 2482.<br>Phone 02 66267000<br>Fax 02 66843018<br>Website www.byron.nsw.gov.au |  | <br><b>BYRON SHIRE COUNCIL</b> |  | Approved on behalf of the General Manager<br> Date 23.09.24 |  | Project:<br><b>Proposed Line Marking and Signage Brunswick Heads</b> |  | Project Pulse Number:                |  |
|  |  |  |  |   |  |   |  |   |  |  |  | Plan title:<br><b>Sheet Layout Plan</b>                              |  | Plan Register Number:<br><b>3091</b> |  |
|  |  |  |  |   |  |   |  |   |  |  |  |  |  | Drawing number<br><b>3091-03</b>     |  |
|  |  |  |  |   |  |   |  |   |  |  |  |  |  | Issue<br><b>1</b>                    |  |

ACAD FILE No: G:\Engineer\CAD\3000-3999\3091 Brunswick Linemarking and Signage\Civil Design\DWG\CONSTRUCTION\3091 CONSTRUCTION Brunswick L&S Plan.dwg




|   |  |  |  |                    |  |  |  |      |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |   |  |  |  |   |  |  |  |          |  |  |  |                       |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
|---|--|--|--|--------------------|--|--|--|------|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|---|--|--|--|---|--|--|--|----------|--|--|--|-----------------------|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|
|   |  |  |  |                    |  |  |  |      |  |  |  | SCALE: A1 SHEET 1:500, A3 SHEET 1:1000 |  |  |  |  |  |  |  | <div><div>Infrastructure Services</div><div>Council offices<br/>70-90 Station Street,<br/>Mullumbimby NSW 2482.</div><div>Phone 02 66267000<br/>Fax 02 66843018<br/>Website www.byron.nsw.gov.au</div></div> |  |  |  |  |  |  |  | Approved on behalf of the General Manager |  |  |  | Project: |  |  |  | Project Pulse Number: |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
|   |  |  |  |                    |  |  |  |      |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |   |  |  |  | Plan Register Number:<br>3091             |  |  |  |          |  |  |  |                       |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
|   |  |  |  |                    |  |  |  |      |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |   |  |  |  |   |  |  |  |          |  |  |  |                       |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
|   |  |  |  |                    |  |  |  |      |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |   |  |  |  |   |  |  |  |          |  |  |  |                       |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
|   |  |  |  |                    |  |  |  |      |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |   |  |  |  |   |  |  |  |          |  |  |  |                       |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
| 1 |  |  |  | Construction Issue |  |  |  | C.A. |  |  |  | A.S.                                   |  |  |  | 19.09.24   |  |  |  | # Use figured dimensions only. Do not scale.   |  |  |  |   |  |  |  |   |  |  |  |          |  |  |  |                       |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |




Legend - Line Marking

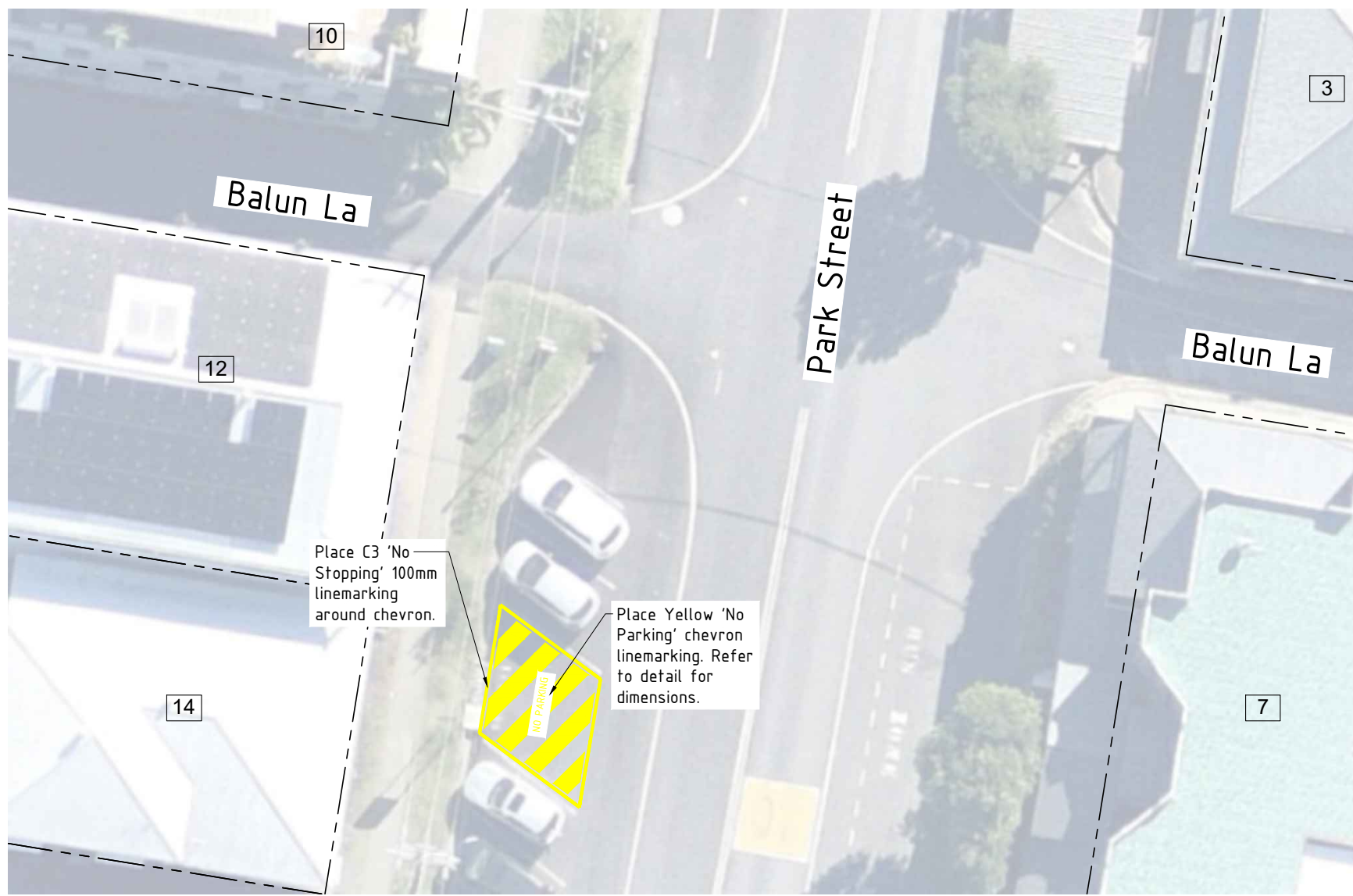
C3 No Stopping Line  100 Yellow

Legend

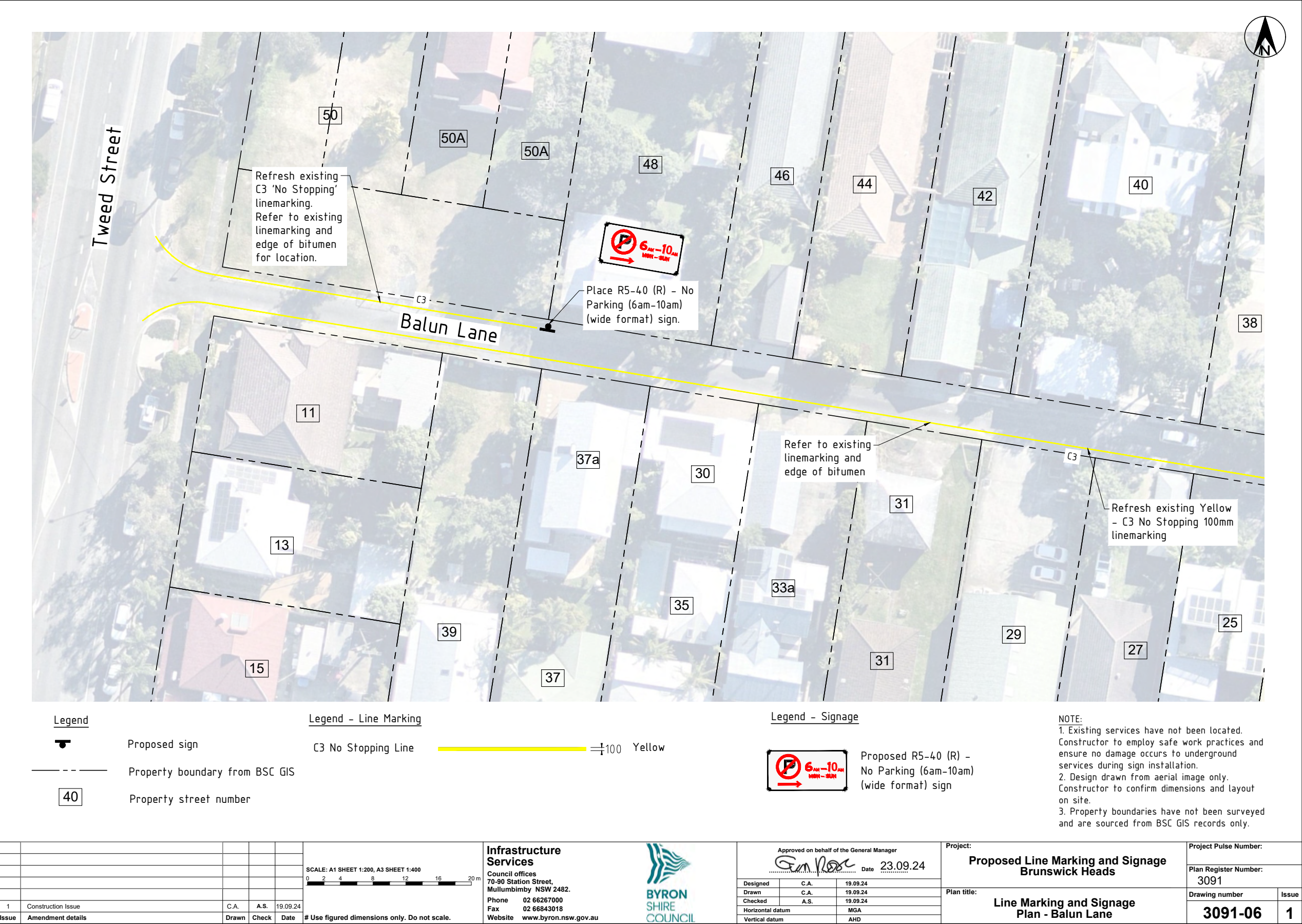
 Property boundary from BSC GIS

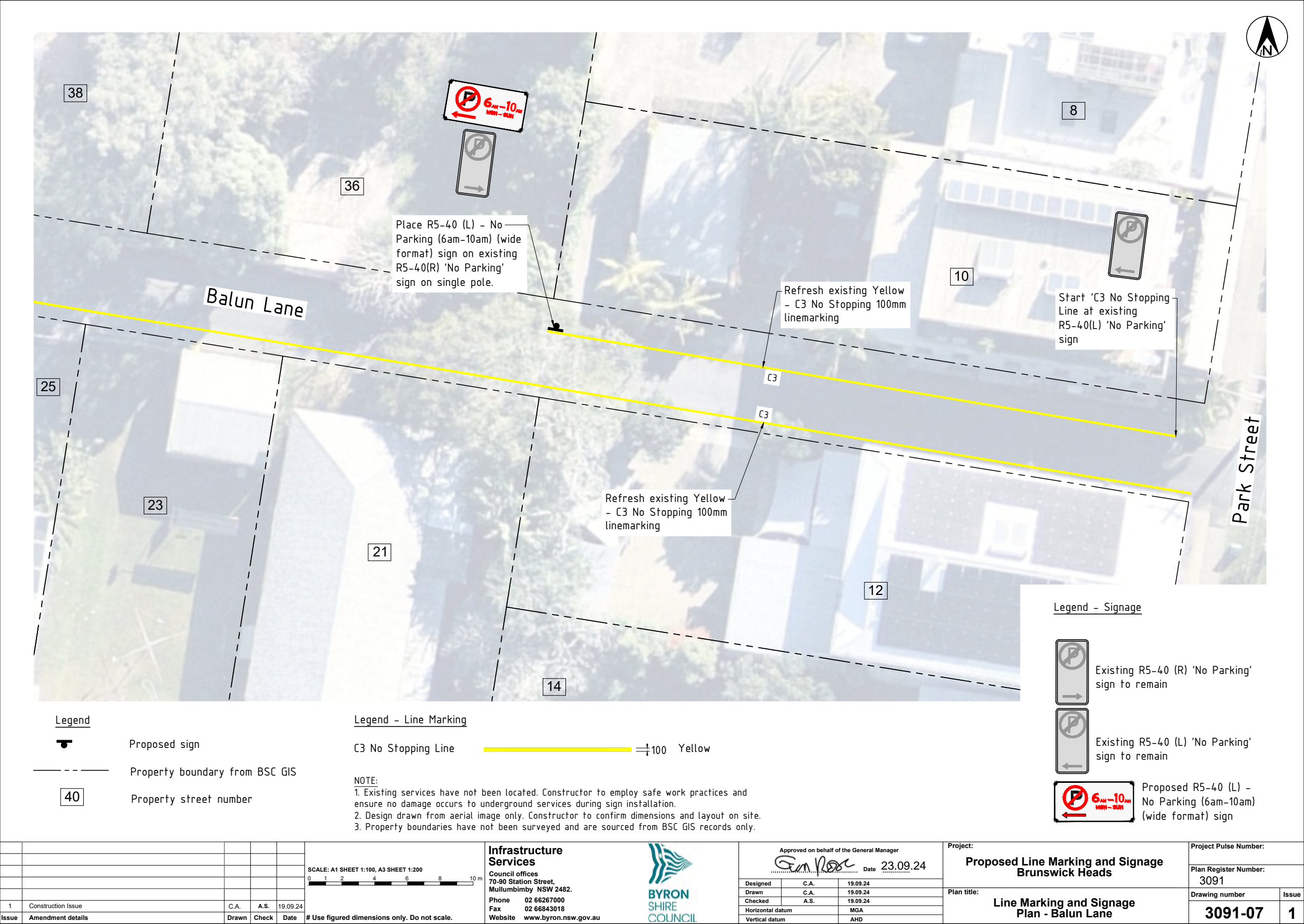
 40 Property street number

NOTE:  
1. Design drawn from aerial image only. Constructor to confirm dimensions and layout on site.  
2. Property boundaries have not been surveyed and are sourced from BSC GIS records only.



|  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |    |
|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|----|
|  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  | </ |
|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|----|







**Legend**

Proposed sign      Property boundary

40      Property street number

**Legend - Line Marking**

C3 No Stopping Line      Yellow

**NOTE:**

1. Existing services have not been located. Constructor to employ safe work practices and ensure no damage occurs to underground services during sign installation.

2. Design drawn from aerial image only. Constructor to confirm dimensions and layout on site.

3. Property boundaries have not been surveyed and are sourced from BSC GIS records only.

|  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|
|  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|

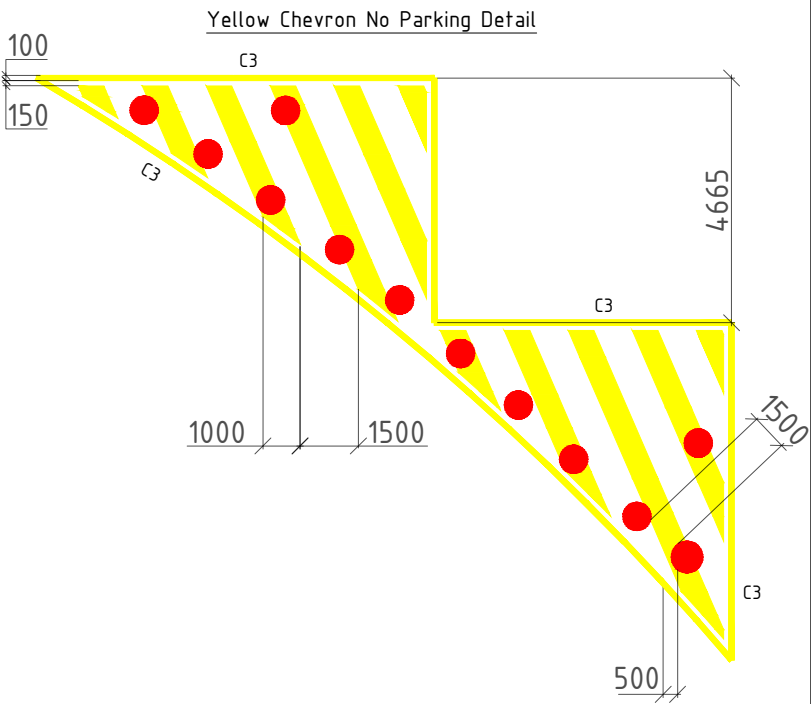


Legend

- Proposed sign
- Property boundary from BSC GIS
- Proposed reboundable PVC traffic bollard 750mm

Legend - Line Marking

C3 No Stopping Line 100 Yellow



NOTE:  
1. Chevron detail not to scale.  
2. Design drawn from aerial image only. Constructor to confirm dimensions and layout on site.  
3. Property boundaries have not been surveyed and are sourced from BSC GIS records only.

|  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|
|  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|

[illegible]