# Notice of Meeting

# Local Traffic Committee Meeting

A Local Traffic Committee Meeting of Byron Shire Council will be held as follows:

Venue	Conference Room, Station Street, Mullumbimby
Date	Tuesday, 19 November 2024
Time	9.00am

Phil Holloway Director Infrastructure Services

I2024/1541 Distributed 12/11/24



LOCAL TRAFFIC COMMITTEE MEETING

### **BUSINESS OF MEETING**

### 1. APOLOGIES

### 2. DECLARATIONS OF INTEREST – PECUNIARY AND NON-PECUNIARY

### 3. ADOPTION OF MINUTES FROM PREVIOUS MEETINGS

3.1 Local Traffic Committee Meeting held on 15 October 2024

### 4. MATTERS ARISING

### 5. OUTSTANDING ISSUES/RESOLUTIONS

### 6. REGULATORY MATTERS

6.1	119-123 Jonson Street, Byron Bay - new line marking and signage	3
6.2	Slessor Lane, Brunswick Heads - No Stopping area	50
6.3	Mullumbimby Road Upgrade	
6.4	Lighthouse Road - Shared path - 'Get NSW Active' Grant Application	
6.5	Byron Street shared path - Bangalow - 'Get NSW Active' Grant Applicati	ion 77

### 7. FOR INFORMATION ONLY

### LOCAL TRAFFIC COMMITTEE MEETING

### **REGULATORY MATTERS**

# Report No. 6.1 119-123 Jonson Street, Byron Bay - new line marking and signage

5 **File No:** I2024/1369

The purpose of this report is to gain Council support for new regulatory signage and line marking for a proposed development at 119-123 Jonson Street, Byron Bay as shown in Attachment 1 (E2024/121173).

- 10 Council has received a Roads Act application associated with a development approval (10.2023.235.1) to Demolish existing development and clear the site, construction of mixed-use development comprising basement parking, ground floor courtyard with commercial premises, food and drink premises, two levels of shop top housing (total of 21), rooftop terraces and pool.
- 15 The Roads Act application is for the associated external works, which requires roadworks within Middleton Lane, Kingsley St and Jonson Street. The new on-street parking with be in accordance with AS2890.5. Middleton Lane, requires a full road reconstruction that will need a "Road Closure" and a detour in place during this construction period.
- A Public Safety Management Plan (PSMP) and Traffic Management Plan (TMP) have also
   been submitted to address the 3 different stages of construction required and meets the associated conditions of the D.A consent refer Attachment 2 (E2024/121172)

Condition 18 of development consent 10.2023.235.1 state:

18.

### Consent required for Works within Road Reserve

Consent from Council must be obtained for works within the road reserve pursuant to Section 138 of the Roads Act 1993. Three (3) copies of engineering construction plans must accompany the application for consent for works within the road reserve.

Such plans are to be in accordance with Council's current Design & Construction Manuals and are to provide for the following works:

### a) Roadworks in Middleton Lane

- A driveway in accordance with Council's standard "Northern Rivers Local Government Development Design & Construction Manuals and Standard Drawings" and approved plans.
- (ii) Full road reconstruction including upright kerb and gutter, road pavement and associated drainage construction and any necessary relocation of services as follows:

<u>6.1</u>

### LOCAL TRAFFIC COMMITTEE MEETING

- across the full frontage of the site, extending across the full frontage of 125 Jonson St and the intersection of Middleton Lane) and Kingsley St (including any driveway upgrading to the existing driveway of 14 Kingsley St;
- retaining walls;
- upgrade existing driveways;
- electrical infrastructure in accordance with service providers requirement
- streetlighting in accordance with AS1158;
- underground drainage;
- other services utilities upgrading requirement; and
- linemarking & signage.

### b) Roadworks in Kingsley St

- (i) Roadworks including the design and construction of on-street parking generally in accordance with the approved plans;
- (ii) Footpath design to meet part D4.2.13 of Chapter D4 of BDCP 2014;
- (iii) Ramps in accordance with Council's standard drawing R-04;
- (iv) Redundant driveway to be removed;
- (v) Walkway and ramps gradient to meet the accessible requirement in AS1428 and must transition with the existing footpath levels with neighbouring sites in Kingsley St and Jonson St;
- (vi) Crossfall of 1 % or 1:100 (maximum 2.5% or 1 in 40);
- (vii) Redesign the 45° metered parking to 90° metered parking to accommodate a minimum of 11 parking spaces including 1 accessible parking;
- (viii) Parking design in accordance with Table 3.3 of AS2890.5;
- (ix) Accessible parking design to address all parts of clause 4.5 of AS2890.5;
- (x) Stormwater drainage upgrade;
- (xi) Electricity & streetlighting in accordance with AS1158; and
- (xii) Linemarking and signage including markings to guide traffic in accordance with Figure 2.1 typical end clearances of AS2890.5.

### c) Roadworks in Jonson St

- (i) Roadworks generally in accordance with the approved plans;
- (ii) Footpath design to meet part D4.2.13 of Chapter D4 of BDCP 2014;
- (iii) Ramps in accordance with Council's standard drawing R-04;
- (iv) Walkway and ramps gradient to meet the accessible requirement in AS1428 and must transition with the existing footpath levels with neighbouring sites in Jonson St;
- (v) Crossfall of 1 % or 1:100 (maximum 2.5% or 1 in 40);
- (vi) Redundant driveways to be removed and replace kerb & gutter to match the existing;
- (vii) Re-linemark the 45° metered parking bays in accordance with the approved plans;
- (viii) Parking design in accordance with Table 3.3 of AS2890.5;

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- (ix) Accessible parking design to address all parts of clause 4.5 of AS2890.5;
- (x) Linemarking and signage including markings to guide traffic in accordance with Figure 2.1 typical end clearances of AS2890.5.

### d) Local Traffic Committee requirements

All works associated with the provisions of regulatory signage and linemarking are subject to Local Traffic Committee Recommendation.

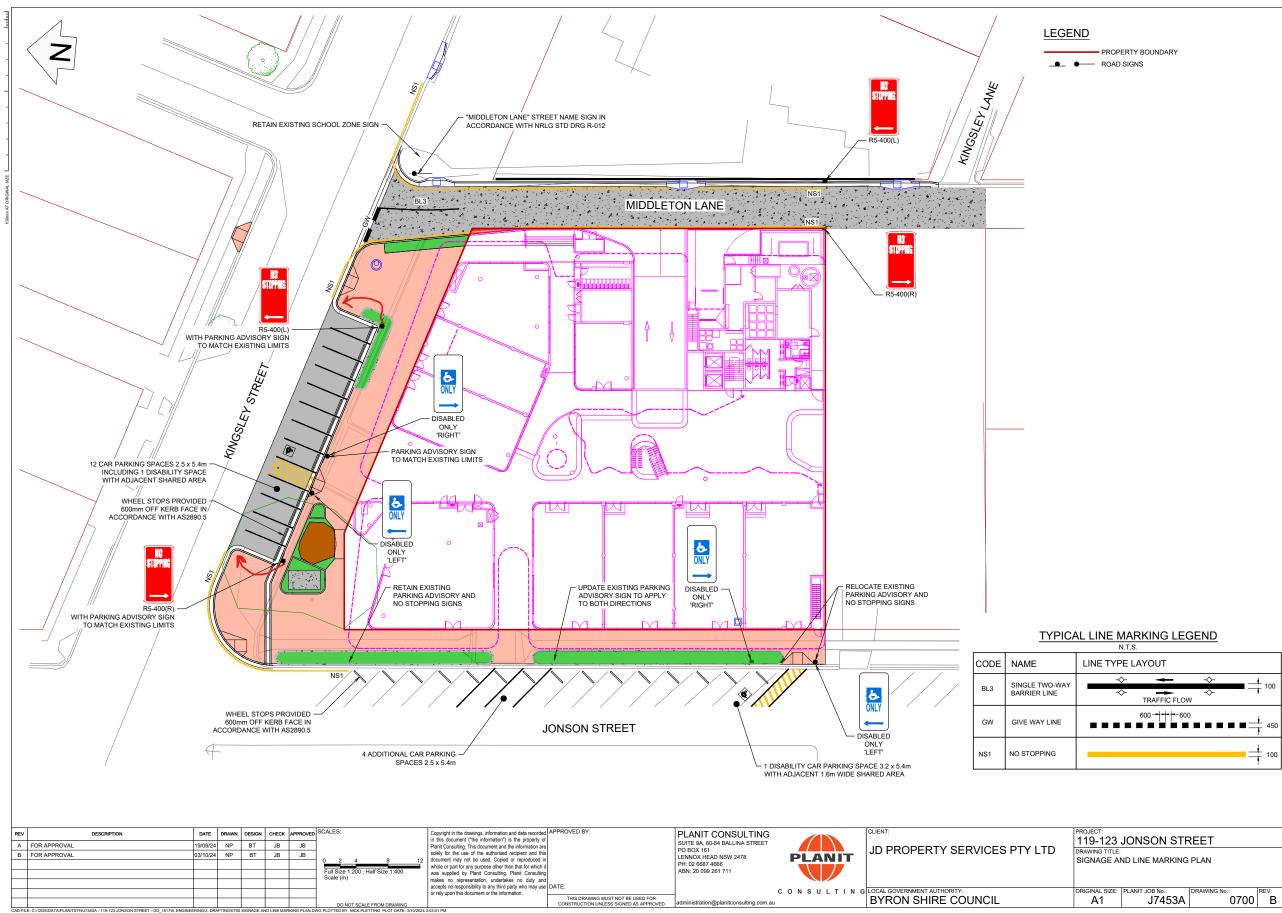
### **RECOMMENDATION:**

5 That Council support the new signage, line markings and traffic control devices associated with the 119-123 Jonson St, Byron Bay development as shown in Attachment 1 (E2024/121173)

### Attachments:

Agenda

- 10 1 51.2023.2325.1 10.2023.235.1 LTC submission, E2024/121173 , page 6 🗓 🛣
  - 2 51.2023.2325.1 10.2023.235.1 PSMP & TMP submission, E2024/121172 , page 7 🗓 🛣



### 6.1 - ATTACHMENT 1

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## Construction Traffic Management Plan and Public Safety Management Plan

119-123 Jonson Street, Byron Bay

Prepared for Cobild By Planit Consulting Pty Ltd

Rev C - September 2024 Job No: J7453A

#### **REGULATORY MATTERS**

#### 6.1 - ATTACHMENT 2

**Construction Traffic Management Plan and Public Safety Management Plan** J7453A - 119-123 Jonson Street, Byron Bay Cobild www.planitconsulting.com.au



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#### **Document Control**

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#### **Revision History**

Revision	Date	Prepared By	Reviewed By	Approved By
vı	30/05/2024	AP	JB	CW
V2	4/06/2024	AP	JB	CW
V2	19/09/2024	JB	ЈВ	CW

#### **Approval Details**

Approved By	Chaej Wrencher
Signature	

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### 1 Project Background

#### 1.1 Site Description

This Construction Traffic Management Plan (CTMP) and Public Safety Management Plan (PSMP) has been prepared in accordance with Council's Development application approval (DA No. 10.2023.235.1), detailing the condition of consent that are required to be satisfied before commencement of works and during construction.

Specifically, this report addresses condition 22, 23 and 24 (Figure 2) of the DA Conditions of Consent for 119-123 Jonson Street, Byron Bay, NSW 2481. Refer to Figure 1 for the subject site location and section 1.2 for an extract of the relevant conditions of consent.



Figure 1: Subject Site Location (Source: Nearmaps)

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#### 1.2 Conditions of Consent – Condition 22, 23 and 24

22 Public Safety Management Plan required Consent from Council must be obtained for a Public Safety Management Plan for those works within the road reserve pursuant to Section 138 of the Roads Act 1993. This public safety management plan is to include provision for (but not be limited to): a pedestrian barrier, alternative footpaths and ramps as necessary; a) an awning sufficient to prevent any substance from, or in connection with, the work b) falling into the road reserve; lighting of the alternative footpath between sunset and sunrise: c) the loading and unloading of building materials; d) parking space for tradesman's vehicles, where such vehicles must be located near the e) site due to tools and equipment contain within the vehicle; Removal of any such hoarding, fence or awning as soon as the particular work has been f) completed. 23 **Traffic Management Plan** Consent from Council must be obtained for a Traffic Management Plan pursuant to Section 138 of the Roads Act 1993. The plans and specifications are to include the measures to be employed to control traffic (inclusive of construction vehicles) during construction of the development. The traffic management plan is to be designed in accordance with the requirements of the Roads and Traffic Authority's Manual, Traffic Control at Work Sites Version 2, and the current Australian Standards, Manual of Uniform Traffic Control Devices Part 3, 'Traffic Control Devices for Works on Roads'. The report must incorporate measures to ensure that motorists using road adjacent to the development, residents and pedestrians in the vicinity of the development are subjected to minimal time delays due to construction on the site or adjacent to the site. The traffic management plan must be prepared by a suitably gualified and RTA accredited Work Site Traffic Controller. Traffic Management Plan for Occasional Service of Loading Bay 24 Prior to issue of the construction certificate, consent from Council must be obtained for a Traffic Management Plan (TMP) pursuant to Section 138 of the Roads Act 1993. The plans and specifications are to include the measures to be employed to control traffic for occasional service of Loading Bay. The TMP is to be designed in accordance with the requirements of the current version of the Transport for NSW Traffic Control at Work Sites Technical Manual. The plan shall incorporate measures to ensure that motorists using road adjacent to the development, residents and pedestrians in the vicinity of the development are subjected to minimal time delays and safety due to occasional service requirements of the loading bay during business hours of the development to satisfy clause 3.2.2 of AS2890.2. The TMP must be prepared by a suitably gualified Transport for NSW accredited person.

Figure 2: Excerpt From Conditions of Consent (Condition 22, 23 & 24)

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#### **1.3 Existing Conditions**

#### 1.3.1 Objectives

The purpose of this report is to provide a Concept Traffic Management Plan (CTMP) and Public Safety Management Plan (PSMP) in support of the proposed mixed used development located at 119-123 Jonson Street, Byron Bay, NSW, 2481 (Figure 3).



Figure 3: Subject Site – Locality Plan

#### 1.4 Subject Site Surrounding Infrastructure

#### 1.4.1 Jonson Street

Jonson Street is the main distributor road that fronts the subject site. The site posted speed limit is 50km/hr. Jonson Street is a classified (state) regional road and the primary road authority is Byron Shire Council.

#### 1.4.2 Public Parking

There are some formalised parking spaces in the vicinity of the project site and opportunities for parallel kerb side and 45-degree parking. Specifically, restricted paid parallel and unrestricted 45-degree angle paid parking is proposed along Jonson Street in proximity to the subject site.

Currently, the site has 11 x car spaces along Jonson Street adjacent to the western boundary of the subject site. The existing car spaces adjacent to the northern boundary of the subject site in Kingsley Street is inclusively proposed for upgrade.

#### 1.4.3 Pedestrian and Cyclist Facilities

Existing footpaths and bike paths are provided along Jonson Street.

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### 2 Public Protection Controls

#### 2.1 Public Protection Controls

Public protection and exclusion controls are required to be implemented throughout the duration of the project. These include signage informing of any site activity and barriers preventing entry to the site.

To achieve adequate public protection and exclusion zones the following is required as part of the Public Safety Management plan:

- A pedestrian barrier, alternative footpaths and ramps as necessary;
- A 1.8m high temporary fence around the property boundary is to be provided;
- Awnings will be required if works are to be located over the footpath to prevent any materials or substance from or in connection with the works falling into the road reserve;
- Lighting shall be provided on any water filled barriers to provide pedestrians with adequate lighting between sunset and sunrise;
- For loading and unloading of building materials, public safety measures shall be with the Traffic guidance scheme control measures. Refer to the attached Public Safety Management Plan (PSMP) in Appendix A and Traffic Guidance Scheme (TGS) in Appendix B for further details';
- Contractors will park along Jonson, Kingsley Street and internally on the site. All work equipment and tools to be located within the subject site;
- Removal of any hoarding, fence or awning shall take place as soon as the works for which they are required is completed.

#### 2.1.1 Cyclists and Pedestrians

Cyclists and pedestrians currently use this area to access local shops, businesses, Fire and Rescue NSW, Byron Bay Fire Station and the public school surrounding the subject site. Due to the large volume of traffic in the area most pedestrians and cyclists use the footpaths to travel through this area.

To enable safe access around the worksite for pedestrians and cyclist during construction, it is proposed to delineate all works area with safety barriers and 1.8-metre fencing, this shall allow safe pedestrian access around proposed work areas. The fencing will have warning signs erected on it to advise the public to keep out of the works area.

Throughout the various stages of construction, pedestrians will require alternative access points. As construction progresses, pedestrians will be directed to cross streets at designated locations, adhering to standard road rules. It is noted Jonson Street, Byron Bay has numerous pedestrian median refuges to allow for safe crossings.

Traffic control signs are proposed to guide pedestrians safely around the construction site.

#### 2.1.2 Existing Signage

The are numerous signs currently in this area (i.e., advertising signs, posted speed limits, advisory signs, no stopping signs). It is proposed all traffic signs and advance warning signs will be placed by a suitably qualified traffic officer and checked and reported daily. If signs are being moved or removed than permanent fixtures may be required.

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#### 2.1.3 Public Notification

#### 2.1.3.1 Letter Drop

Occupants of the properties, businesses, school and fire station that surround the works will be notified through a letter drop 2-weeks prior to construction. The letter drop will outline the following as a minimum:

- Proposed works;
- Timeframes;
- Potential delays to the community; and
- Relevant Contact information.

It is imperative for the contractor to keep a record of all communications with the public and to close out any concerns in a reasonable time frame. BSC and Contractor will meet weekly to discuss community communications and safety concerns.

#### 2.1.3.2 Council Website Updates

Council is to be notified of proposed work and council websites to be updated, accordingly.

#### 2.1.3.3 Bus Services

Blanch buses and other bus services are to be notified of proposed traffic activities to ensure no delays in scheduled timeframes.

#### 2.1.3.4 Variable Message Signs (VMS)

A VMS will be set up on Jonson or Kingsley Street to advise road users and the community of the proposed works. The VMS board will be set up 1-2 weeks prior to the proposed start date of the construction works.

The contractor to confirm preferred location with BSC.

#### 2.1.4 Emergency Arrangements

Emergency services will have continual access to all businesses and properties hence no specific facilities are required. Furthermore, the Byron Bay Fire Station opposite the subject site at Fletcher Street, Byron Bay will have continual access throughout the duration of the project.

The project supervisor shall assist emergency vehicles requiring entry/exit through the works area.

#### 2.1.5 Proximity to the existing public school and Fire Station

The existing public school (i.e., Byron Bay Public) and fire station are adjacent to the subject site, and it is important the site supervisor to provide periodic updates of the proposed works and traffic, public and emergency operations. This will ensure safety and transparency during the construction process of the project.

#### 2.1.6 Traffic Uniformity with the adjacent development: 116-118 Jonson Street, Byron Bay Development (Bonobo Development)

It is imperative for the site supervisor to liaise with the Bonobo (116-118 Jonson Street, Byron Bay Development) site supervisor regarding public safety and proposed traffic controls. This will ensure safety and uniformity with traffic and pedestrian safety controls. Refer to the Figure 4 for the location of the Bonobo development.

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Figure 4: Site Locality of Adjacent Development 'BONOBO' 116-118 Jonson Street, Byron Bay.

It is noted majority of the construction activities for Bonobo development are located at the rear of the subject site along the old railway tracks.

Refer to the attached Public Safety Management Plan (PSMP) in Appendix A for further details.

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### **3** Construction Operations

#### 3.1 Site Location and Loading Area

Refer to Figure 5 below for the proposed site arrangement for each phase of the project. Refer to Appendix A for full TGS plans.

Stage 1	TOTAL A CASE OF
Stage 2	ERRER ORDER LINGWICH
Stage 3	NICHER PORTUGATION ACTIVATION ACT

Figure 5: Construction Site Arrangement

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#### **3.2 Construction Activities**

The proposed typical construction activities for the development are shown in Table 1 below: Table 1: Construction Activities

Onsite Works	Approx. Duration (Weeks/Months)
Stage 1 – Retention and Excavation	5 Months
Stage 2 – Structure	7 Months
Stage 3 – External Works and Fit out	10 Months

#### 3.3 Construction Hours

#### 3.3.1 Hours of Work – Condition 49 Requirements

As per condition 68 (Hours of Work), Construction works for the development, demolition or vegetation removal including delivery of machinery and materials to and from the site will occur between the hours of:

- Monday to Friday 7:00am-6:00pm.
- Saturday 8:00am 1:00pm; and
- No work on Sundays or Public Holidays.

Where practicable, access and activities should be scheduled to occur outside of the peak periods. Based on traffic data and site inspections, peak period traffic between 8:15am-9:15am in the morning, 3:00pm and 4:30pm in the afternoon, and Weekend Peak 1:00-2:00pm.

It must be ensured by the principal certifier that the building work, demolition, or vegetation removal is not carried out on Sundays and public holidays, except where the is an emergency.

Furthermore – unless approved within the construction site management plan, construction vehicles, machinery, goods, or materials must not be delivered to the site outside the approved hours of site works.

#### 3.4 Use of Council Land/Road Reserve to Enable Construction Works, Events or Temporary use.

During the Construction phases of the project, it is proposed to utilise Council Land/Road reserve to enable construction works. The Project Construction manager will be required to liaise with BSC to use council land/road reserve to enable construction works, events or temporary use.

Construction Manager is to contact council with the proposed area/number of parking bays and duration proposed, and an invoice from council shall be issued for payment. Council will require details of each phase prior to any of these associated works starting. Failure to do so, will lead councils compliance team to take appropriate actions.

#### 3.5 Construction Worker Induction & Safe Work Procedures

#### 3.5.1 Safe Work Requirements

To protect the safety of workers and the public, the work site should be adequately secured (i.e., security fence) to prevent access by unauthorized personnel. Additionally, all workers must always be inducted in accordance with the relevant SafeWork requirements.

#### 3.5.2 Safe Work Method Statements (SWMS)

A SWMS should be completed whenever any person undertaking works on or adjacent to the public domain.

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#### 3.5.3 Truck Driver Code of Conduct

Drivers associated with the project are to abide by a code of conduct in order to:

- Minimise impacts of construction on the local road network;
- Minimise conflict with other road users;
- Minimise road traffic noise; and
- Ensure truck drivers used specified and appropriate routes.

#### **3.6** Dilapidation Report – Condition 34 Requirements

As per DA Condition 34 – **Dilapidation Report,** a full dilapidation report will be undertaken:

A second Dilapidation Report must be submitted to Council, prior to the issue of an Occupation Certificate, to ascertain if any structural damage has occurred to any adjoining building, infrastructure, or roads.

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### 4 Truck Movements

#### 4.1 Construction Vehicle Routes

It is anticipated that majority of the pre-cast materials shall be delivered/sourced from the south (e.g., Ballina, Lennox Head) and it is expected that majority of the construction trucks will be utilising the M1 Highway as the preferred route to the subject site.

Construction vehicle movements will occur to and from the site through the Byron Bay town centre from Ewingsdale Road and Bangalow Road.

The key construction access and egress routes are as follows:

- Construction traffic travelling on Ewingsdale Road will access the site via the following proposed route:
  - o Continue straight from Ewingsdale road onto Shirley Street;
  - Proceed on Shirley Street and continue straight onto Butler Street (Byron Bay bypass);
  - Proceed along Butler Street (Byron Bypass) and turn left on the Jonson/Browning Street;
  - o Proceed approximately 250m straight along Jonson Street; and
  - o Access the site by turning right into the designated site access area.
- Construction traffic travelling on Bangalow Road will access the site via the following proposed route:
  - o Continue Straight on Bangalow Road and turn left onto Browning Street;
  - Proceed along Butler Street/Jonson Street Roundabout and turn right onto Jonson Street;
  - o Proceed approximately 250m straight along Jonson Street; and
  - $\circ \quad$  Access the site by turning right into the designated site access area.
- Construction Traffic Travelling on Jonson Street will egress the subject site via the following route:
  - Exit the site by turning right onto Jonson Street and continue to travel Northbound;
  - o Turn right onto Marvell Street and continue along Tennyson Street;
  - o Continue on Tennyson Street and turn right onto Browning Street; and
  - Either turn left onto Bangalow Road or right onto Butler street.

Refer to Figure 6 (to the site) and 7 (from the site) for the proposed construction vehicle routes for vehicles travelling along Ewingsdale Road and Bangalow Road to and from the subject site and Appendix B for the proposed swept paths.

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SUBJECT SITE

Figure 6: Proposed Construction Vehicle Routes to the Subject Site (Source: Nearmaps)

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SUBJECT SITE

Figure 7: Proposed Construction Vehicle Routes exiting the Subject Site (Source: Nearmaps) Construction Vehicle Access to the Site will be primarily from Kingsley Street, Byron Bay.

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#### 4.2 Types of Construction Vehicles

A combination of truck and vehicle types will be used during the construction of the project. Typical vehicle traffic is expected to comprise of the following vehicles:

- Light vehicles for the movement of construction personnel, including contractors, the project labour force and management staff;
- Small and medium vehicles for machinery delivery; and
- Heavy vehicle movements for the delivery and removal of construction machinery and materials, spoil and waste.

The largest vehicles used in terms of length and mass for construction activities will be:

- 16.5m Truck and Dog; and
- Semi-trailers (19m Articulated Vehicle AV).

#### 4.3 Number of Truck Movements

The number of truck movements required for cut/fill operations has been assessed based on the estimated bulk earthworks volumes and proposed earthworks construction phases. These truck movements are summarised in Table 2 with the inclusion of an estimate of the number of days required to excavate the site. The estimates below have been prepared on the assumption of excavation of approximately  $15m^3$  of material per load.

Table 2: Earthworks and Truck Movements

Earthworks and Truck Movements							
Stage	Number of truck loads (15m <sup>3</sup> per truck)	Number of required					
Stage	(m³)	(m³)	(m³)		days		
Bulk Earthworks	12,000	-	12,000	800	90		

#### 4.4 Proposed Construction Traffic Impact on Jonson and Kingsley Street

During the construction phase, the maximum traffic volumes expected to occur during the bulk earthwork stage are described in Table 3.

Table 3: Summary of Truck Movements

Stage	Total Truck Movements	Total Duration (Days)	Daily Truck Volumes (Trips/day)	Maximum Allowable Peak Hour Truck Trips
Bulk Earthworks	800	90	9	6

As demonstrated in Table 3, during the bulk earthwork phase, the proposal development is expected to generate a maximum of 20 vehicle movements per day and a maximum of 6 vehicle trips per hour in both the AM and PM peak periods. This equates to approximately one (1) vehicle movement every (10) minutes during peak periods.

Trucks entering the site are not to exceed 6 trucks in any 1-hour period. The Project Manager and Supervisor on-site will be responsible for monitoring truck movements and reporting back to Byron Shire Council (BSC). Furthermore, trucks are restricted to one truck on or around the subject site at any given time.

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### 4.5 Truck Queuing Location

During the construction phases, it is proposed that at any given time only one truck shall be located on or around the subject site at a time. Therefore, it is proposed that a truck queuing location is required to mitigate any queuing within the CBD of Byron Bay. It is proposed to utilise the southern portion of the Cavanbah centre carpark when appropriate as a truck queuing location.

Cavanbah Centre is approximately 4-5km away (8-minutes) from the subject and it is anticipated that trucks queuing in the southern end of the Cavanbah centre carpark shall have minimal impact to the surrounding areas.

Refer to Figure 8 for the proposed truck queuing location.



Figure 8: Proposed Truck Queuing Location Option (Source: Nearmaps)

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### 5 Proposed Mobile Crane Setups

Based on the proposed construction activities, it is proposed that mobile cranes will be used as required for 'special deliveries' only. Figure 9 shows indicative temporary mobile crane setup locations, these mobile crane lifts will occur at scheduled times of the project and are proposed to be utilised for 'special deliveries' only subject to separate TGS.

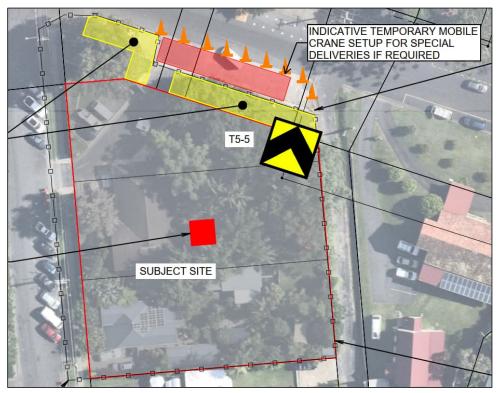


Figure 9: Proposed Mobile Crane & Tower Crane Setups

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### 6 Hazard Identification/Risk Assessment

A Hazard identification and Risk Assessment has been completed for public protection to demonstrate that hazards and risk can be managed and mitigated in an efficient and controlled manner. The hazard identification/risk assessment is conducted throughout the design and has the following process:

- 1. Identifies key hazards associated with pedestrian and traffic activities;
- 2. Assesses the likelihood and consequences of the hazard to give a risk score;
- 3. Provide recommendations for risk control measures following the hierarchy of controls; and
- 4. Re-assess the likelihood and consequences of the hazard under the proposed control measures to give an updated risk score.

The risk assessment matrix, likelihood and consequence descriptions are shown in Figures 10-12 respectively.

RISK ASSESSMENT MATIRX							
LIKFLIHOOD	CONSEQUENCES						
LIKELIHOOD	1 Insignificant	2 Minor	3 Moderate	4 Major	5 Catastrophic		
5 Almost Certain	5 Low (+)	10 Medium (+)	15 High	20 Very High	25 Extreme		
4 Likely	4 Low (-)	8 Medium (-)	12 Medium (+)	16 High	20 Very High		
3 Moderate	3 Negligible	6 Low (+)	9 Medium (-)	12 Medium (+)	15 High		
2 Unlikely	2 Negligible	4 Low (-)	6 Low (+)	8 Medium (-)	10 Medium (+)		
1 Rare	1 Negligible	2 Negligible	3 Negligible	4 Low (-)	5 Low (+)		

Figure 10: Risk Assessment Matrix

LIKELIHOOD					
Almost Certain	The event is expected to occur in most circumstances				
Likely	The event will probably occur in most circumstances				
Moderate	The event should occur at some time				
Unlikely	The event could occur at some time				
Rare	The event may occur only in exceptional circumstances				

Figure 11: Likelihood Description

	CONSEQUENCES					
Catastrophic	Death; very large financial loss					
Major	Extensive injuries requiring hospitalisation; major financial loss					
Moderate	Medical treatment required; high financial loss					
Minor	First aid treatment; some financial loss					
Insignificant	No injuries; no financial loss					

Figure 12: Consequences Description

Refer to Table 4 Below for the risk assessment prepared.

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#### Table 4: Risk Assessment

Hazards and Risks	Risk Assessment			Recommended Risk Control Measures	Residual Risk Assessment			Responsible for monitoring and reviewing this activity to ensure safety and
	Likelihood	Consequence	Risk Rating		Residual Likelihood	Residual Consequence	Residual Risk Rating	compliance
Truck entering the site and not seeing pedestrians resulting in a collision.	5	5	Extreme	The closure of footpaths and roads, will require the employment of traffic controllers (e.g., stop/go person) or the preparation of a traffic management plan in accordance with relevant standards.	2	5	Medium	Site Manager
Falling Objects landing on public places.	3	5	High	<ul> <li>When work is carried out at a height exceeding 4m above the lowest ground level of the adjoining public place the following will be implemented: <ul> <li>A hoarding at least 1800mm high that is fully sheeted with timber; Plywood, metal, or sturdy synthetic sheets, and one of the following, a gantry (or overhead protective structure), (incorporating the hoarding), or the adjoining area is closed to prevent and mitigate objects falling on or otherwise hitting members of the public in that vicinity, or a catch platform with vertical sheeting or perimeter containment sheeting is installed.</li> </ul> </li> </ul>	2	5	Medium	Site Manager

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Hazards and Risks	Risk Assessment		Risk Assessment Recommended Risk Control Measures	Residual Risk Assessment			Responsible for monitoring and reviewing this activity to ensure safety and	
	Likelihood	Consequence	Risk Rating		Residual Likelihood	Residual Consequence	Residual Risk Rating	compliance
Slips, trips and falls by members of the public	3	4	Medium	Public areas will be kept clear of any debris, trip or slip hazards and protected from any projectiles, dust etc. that may be released as a product of work undertaken	2	3	Medium	Site Manager
Trucks entering the site unannounced and traffic control not ready causing unsafe maneuvering around the site.	3	4	Medium	<ul> <li>When the driver of vehicles, trucks, and plant turn up unannounced to site and are required to reverse into the site across public footpaths an walkways the following is to be implemented: <ul> <li>Erect a sign at the entry gates 'directing drivers to contact the site manager/foreman before entering the site - provide contact details;</li> <li>Email suppliers and subcontractors to advise them of the health and safety protocol to be followed for plant and vehicle movements onto the site;</li> <li>Where there are members of the public using the footpath or walkways provide a stop/go person or a spotter or mechanical aids to control their movements</li> <li>The reversing vehicle, plant and trucks from the street to be controlled as per the traffic management plan for the project.</li> </ul> </li> </ul>	2	4	Medium	Site Manager

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Hazards and Risks	Risk Assessment			Recommended Risk Control Measures	Res	Responsible for monitoring and reviewing this activity to ensure safety and		
	Likelihood	Consequence	Risk Rating		Residual Likelihood	Residual Consequence	Residual Risk Rating	compliance
Partial road closure causing delays to the public.	2	2	low	Written approval will be obtained from relevant authority to close adjoining areas when excluding members of the public. Any controls imposed by the authority will be fully implemented and monitored during the project	2	1	Negligible	Site Manager
Individuals entering the construction site resulting in injuries and site damage.	2	4	Medium	"Construction Site - No Un-authorised entry" signs and fencing will be used to clearly identify areas on and around the perimeter of the sites as being a place of work.	2	4	Medium	Site manager/Foreman

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### 7 Environmental Control Measures

#### 7.1 Noise and Vibration

Throughout the duration of the proposed construction stages, all practicable efforts shall be made to mitigate the noise and vibration generated from the construction activities. A project specific noise and vibration management plan has been prepared by ENV. All noise and vibration measures noted within this plan will be implemented on the project and will include mitigation measures to satisfy the following DA conditions:

- DA Condition 33: Noise and Vibration Management Plan;
- DA Condition 10: Environmental Management Plan;
- DA Condition 68: Construction Times; and
- DA Condition 69: Construction Noise.

Performance Criteria:

- Construction works are undertaken within scheduled work hours in accordance with the EPA and BSC guidelines; and
- No noise complaints from project related works to be lodged to the contractor or relevant authorities.

Refer to Table 5 for the proposed management/mitigation strategies:

Table 5: Construction Mitigation Strategies

Hazard	Control	Timing	Responsibility
Noise Pollution	<ul> <li>Normal Construction hours:</li> <li>Monday to Friday 7:00am- 6:00pm</li> <li>Saturday 8:00am – 1:00pm</li> <li>No work on Sundays or public Holidays</li> </ul>	Construction	Project Manager
Noise Pollution	Demolition and excavation works are to be restricted to only operate within hours from 8:00am – 5:00pm Monday to Friday.	Construction	Project Manager
Noise Pollution	All personnel will be informed during inductions of the potential negative affect noise pollution could have on local residents and the surrounding area.	Construction	Supervisor/Environmental Officer
Noise Pollution	Residents in the general vicinity will be informed of the proposed works and consultation with affected landowners will be maintained throughout the construction period.	Pre-construction/ Construction	Project Manager/Environmental Officer
Noise Pollution	During Construction hours, any proposed noise generating construction activity shall not result in the emission of offensive noise.	Construction	Project Manager/ Environmental Officer
Excessive Vibration	Vibration from works is to be undertaken in accordance with industry best practice, to ensure excessive levels of vibration do not occur.	Construction	Project Manager/Environmental

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#### 7.2 Dust Mitigation

To mitigate the impact of dust, on-site watering of equipment will be on hand to dampen any dust activities. A project specific Construction Environmental Management Plan has been compiled by ENV, this plan will be implemented. Monitoring of dust will include visual inspection of the quantity of dust on surrounding vegetation and particles suspended within the air. This shall provide an indication of the effectiveness of dust control measures. The results will be documented in a monthly report, including inspections, non-conformances detected and corrective actions.

In the case of an non-conformances (e.g., a compliant from residents or if the site manager or relevant authorities believes there is excessive dust being generated), dust deposition gauges are required to be installed to quantify dust movement/generation on the property boundary of the closet resident downwind of the development site.

The values obtained from the dust deposition gauge will be compared with the National Environment Protection Council (NEPC) standards shown in Table 6.

Table 6: NEPC Standards

Pollutant	Averaging Period	Maximum Concentration
PM10	1-day	50 µg/m³
PM10	1-year	25 µg/m³
PM2.5	1-day	25 µg/m³
PM2.5	1-year	8 µg/m³

Should dust onsite exceed NEPC standards, immediate actions shall be taken to mitigate the quantity of dust being generated. This will be achieved by the proposed mitigation strategies shown in Table 7.

Table 7: Air Quality Management Actions

Hazard	Control	Timing	Responsibility
Dust	In winds exceeding 35km/hr, works shall cease unless further dust controls are implemented and approved by the superintendent.	Construction	Project Manager/ Environmental Officer
Dust	Water carts shall be used regularly to dampen stockpiles, stripped areas and other exposed surfaces.	Construction	Project Manager/Supervisor/ Environmental Officer
Dust	The site access is to ensure it is designed for the traffic volume and appropriately stabilised with gravel.	Construction	Project Manager/ Environmental Officer
Dust	Loads are to be securely covered on both internal and public roads whereby there is a risk of release.	Construction	Supervisor/ Environmental officer
Dust	Erosion and Sediment controls are to be maintained to ensure devices do not increase dust generation.	Construction	Supervisor/ Environmental officer
Dust	Disturbed areas are to be stabilised as soon as practical to minimised exposure earth.	Construction	Supervisor/ Environmental officer
Dust	Excavated topsoil shall be isolated for reuse during the restoration process.	Construction	Supervisor/ Environmental Officer
Dust	Excavated material shall be stockpiled in an appropriate area.	Construction	Supervisor/ Environmental Officer
Dust	Contaminated soil shall be disposed of at an approved facility.	Construction	Project Manager/ Environmental Officer

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Hazard	Control	Timing	Responsibility
Dust	Roads/haul roads shall be clearly defined and marked out to reduce the area of traffic loading.	Construction	Supervisor/ Environmental Officer
Dust	All vehicles entering/egressing the site shall use the designated stabilised access location.	Construction	Supervisor/ Environmental Officer
Dust	Transportation of soil material onto pavement surfaces is to be cleaned and removed.	Construction	Supervisor/ Environmental Officer
Dust	Material Spilled from trucks onto any roads is to be removed and cleaned immediately.	Construction	Project Manager/ Environmental Officer
Dust	Loads are to be securely covered on both internal and public roads whereby there is a risk of release.	Construction	Project Manager/ Environmental Officer
Dust Complaints	Ensure public concerns can be heard by making contact numbers available.	Construction	Project Manager/ Environmental Officer
Dust Complaints	Make appropriate corrective action when a complaint is made.	Construction	Project Manager/ Environmental Officer
Air Quality	Prepare monitoring program and report on observations, non-conformances, and corrective actions.	Pre- construction /construction	Project Manager/ Environmental Officer
Fumes	No burning or incinerating of waste to take place on-site.	Construction	Supervisor/ Environmental Officer
Fumes	Construction Plants, Vehicles, and equipment shall be well maintained, ensuring an efficient exhaust system.	Construction	Supervisor

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### 8 Monitoring and Evaluation

#### 8.1 Inspections and Monitoring

In addition to traffic control safety Inspections, formal and documented daily and weekly inspections shall be undertaken at work sites by individuals holding the prepare Work Zone Traffic Management Plan gualification.

It should be noted any incidents (e.g., 'Close calls') must be recorded and documented. This shall be reviewed as part of any inspection and action on immediately.

#### 8.2 What the Works Supervisor Shall Do

For all long-term work sites, the works supervisor who is appropriately qualified shall:

- Inspect the traffic control layout on the day before the works begin and at least once per week during the duration of the work;
- Inspect the traffic control layout between shifts at least once during the first week and at least once every two months for the duration of work;
- Review the reported 'close calls', 'near miss incidents', and any incidents around the subject site;
- Provide after-hours contact to local police for the duration of the work period;
- Inspect the site on the final day to ensure that un-necessary signs and devices are removed;
- Record results of these inspections noting date, time, deficiencies and any corrective action taken or specified; and
- Ensure that any specified corrective action is taken.

#### 8.3 What The Leader Should Do

For all works, the team leader (or site supervisor) shall:

- Keep a record of the TCP that was implemented;
- Have a copy of the TCP used on site;
- Record start and finish times and location of the works;
- Record near misses;
- Carry out inspections before work starts, during the works and pre-closedown of the site using the nominated checklist, noting:
  - Date and time of inspection;
  - o Deficiencies identified and corrective action taken; and
  - o Changes or modifications made to the site.
- Periodically check that all signs and devices are satisfactory and in their correct position; and
- Make these records readily available to authorised staff.

#### 8.4 What The Project Manager Shall Do

The project manager shall:

- Ensure that a traffic control safety inspection is carried out at least once per month by a person qualified in Prepare Work Zone Traffic Management Plans and that the date, time and deficiencies are recorded;
- Ensure that a Traffic Control safety inspection or road safety audit is carried out prior to the implementation of any changes in traffic control or a TCP;
- Ensure that near miss incidents are being reported and recorded then reviewed; and

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• Ensure that any corrective action specified is taken and recorded.

#### 8.5 Legal Procedures

In the case of accidents, either witnessed or reported, involving the public or from which legal proceedings might arise, the following is to be recorded:

- Actual type, size, and location of signs;
- Devices in use at the time of the accident should be recorded and the sign arrangement photographed for subsequent reporting;
- The actual travelled path width and condition;
- Weather conditions should also be recorded;
- Personal injury;
- Extent of vehicle damage; and
- Vehicle details (e.g., rego).

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### 9 Service Bay Occasional Use

Once the project reaches its operation phase, the service bay is proposed to be used for both waste collection and the loading/unloading of goods.

To minimise the risk of clashes between waste collection and the unloading/loading of goods, scheduling shall be implemented organising unloading/loading of good outside typical waste collection hours where possible.

As these activities will be occurring onsite, traffic control measures should not be required during these operations. In addition, commentary from Solo waste has been obtained stating that waste collection from the Jonson Ln development (is currently in operation and is a similar size and has similar waste collection requirements), has not had any issues with waste collection.

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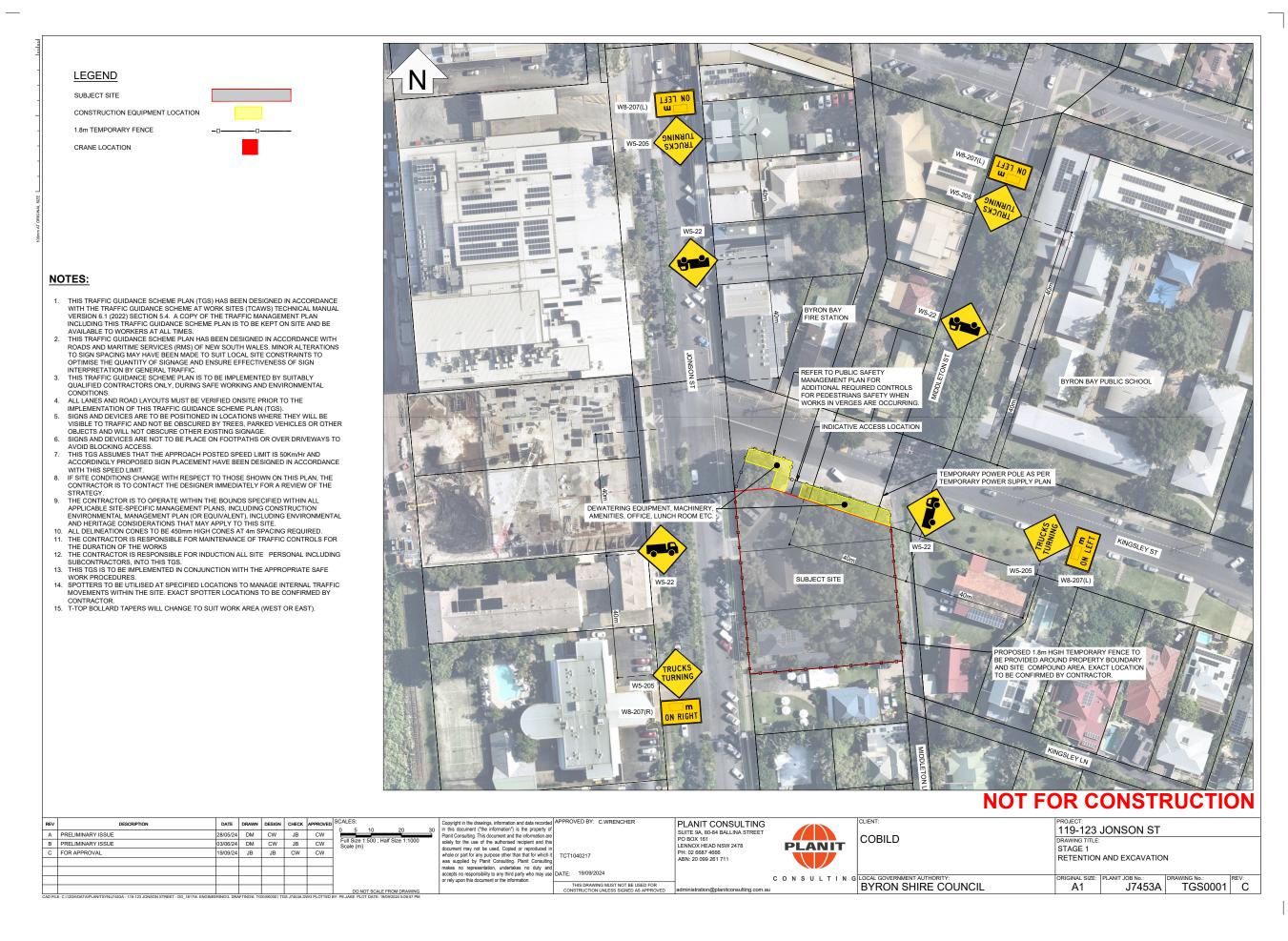
Page **30** of **31** 



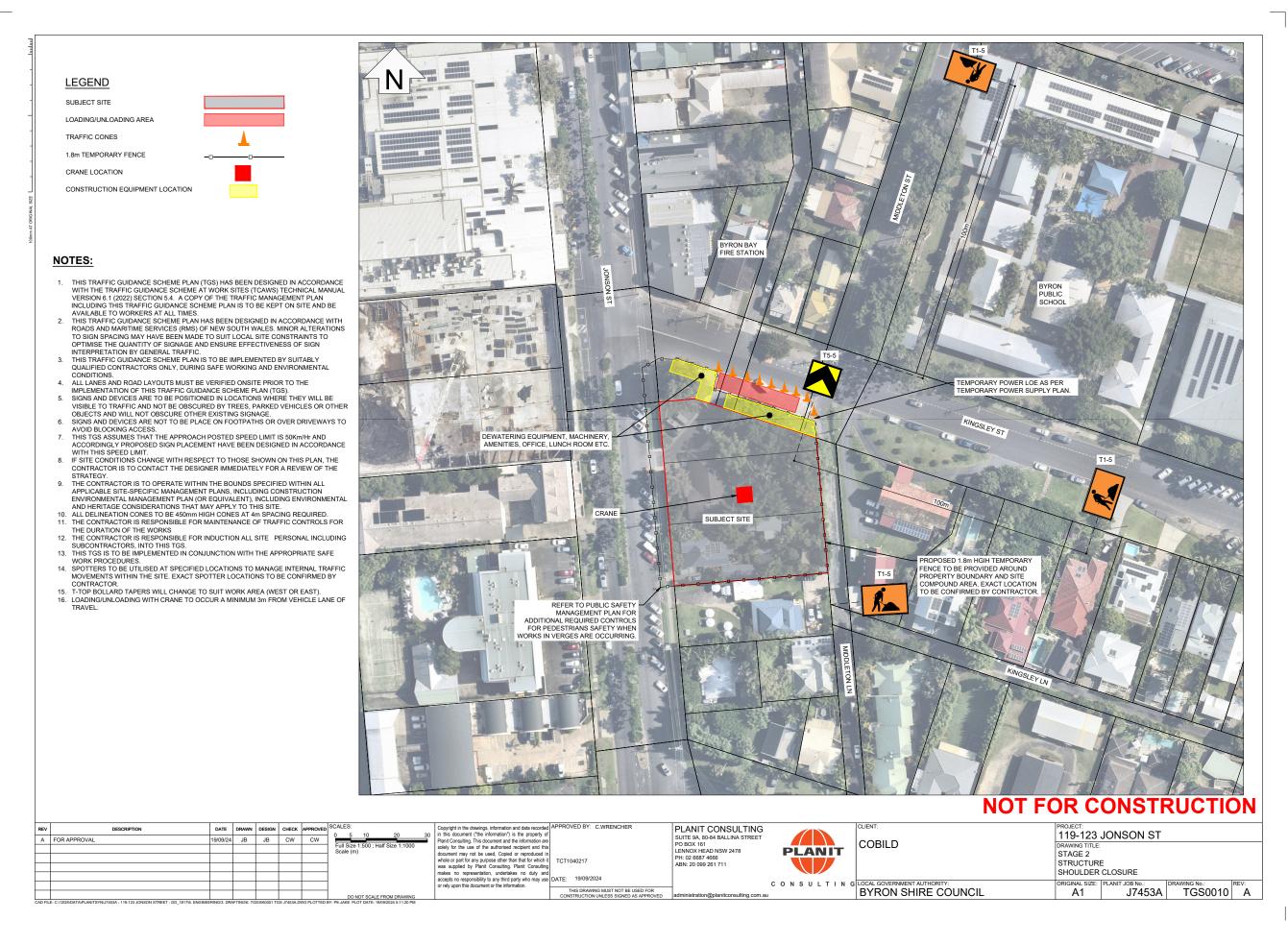
# Appendix A – Traffic Guidance Schemes (TGS)

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#### **REGULATORY MATTERS**



#### **REGULATORY MATTERS**



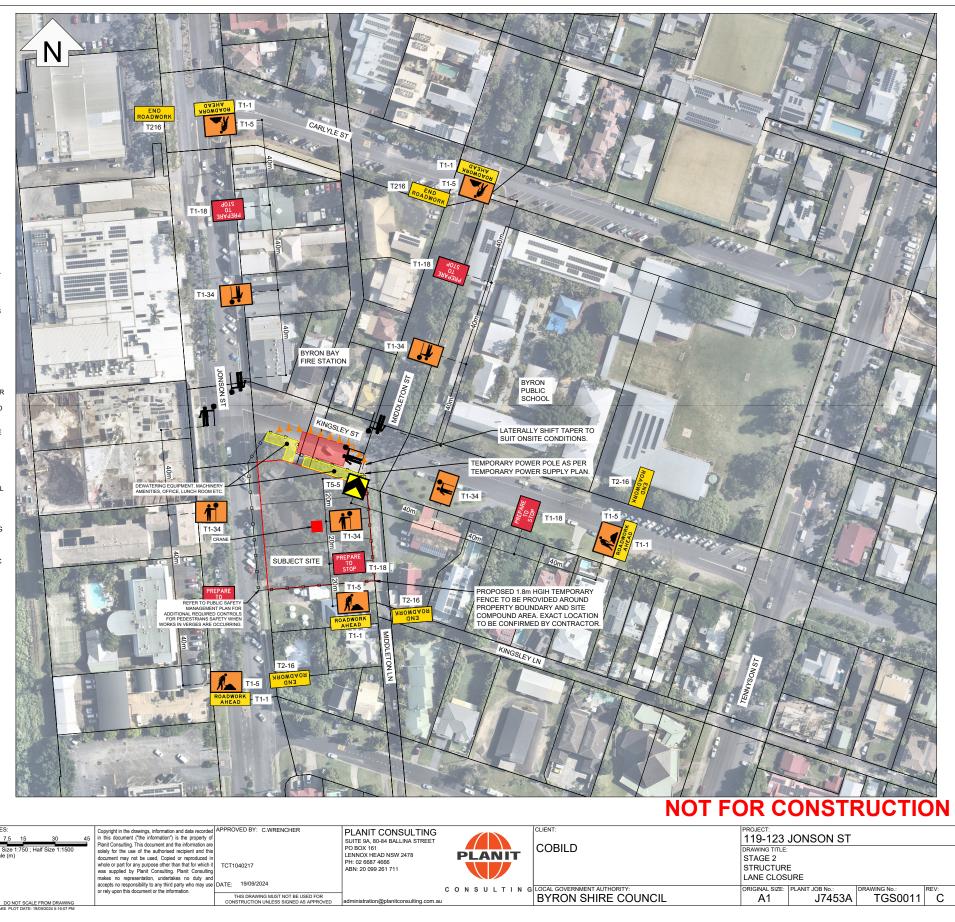
#### **REGULATORY MATTERS**

LEGEND SUBJECT SITE LOADING/UNLOADING AREA TRAFFIC CONES TRAFFIC CONTROLLER 相 1.8m TEMPORARY FENCE CRANE LOCATION CONSTRUCTION EQUIPMENT LOCATION

#### NOTES:

- 1.
- THIS TRAFFIC GUIDANCE SCHEME PLAN (TGS) HAS BEEN DESIGNED IN ACCORDANCE WITH THE TRAFFIC GUIDANCE SCHEME AT WORK SITES (TCAWS) TECHNICAL MANUAL VERSION 6.1 (2022) SECTION 5.4. A COPY OF THE TRAFFIC MANAGEMENT PLAN INCLUDING THIS TRAFFIC GUIDANCE SCHEME PLAN IS TO BE KEPT ON SITE AND BE AVAILABLE TO WORKERS AT ALL TIMES.
   THIS TRAFFIC GUIDANCE SCHEME PLAN HAS BEEN DESIGNED IN ACCORDANCE WITH ROADS AND MARITIME SERVICES (RMS) OF NEW SOUTH WALES. MINOR ALTERATIONS TO SIGN SPACING MAY HAVE BEEN MADE TO SUIT LOCAL SITE CONSTRAINTS TO OPTIMISE THE QUANTITY OF SIGNAGE AND ENSURE EFFECTIVENESS OF SIGN INTERPRETATION BY GENERAL TRAFFIC.
   THIS TRAFFIC GUIDANCE SCHEME PLAN IS TO BE IMPLEMENTED BY SUITABLY QUALIFIED CONTRACTORS ONLY, DURING SAFE WORKING AND ENVIRONMENTAL CONDITIONS.
   ALL LANES AND ROAD LAYOUTS MUST BE VERIFIED ONSITE PRIOR TO THE IMPLEMENTATION OF THIS TRAFFIC GUIDANCE SCHEME PLAN (TGS).
   SIGNS AND DEVICES ARE TO BE POSITIONED BY INCEXTIONS WHERE THEY WILL BE VISIBLE TO TRAFFIC AND NOT BE OBSCURED BY TREES, PARKED VEHICLES OR OTHER OBJECTS AND VILL NOT DBSCURE OTHER EXISTING SIGNAGE.
   SIGNS AND DEVICES ARE NOT TO BE PLACE ON FOOTPATHS OR OVER DRIVEWAYS TO AVOID BLOCKING ACCESS.
   THIS TRAFFIC AND NOT BE PROACH POSTED SPEED LIMIT IS SOKM/Hr AND ACCORDINGLY PROPOSED SIGN PLACEMENT HAVE BEEN DESIGNED IN ACCORDANCE WITH THIS SPEED LIMIT.
   IF SITE CONDITIONS CHANGE WITH RESPECT TO THOSE SHOWN ON THIS PLAN, THE CONTRACTOR IS TO CONTACT THE DESIGNER IMMEDIATELY FOR A REVIEW OF THE STRATEGY 2.
- 3
- 4.
- 5.
- 7.
- 8.
- IF SITE CONDITIONS CHANGE WITH RESPECT TO THOSE SHOWN ON THIS PLAN, THE CONTRACTOR IS TO CONTACT THE DESIGNER IMMEDIATELY FOR A REVIEW OF THE STRATEGY.
   THE CONTRACTOR IS TO OPERATE WITHIN THE BOUNDS SPECIFIED WITHIN ALL APPLICABLE SITE-SPECIFIC MANAGEMENT PLANS, INCLUDING CONSTRUCTION ENVIRONMENTAL MANAGEMENT PLAN (OR EQUIVALENT), INCLUDING ENVIRONMENTAL AND HERITAGE CONSIDERATIONS THAT MAY APPLY TO THIS SITE.
   ALL DELINEATION CONES TO BE 450mm HIGH CONES AT 4m SPACING REQUIRED.
   THE CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF TRAFFIC CONTROLS FOR THE DURATION OF THE WORKS
   THE CONTRACTOR IS RESPONSIBLE FOR INDUCTION ALL SITE PERSONAL INCLUDING SUBCONTRACTORS IS MEDIATED IN CONJUNCTION WITH THE APPROPRIATE SAFE

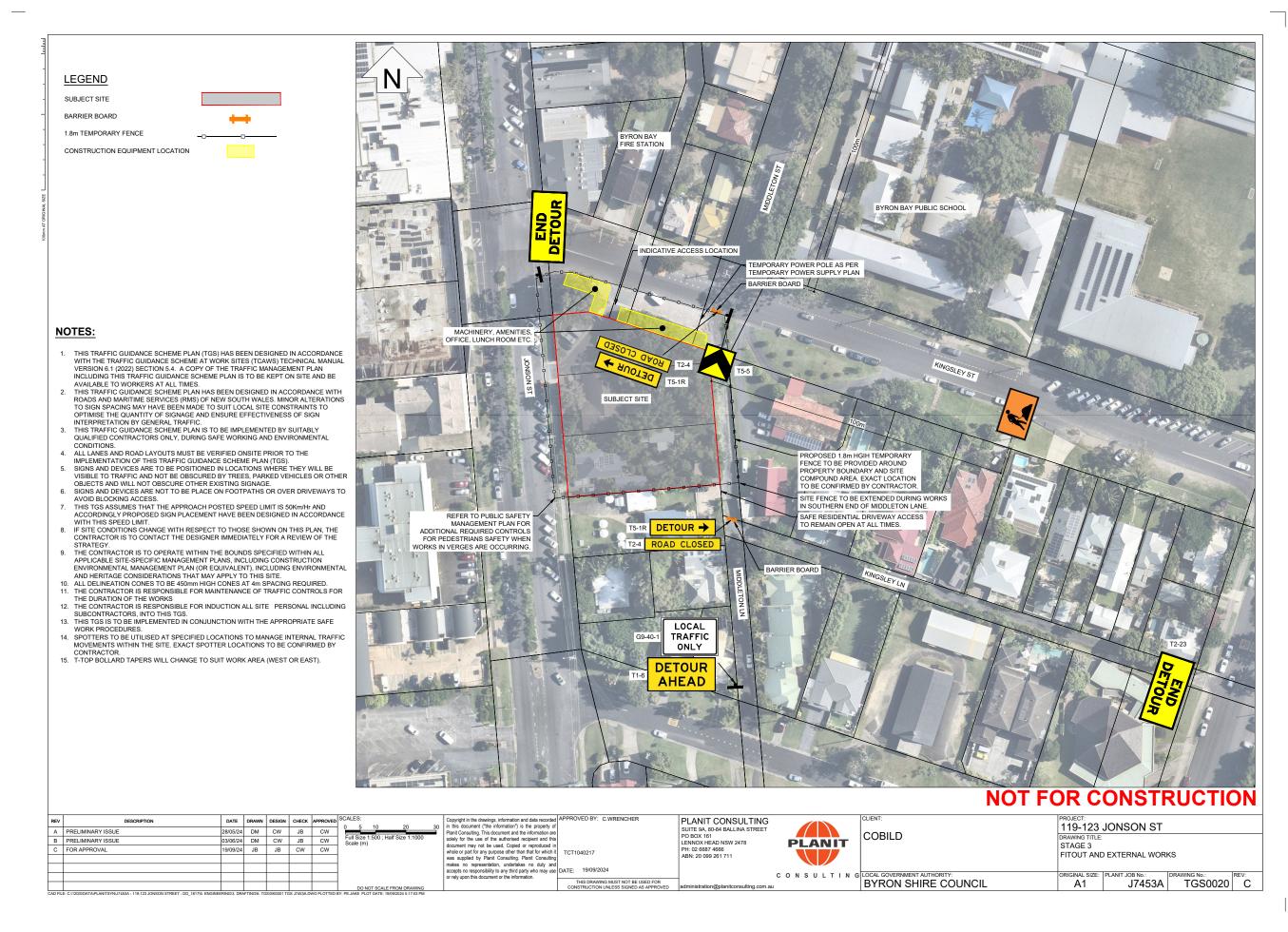
- 13. THIS TGS IS TO BE IMPLEMENTED IN CONJUNCTION WITH THE APPROPRIATE SAFE
- WORK PROCEDURES. WORK PROCEDURES. 14. SPOTTERS TO BE UTILISED AT SPECIFIED LOCATIONS TO MANAGE INTERNAL TRAFFIC MOVEMENTS WITHIN THE SITE. EXACT SPOTTER LOCATIONS TO BE CONFIRMED BY CONTRACTOR. 15. T-TOP BOLLARD TAPERS WILL CHANGE TO SUIT WORK AREA (WEST OR EAST).

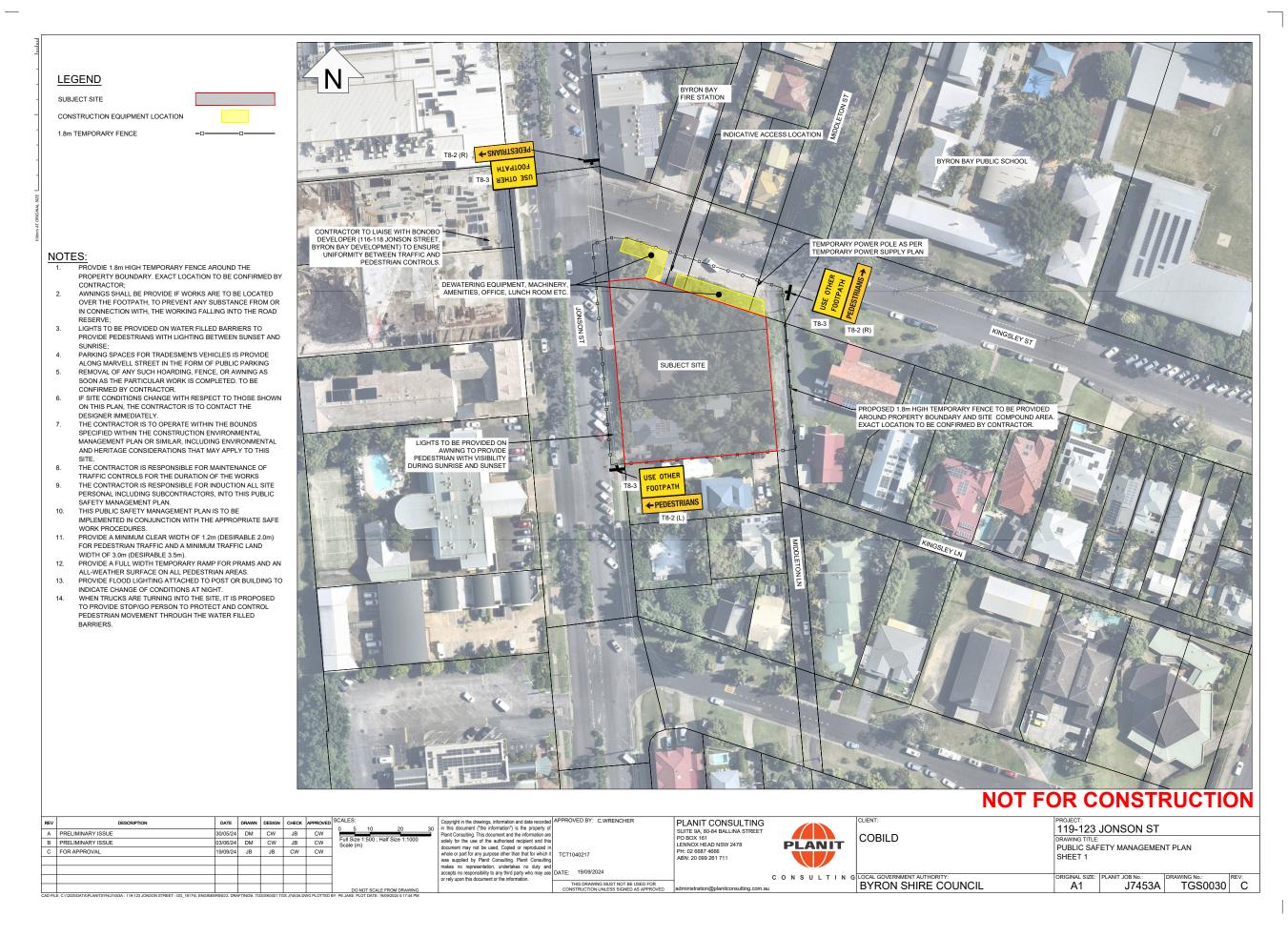


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Agenda

#### **REGULATORY MATTERS**





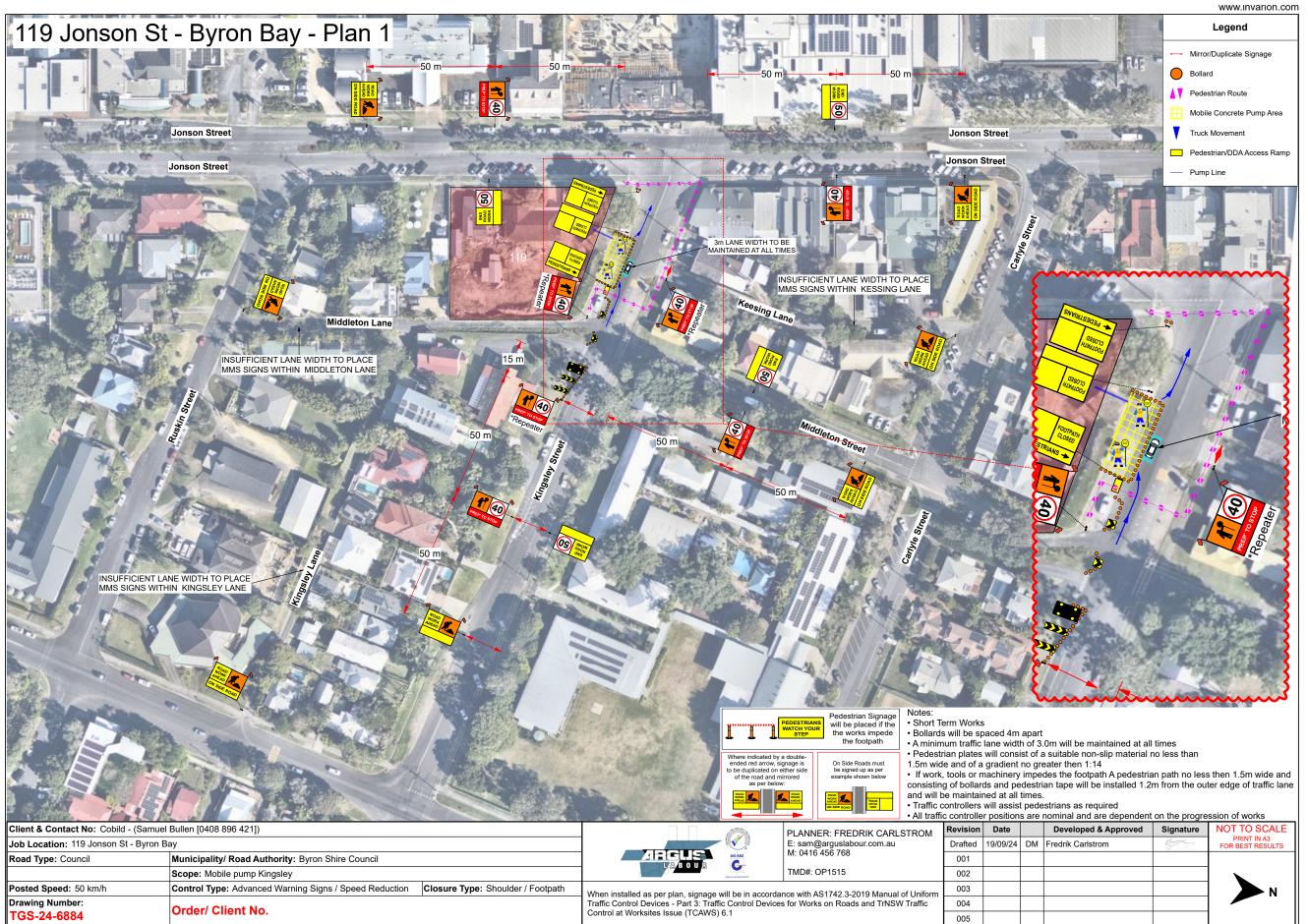
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huhud i i i l -	LEGEND SUBJECT SITE	
ORIGINAL SIZE	1.8m TEMPORARY FENCE -D	BYRON BAY TB-2 (L) TB-3
100mm AT	NOTES:           1.         PROVDIE 1.8m HIGH TEMPORARY FENCE AROUND THE PROPERTY BOUNDARY. EXACT LOCATION TO BE CONFIRMED BY CONTRACTOR;           2.         AWNINGS SHALL BE PROVIDE IF WORKS ARE TO BE LOCATED OVER THE FOOTPATH, TO PREVENT ANY SUBSTANCE FROM OR IN CONNECTION WITH, THE WORKING FALLING INTO THE ROAD	CONTRACTOR TO LIAISE WITH BONOBO DEVELOPER (116-118.JONSON STREET BYRON BAY DEVELOPMENT TO E DISURE UNFORMITY BETWEEN TRAFFIC AND PEDESTRIAN CONTROLS.
	<ul> <li>RESERVE;</li> <li>LIGHTS TO BE PROVIDED ON WATER FILLED BARRIERS TO PROVIDE PEDESTRIANS WITH LIGHTING BETWEEN SUNSET AND SUNRISE;</li> <li>PARKING SPACES FOR TRADESMEN'S VEHICLES IS PROVIDE ALONG MARVELL STREET IN THE FORM OF PUBLIC PARKING S. REMOVAL OF ANY SUCH HOARDING, FENCE, OR AWNING AS SOON AS THE PARTICULAR WORK IS COMPLETED. TO BE</li> </ul>	DEWATERING EQUIPMENT, MACHINERY, MENITIES, OFFICE, LUNCH ROOM ETC.
	CONFIRMED BY CONTRACTOR. 6. IF SITE CONDITIONS CHANGE WITH RESPECT TO THOSE SHOWN ON THIS PLAN, THE CONTRACTOR IS TO CONTACT THE DESIGNER IMMEDIATELY. 7. THE CONTRACTOR IS TO OPERATE WITHIN THE BOUNDS SPECIFIED WITHIN THE CONSTRUCTION ENVIRONMENTAL MANAGEMENT PLAN OR SIMILAR, INCLUDING ENVIRONMENTAL AND HERITAGE CONSIDERATIONS THAT MAY APPLY TO THIS SITE.	SUBJECT SITE TEMPORARY POWER SUPPLY PLAN PROPOSED 1.8m HGH TEMPORARY AROUND PROPERTY BOUNDARY AN
	<ol> <li>STIE.</li> <li>THE CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF TRAFFIC CONTROLS FOR THE DURATION OF THE WORKS</li> <li>THE CONTRACTOR IS RESPONSIBLE FOR INDUCTION ALL SITE PERSONAL INCLUDING SUBCONTRACTORS, INTO THIS PUBLIC SAFETY MANAGEMENT PLAN.</li> <li>THIS PUBLIC SAFETY MANAGEMENT PLAN IS TO BE IMPLEMENTED IN CONJUNCTION WITH THE APPROPRIATE SAFE WORK PROCEDURES.</li> </ol>	EACT LOCATION TO BE CONFIRMED
	<ol> <li>PROVIDE A MINIMUM CLEAR WIDTH OF 1.2m (DESIRABLE 2.0m) FOR PEDESTRIAN TRAFFIC AND A MINIMUM TRAFFIC LAND WIDTH OF 3.0m (DESIRABLE 3.5m).</li> <li>PROVIDE A FULL WIDTH TEMPORARY RAMP FOR PRAMS AND AN ALL-WEATHER SURFACE ON ALL PEDESTRIAN AREAS.</li> <li>PROVIDE FLOOD LIGHTING ATTACHED TO POST OR BUILDING TO INDICATE CHANGE OF CONDITIONS AT NIGHT.</li> </ol>	
	14. WHEN TRUCKS ARE TURNING INTO THE SITE, IT IS PROPOSED TO PROVIDE STOP/GO PERSON TO PROTECT AND CONTROL PEDESTRIAN MOVEMENT THROUGH THE WATER FILLED BARRIERS.	E CONTRACTOR OF CO
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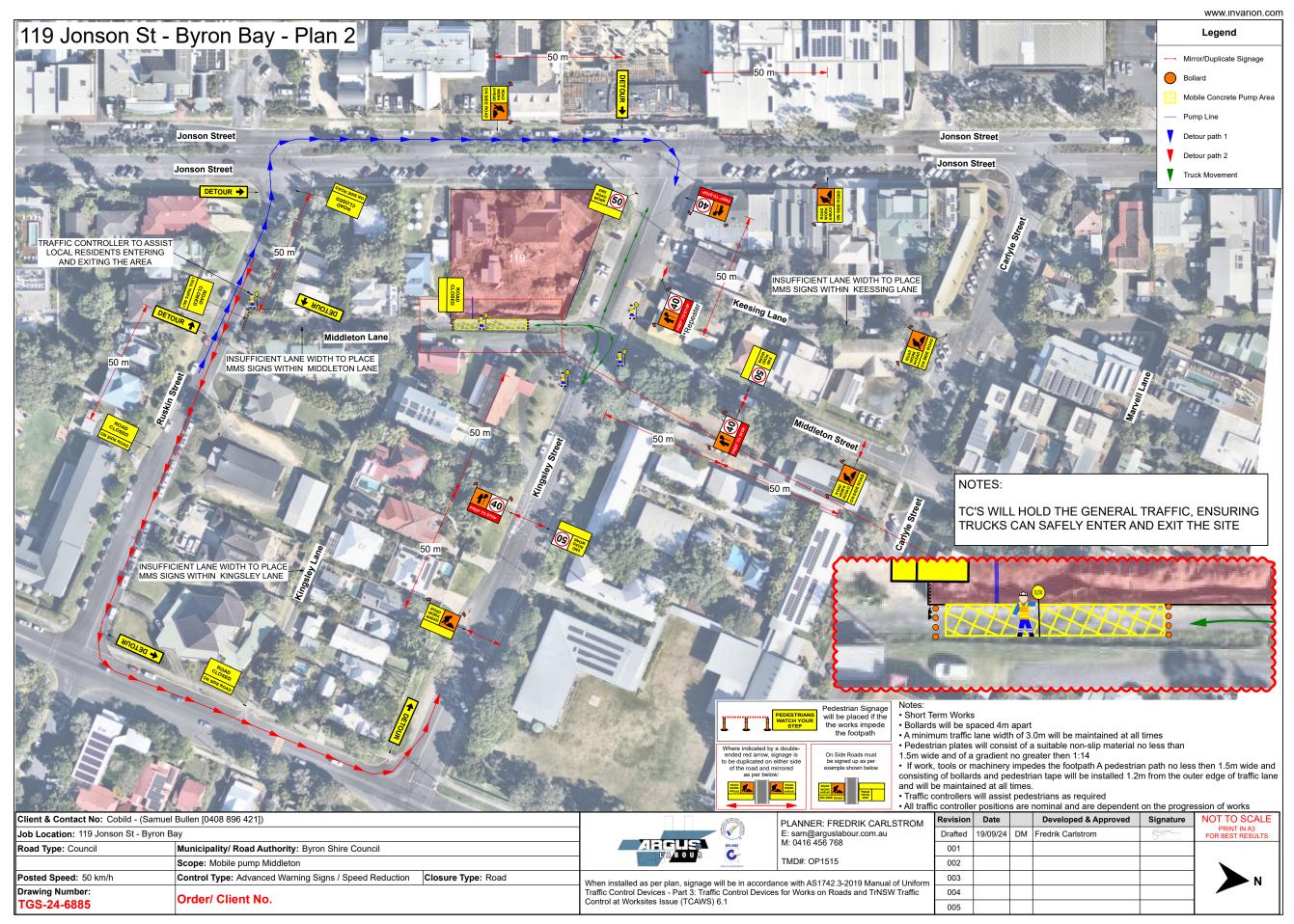
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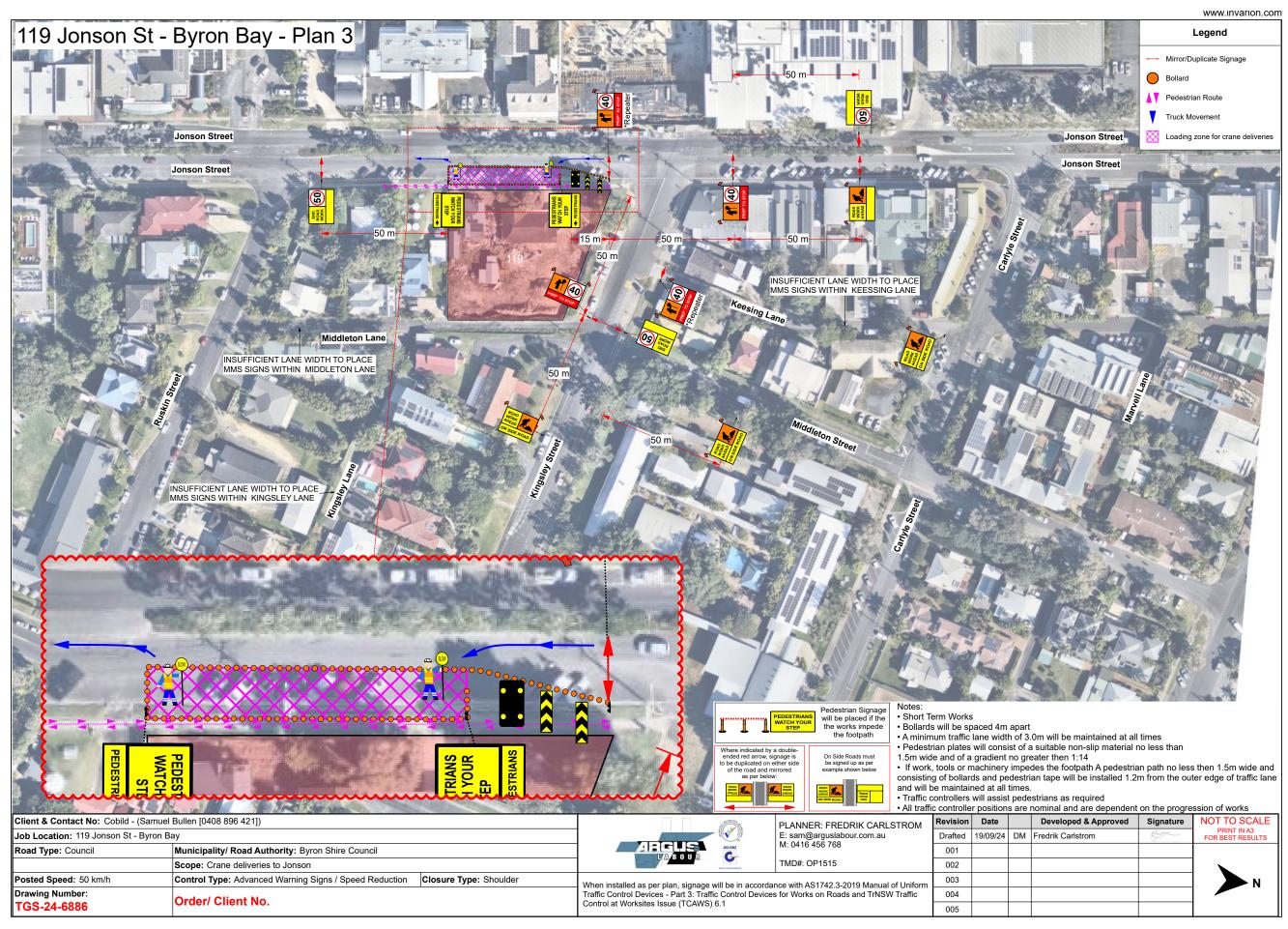
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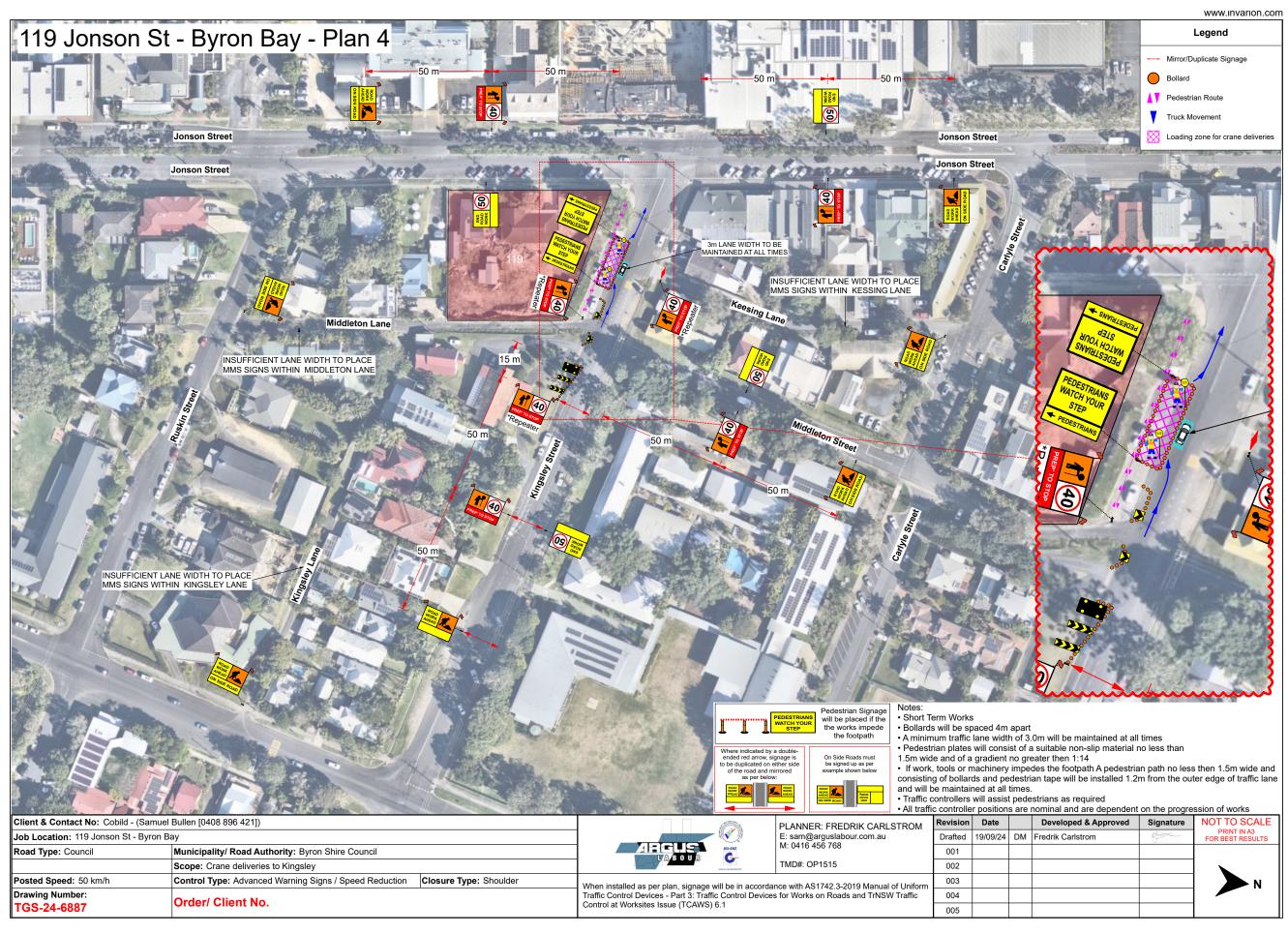


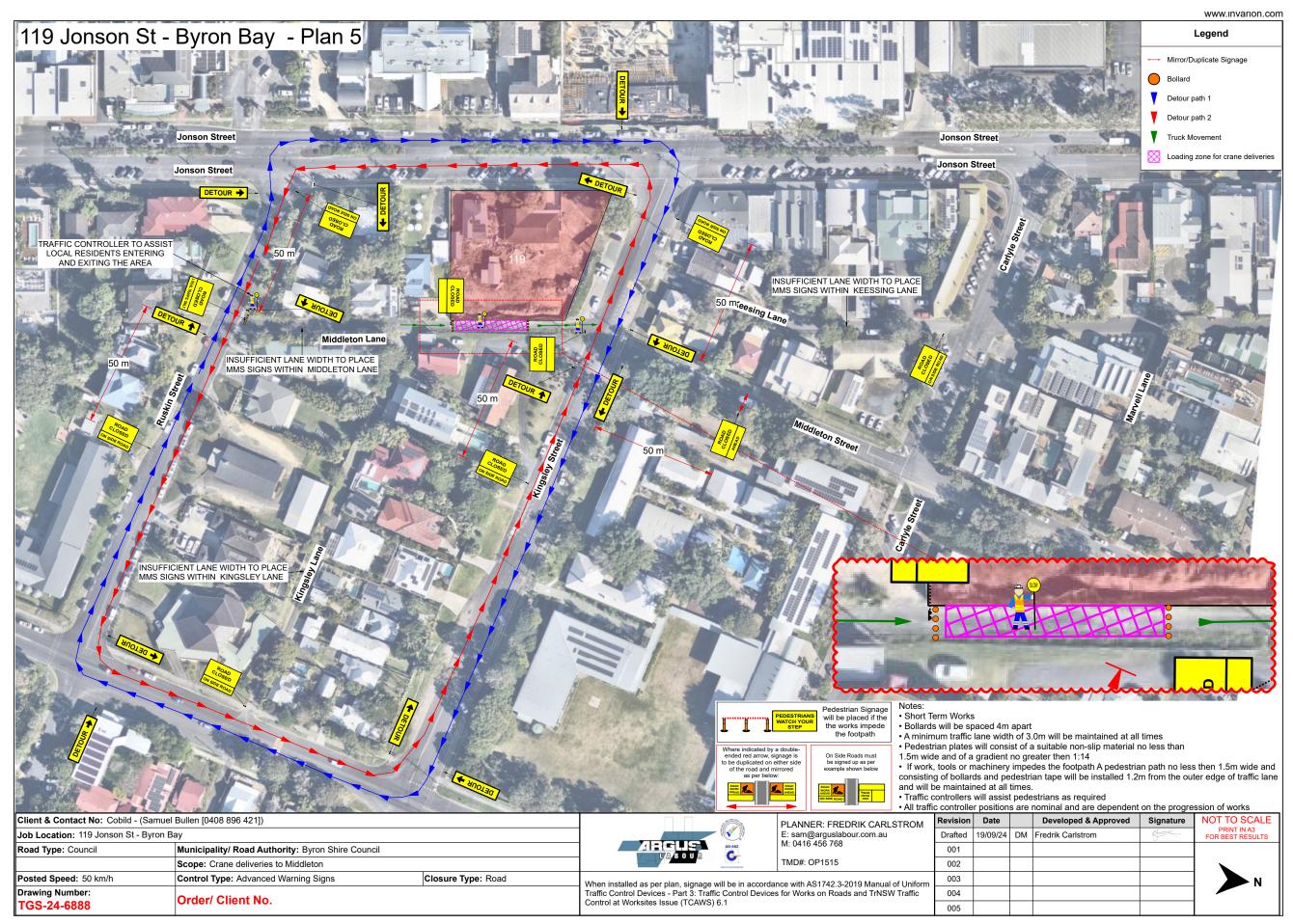


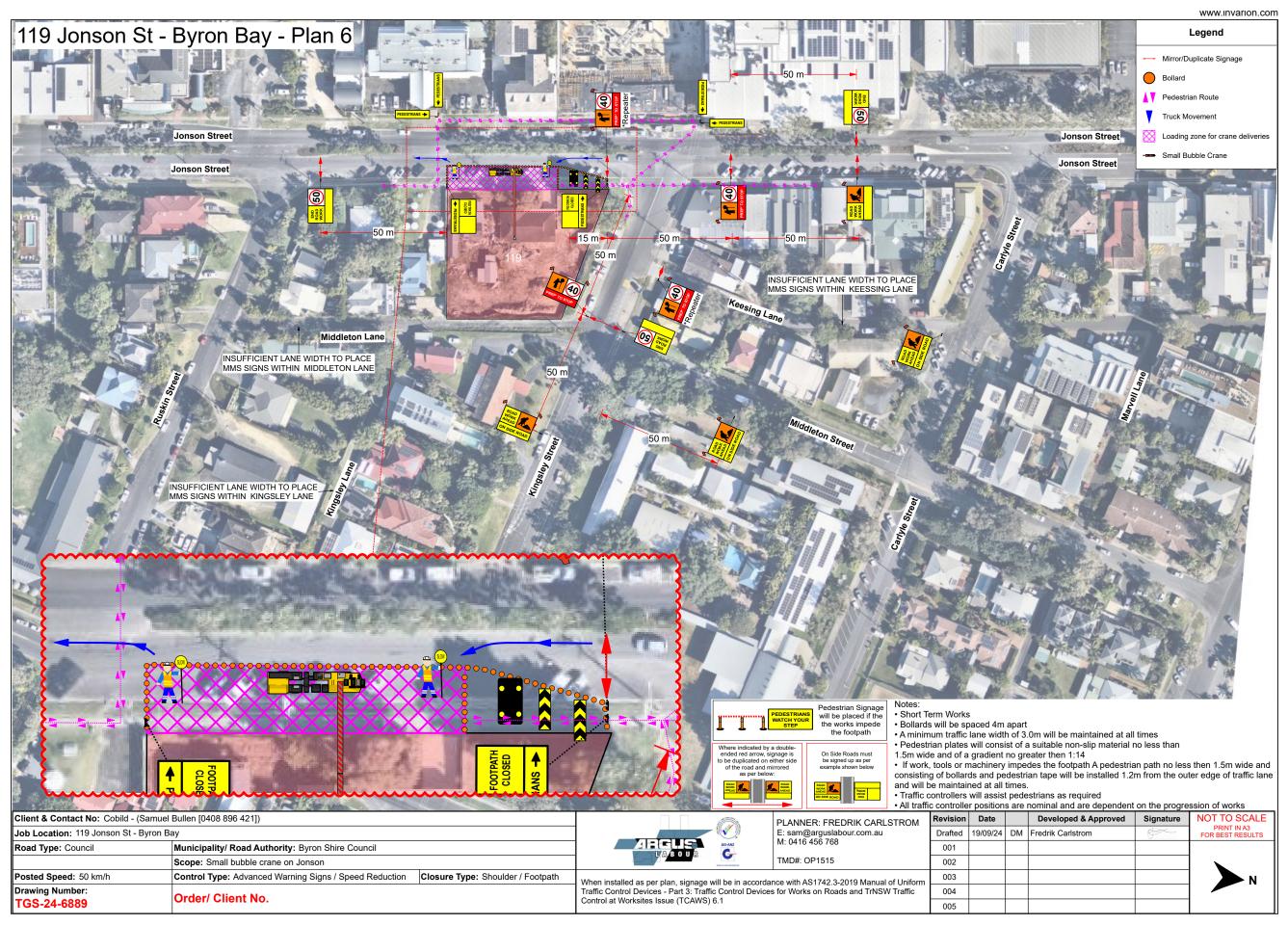
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LOCAL TRAFFIC COMMITTEE MEETING

## Report No. 6.2 Slessor Lane, Brunswick Heads - No Stopping area

**File No:** I2024/1373

## 5 Purpose

This report proposes new parking signage and line marking on Slessor Lane to prevent driver confusion and avoid conflicts between vehicles and pedestrians. The proposed changes also aim to improve the safety of residents, businesses, workers, emergency services, Council Staff, and other road users.

10 This is in response to the Notice of Motion raised by Cr Michael Lyon during the Council Planning meeting held on 1 August 2024 and resolved under Council resolution 24-364, which states that Council:

 Investigates, through the Local Traffic Committee, changes to Slessor Lane in Brunswick Heads, where recent development activity on the corner of Tweed Street is causing conflicts between patrons and residents, including investigating changing it to oneway out to Tweed Street.

2. Investigates other laneways in Brunswick Heads that may benefit from changes to traffic conditions.

Note – Part 2 of the above resolution will be dealt with separately.

## 20 Information/background

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Three tenancies have been created in the building where the former mechanic used to operate on the corner of Tweed Street and Slessor Lane in Brunswick Heads. These include a retail store, a café and a proposed gym which is under construction. These tenancies have all been operated independently and all relied on provisions relating to existing use under a SEPP. Staff have done a great job of attempting to bring this into compliance and a DA is expected to be lodged shortly to deal with the change of uses and the increase in parking that is needed to service them.

Council has recently received numerous requests from the community for investigations of traffic impacts which have been caused by changes in land use, typically approved as a new development application with council, or utilising existing development consent. Traffic impacts on Slessor Lane associated with the development of 38 Tweed Street are displayed in Figure 1 below.

## LOCAL TRAFFIC COMMITTEE MEETING



Figure 1 – Construction traffic using Slessor Lane for parking

Vehicles are currently parking in Slessor Lane to access the tenancies within the development at 38 Tweed Street. This is creating confusion and safety hazards between residents, patrons and pedestrians.

5 Vehicles parking on Slessor Lane also increases the difficulty of refuse collection and servicing, given the narrow roadway.

Council staff recommend installation of yellow line marking and 'No Stopping' signs on both sides of Slessor Lane adjacent to the 38 Tweed Street development. See Figure 2 below for locations.

## LOCAL TRAFFIC COMMITTEE MEETING



Figure 2: Map showing no parking area on Slessor Lane, Brunswick Heads

Installing new signs and new line marking will reduce the likelihood of vehicles parking at the corner of the laneway and conflicts between patrons and residents, while a permanent solution including one-way laneways is investigated for the wider Brunswick Heads area.

It is anticipated that Council enforcement officers and Police officers would use their discretion when enforcing this signage.

See Figure 3 for an example of the signage type.

The proposed signage would replace existing in the same location.

NO STOPPING

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Figure 3: Example of No Stopping sign

## LOCAL TRAFFIC COMMITTEE MEETING

## **RECOMMENDATION:**

That the Local Traffic Committee support installation of yellow line marking and "No Stopping" signage on Slessor Lane where it meets Tweed Street, Brunswick Heads.

LOCAL TRAFFIC COMMITTEE MEETING

## Report No. 6.3 Mullumbimby Road Upgrade

File No:

12024/1439

The purpose of this report is to gain Local Traffic Committee and Council support for the proposed changes to Mullumbimby Road.

The proposed works will improve road widths, turning lanes, drainage and the poor condition road surface, creating a safer road environment.

Pavement repair works will be undertaken from Mullumbimby to Gulgan Road while pavement widening and drainage improvements will be focused around the stretch of Mullumbimby Road from McAuleys Lane to Gulgan Road.

The funding for this project is divided into two Stages. Stage 1A is Blackspot, Council and R2R funding for improvements at McAuleys Lane. Stage 1B is Betterment and EPAR funding for the pavement repairs to the full length of the road and drainage improvements centred around Gulgan Road. Draft plans for both Stages have been attached to this report (Attachment 1 E2024/130657 and Attachment 2).

- Design is currently being finalised on the project, however, lines and signs will generally be in accordance with the attached plans. Approval is sought in prinical and should any significant changes occur to the proposed signs and linemarking then the plans will be resubmitted to LTC for approval.
- 20 The main changes to the road layout are as follows:
  - Road widening to maintain minimum lane widths of 3.5m and standard shoulder widths of 1.75m and an absolute minimum width of 0.5m where highly constrained.
  - Introduction of a left turn deceleration lane for McAuleys Lane and a protected right turn lane.
- The Saddle Road and the residential driveway located between The Saddle Road and McAuleys Lane will formally signed and linemarked as left in left out only. This is due to the dangerous and inadequate stopping sight distance , manoeuvre sight distance, minimum gap sight distance, and safe intersection sight distance at these accesses.
- 30 A meeting was held with Transport to discuss the possibility of lowering the speed limit on Mullumbimby Road to address the sight distance issues at the Saddle Road and McAuleys Lane intersections. At this time Transport was not supportive of lowering the speed limit from 80km/hr in this location.

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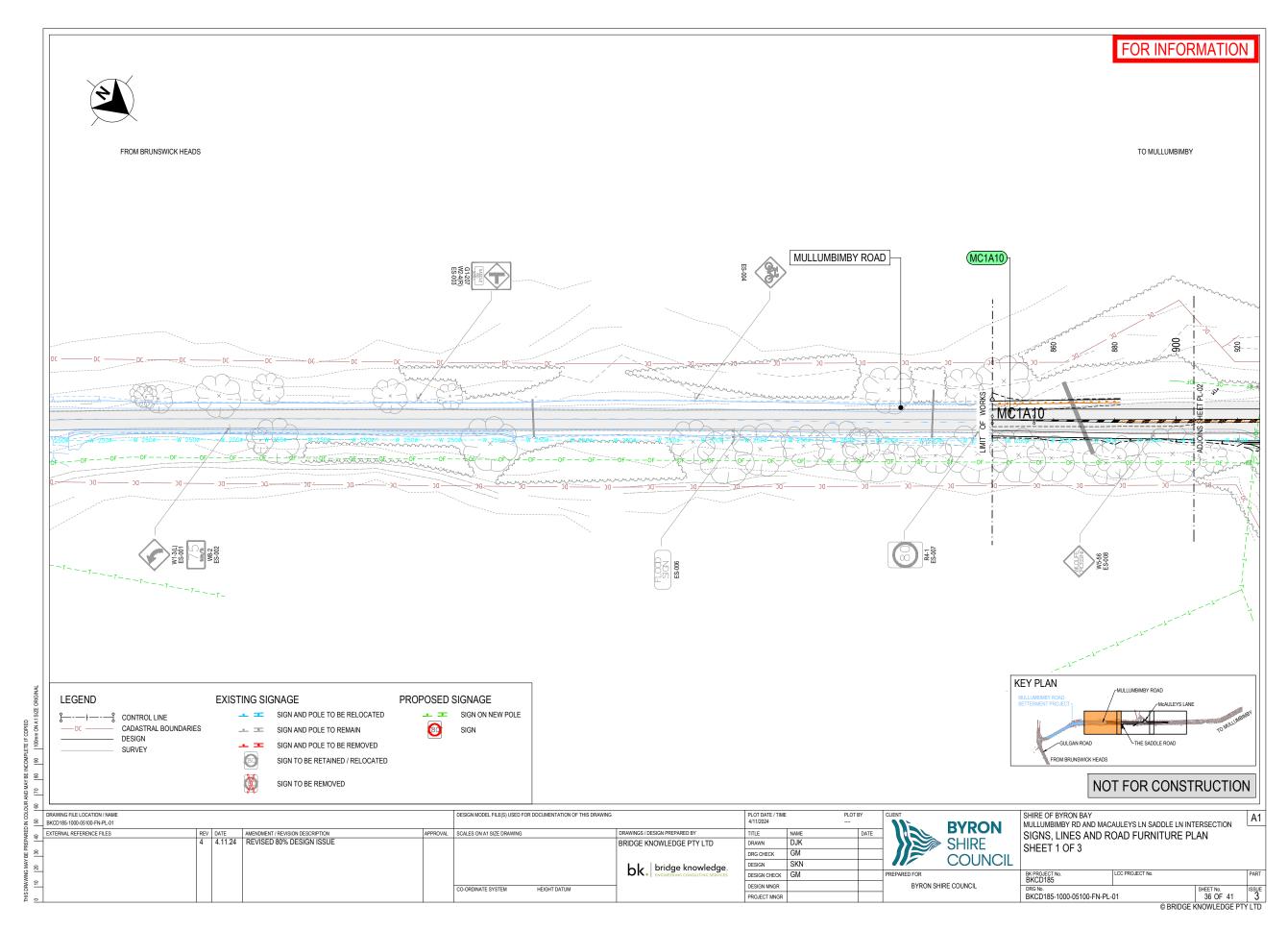
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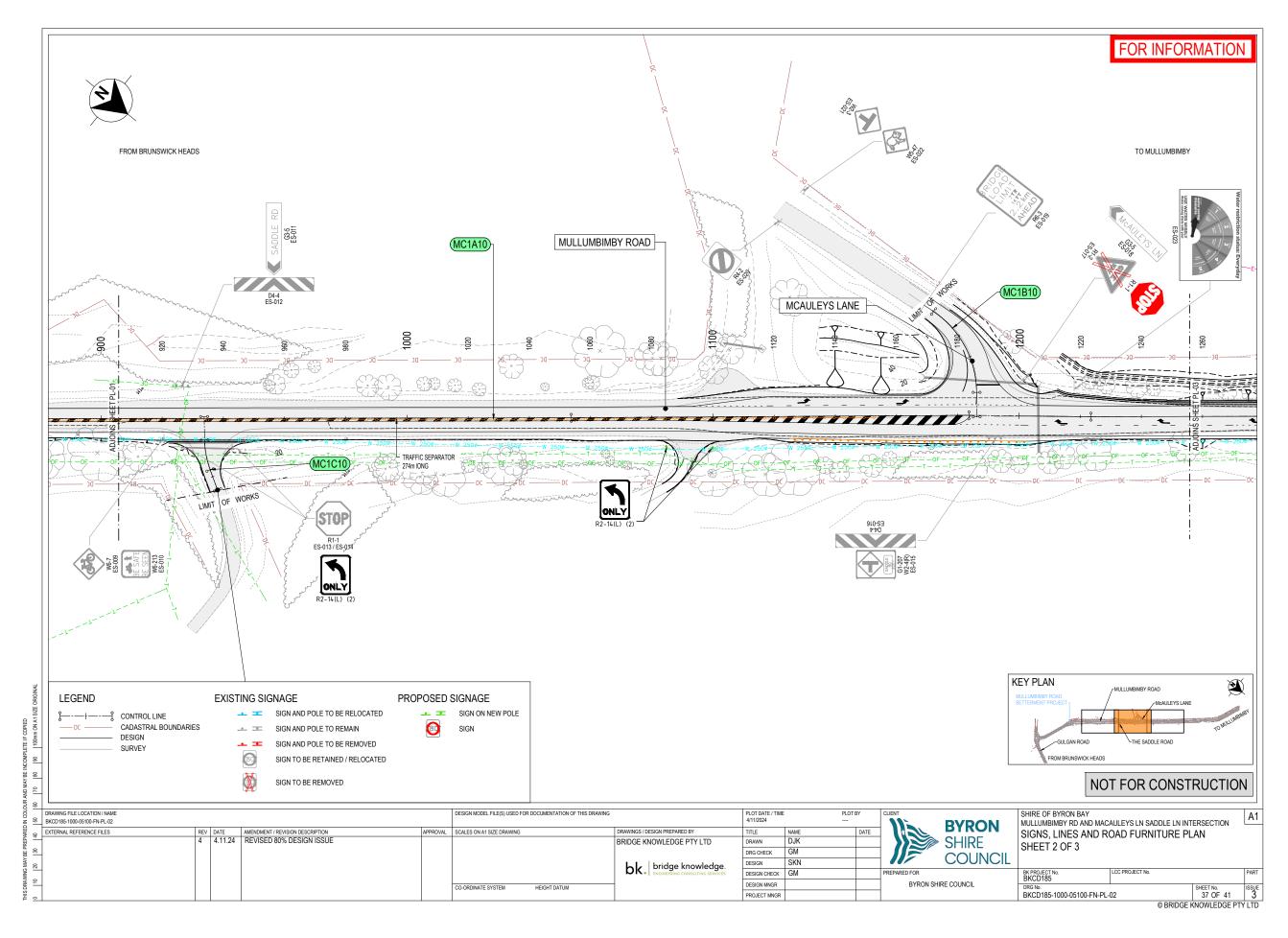
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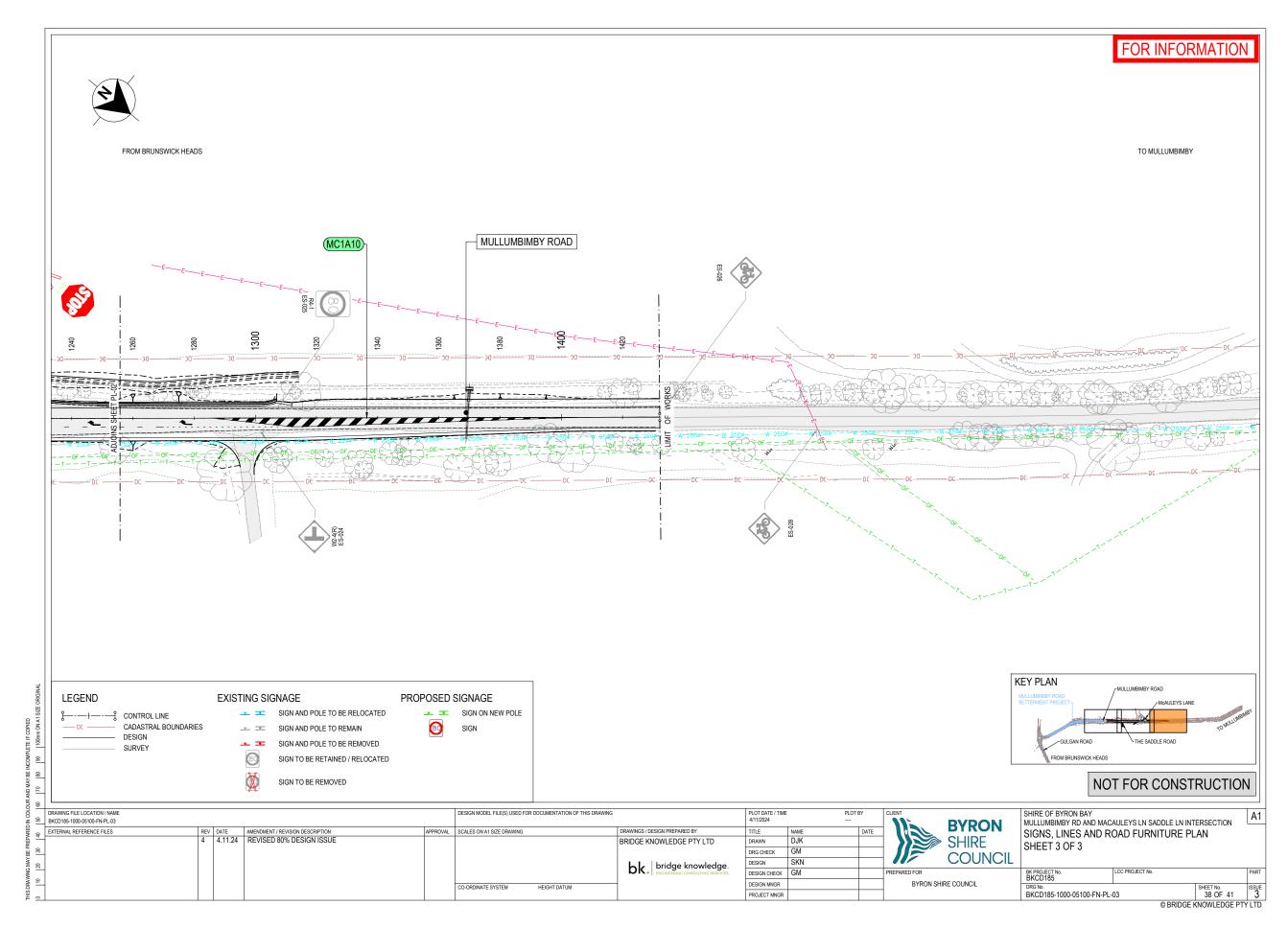
That Council supports the signage, line markings and traffic control devices associated with the Mullumbimby Road works generally in accordance with Attachment 1 (E2024/130657) and Attachment 2 (E2024/130677)

## Attachments:

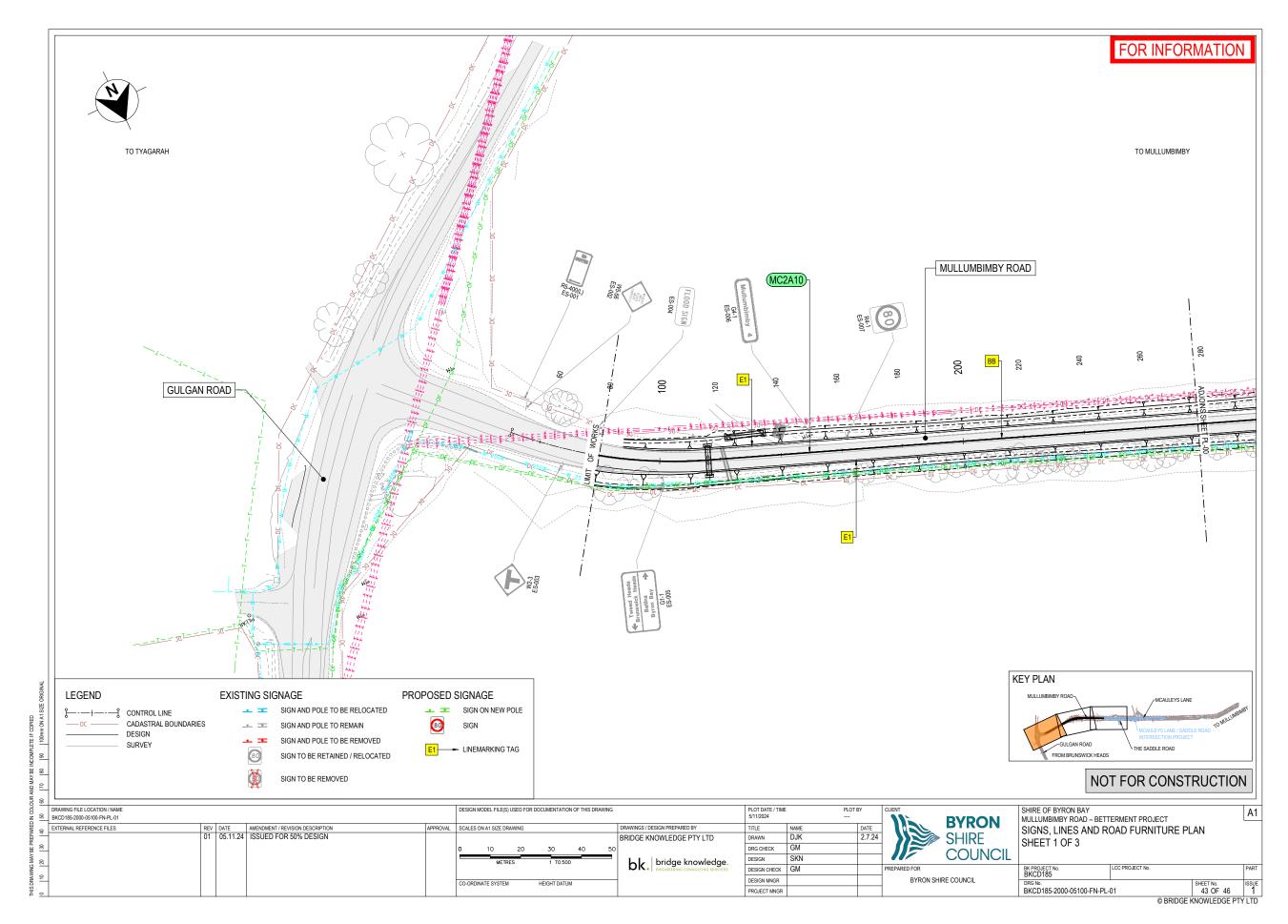
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- 1 Mullumbimby Road McAuleys Design Plans, E2024/130657, page 56 🗓 🛣
- 2 Mullumbimby Road Betterment Design Plans LTC, E2024/130677, page 59 🗓 🖾

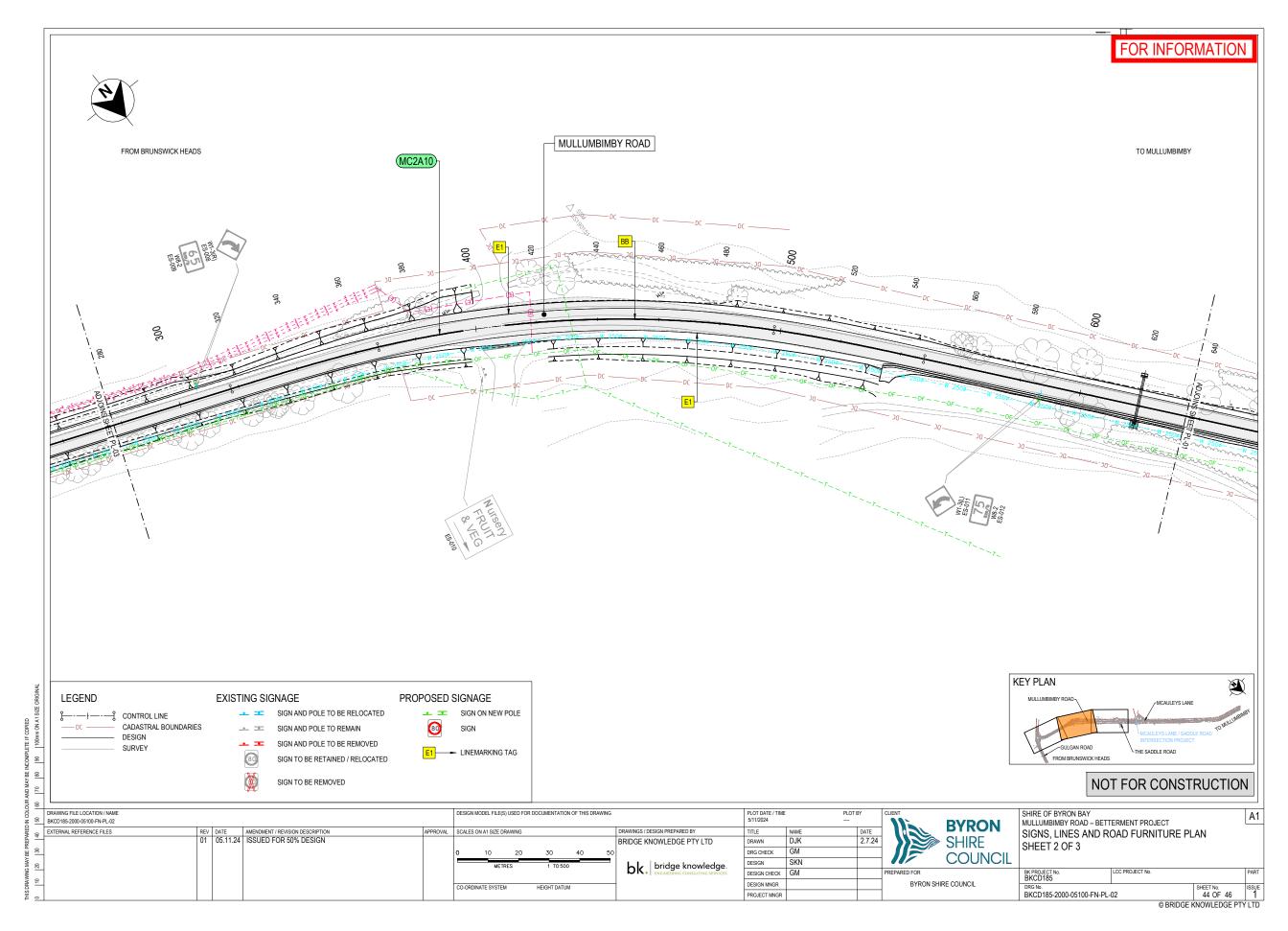


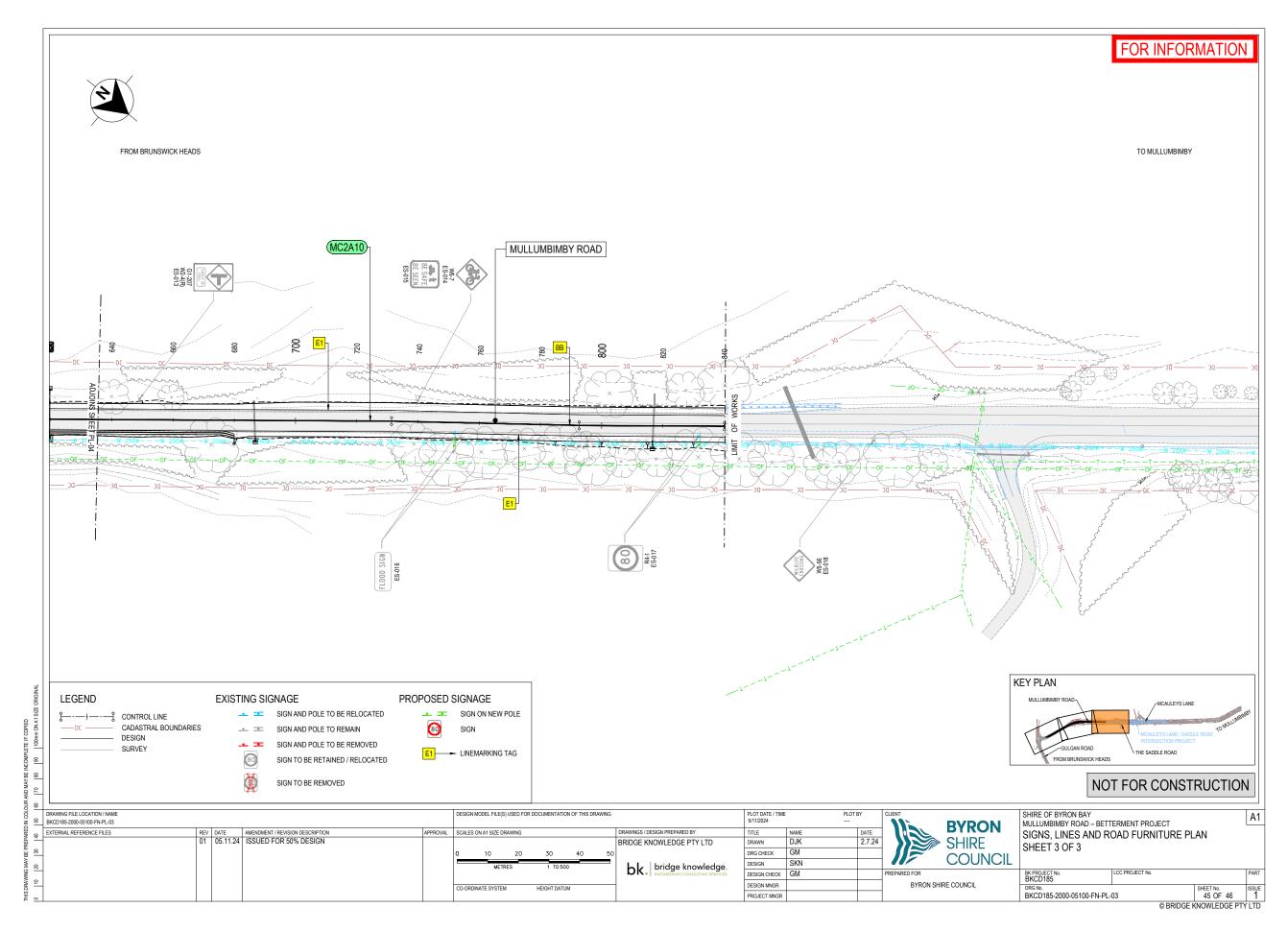




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LOCAL TRAFFIC COMMITTEE MEETING

## Report No. 6.4 Lighthouse Road - Shared path - 'Get NSW Active' Grant Application

File No: 12024/1459

5 The purpose of this report is to gain Local Traffic Committee and Council support for the proposed changes to Lighthouse Road, Byron Bay.

The project entails construction of a footpath commencing at the intersection of Brooke Road, extending toward the lighthouse, for 852m. Inclusive of three new crossings, new signage, and line marking, as per attachment 1.

10 The width of the footpath varies along the alignment of Lighthouse Road, primarily due to the steep topography of the land and limited available space. The following describes the footpath alignment along the road:

CH0 - 33 = 1.8m path | Footpath Type 1, Approach to intersection crossing with Brooke Road, also have a width limitation due to an existing bus stop that will be maintained.

15 There is a 2.5m buffer zone which is used as the width for the bus to pull over. Road speed limit of 50km/hr – likely to be reduced to 30km/h, or at a maximum to 40km/h, as per TfNSW speed limit review of Lighthouse Road - awaiting TfNSW formal support letter.

CH33 - 210 = 2.5m = Shared path | Footpath Type 2, Raised boardwalk due to steep embankment for the length adjacent to Lighthouse Road. There is a 0.5m buffer zone with either bollards or a cycle friendly railing. Road speed limit of 50km/hr - likely to be reduced to 30km/h, or at a maximum to 40km/h as per TfNSW speed limit review of Lighthouse Road - awaiting TfNSW formal support letter.

CH210 - 842 = 1.5m = pedestrian path (uphill cyclists directed to continue along Lighthouse Rd, downhill cyclists to use one-way vehicle lane) | Footpath Type 1, buffer
zone of 0.25m with guide-posts adjacent to one way traffic with a posted 10km/hr shared zone speed limit.

Staff is now working on the 'Get NSW Active' FY2025/25 Grant application to fund construction.

30 Staff has applied for the same Grant FY2024/25 which was unsuccessful. As a result of that, staff has worked to improve the application based on TfNSW feedback.

Based on the project specific feedback, staff has consulted with TfNSW regarding the speed limit of the road and it happened to be at the same time, TfNSW was already undertaking a speed limit review for Lighthouse Road.

The speed limit is being proposed to reduce from 50km/h to 30km/h, and the 10km/h shared space to continue.

A Review of Environmental Factors and a Safety in Design Report have been completed for the proposed work.

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## LOCAL TRAFFIC COMMITTEE MEETING

## **RECOMMENDATION:**

That the Local Traffic Committee endorses the detail design of the Lighthouse Road footpath extension project, as per drawings (E2024/126445) in attachment 1.

## 5 Attachments:

1 100%\_SET\_LIGHTHOUSE RD - shared path 23.10.2024, E2024/126445 , page 65 🗓 🛣

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Locality Map



## Shared Path

PM-22-1386

Lighthouse Road, Byron Bay, NSW

For: Byron Shire Council



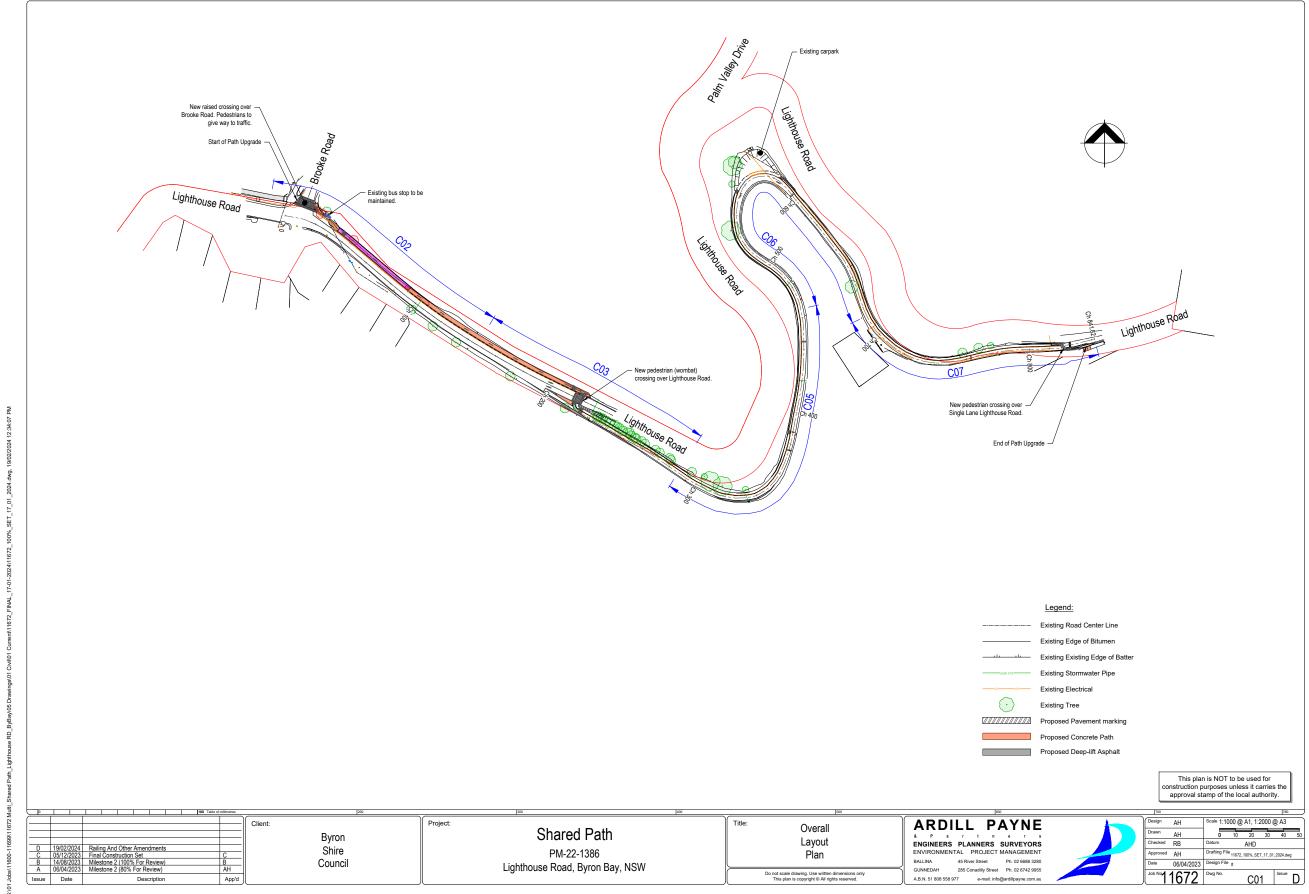
ENVIRONMENTAL PROJECT MANAGEMENT

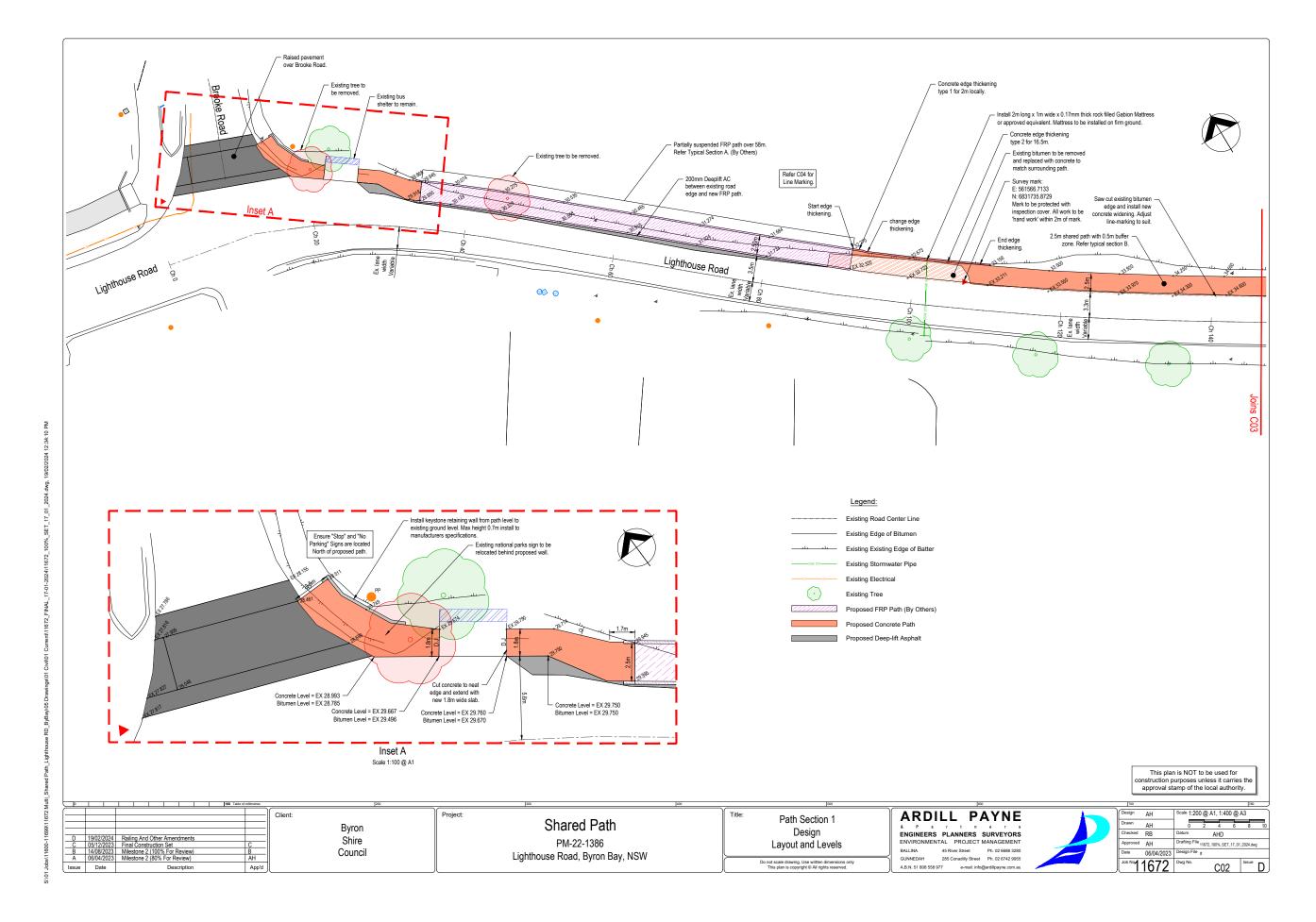
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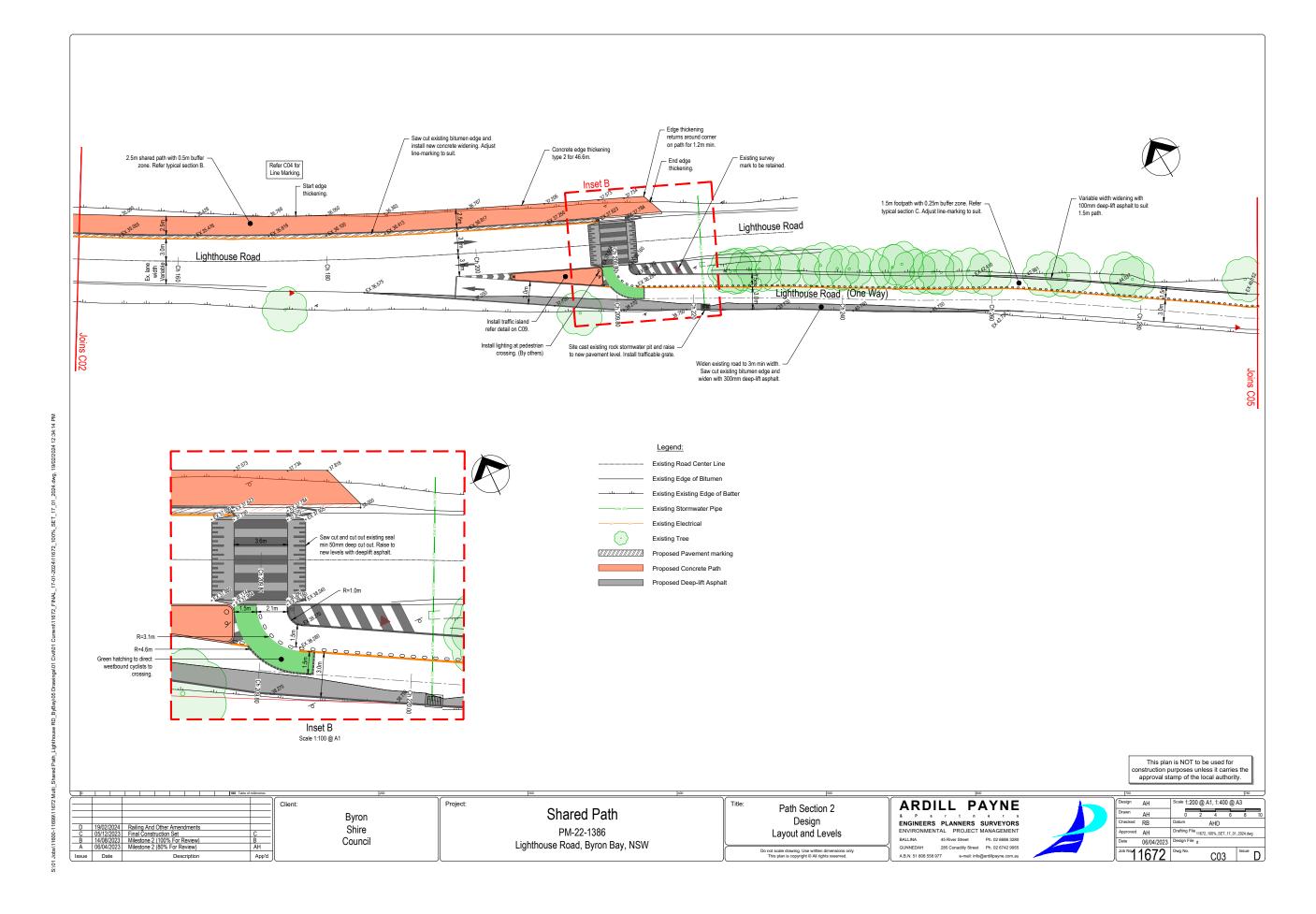
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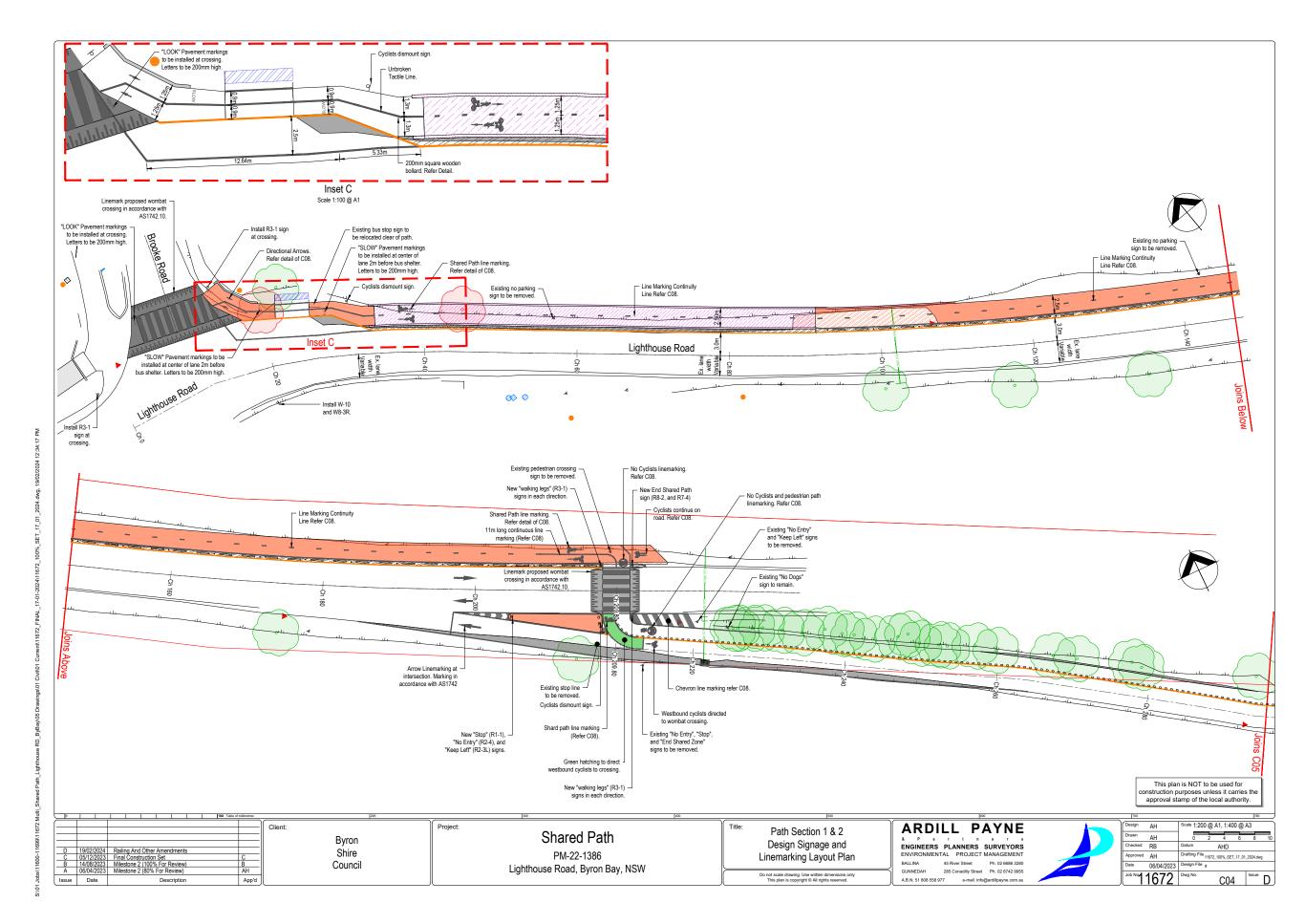
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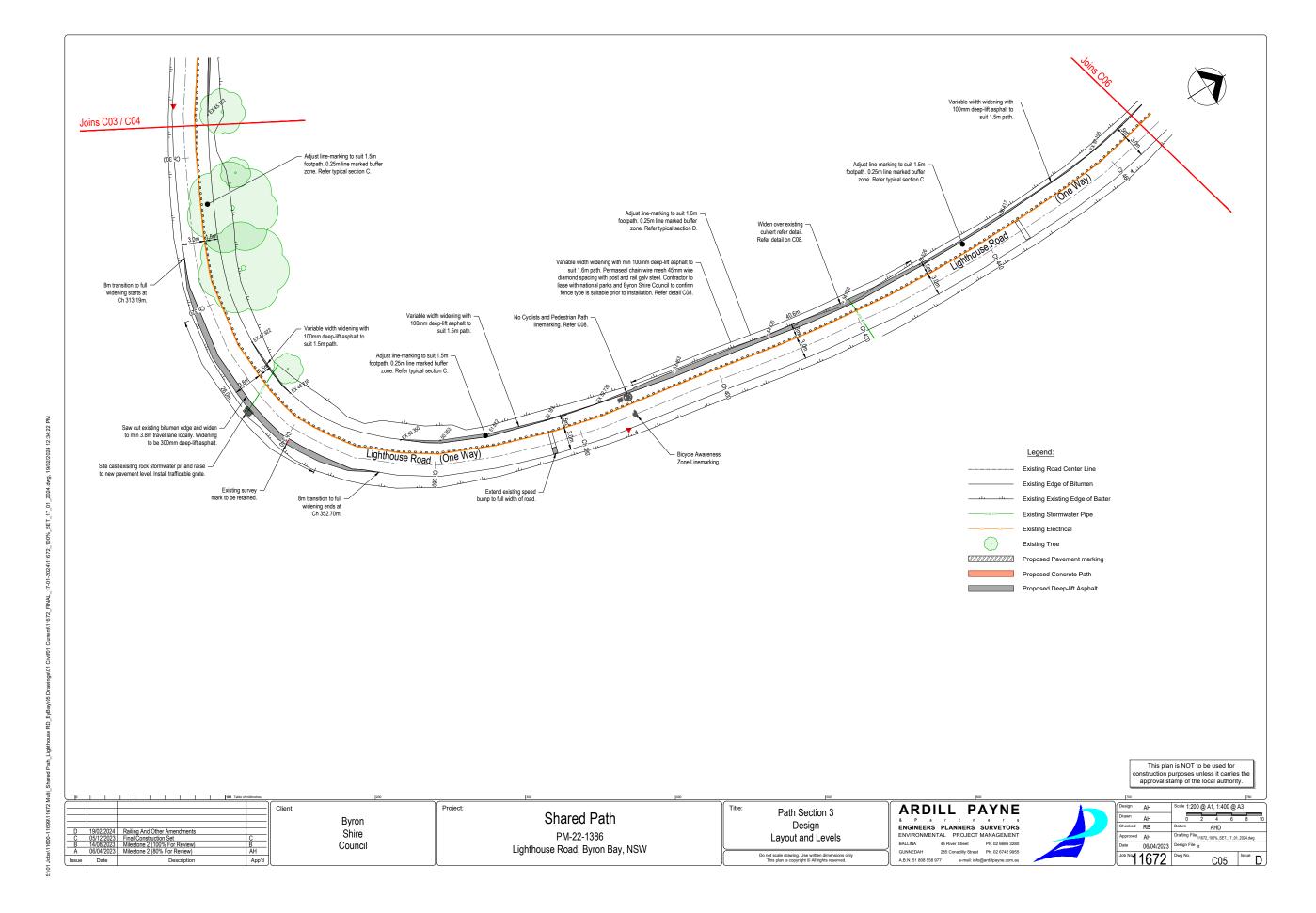
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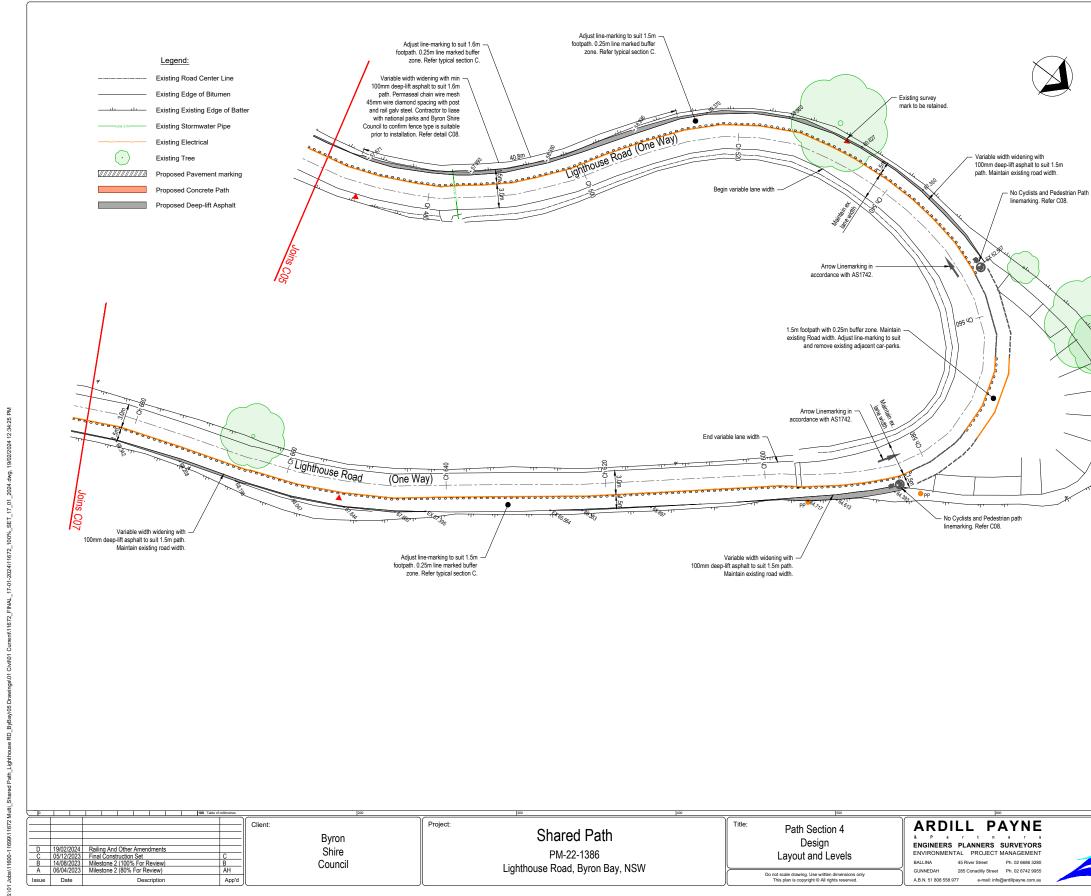




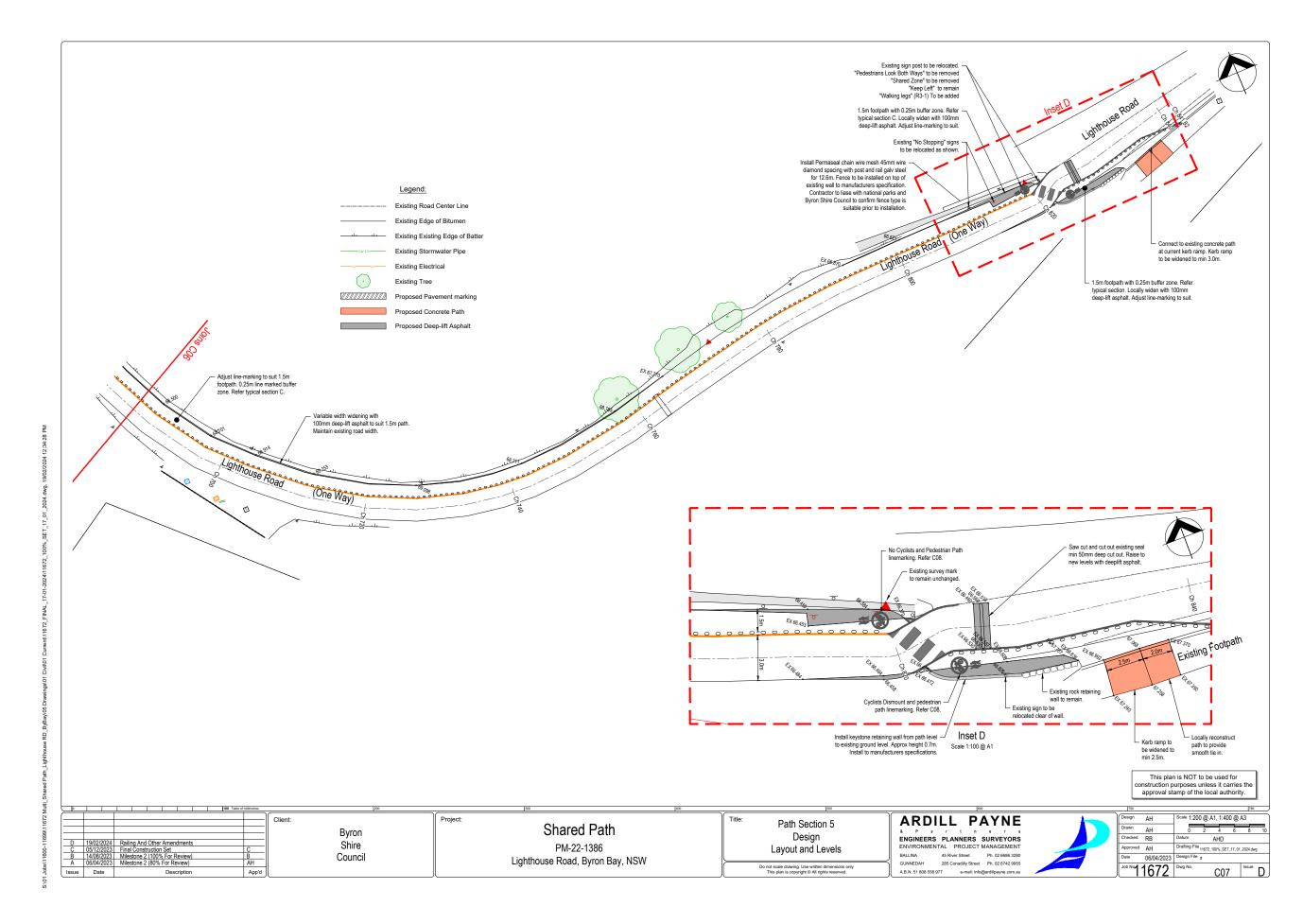


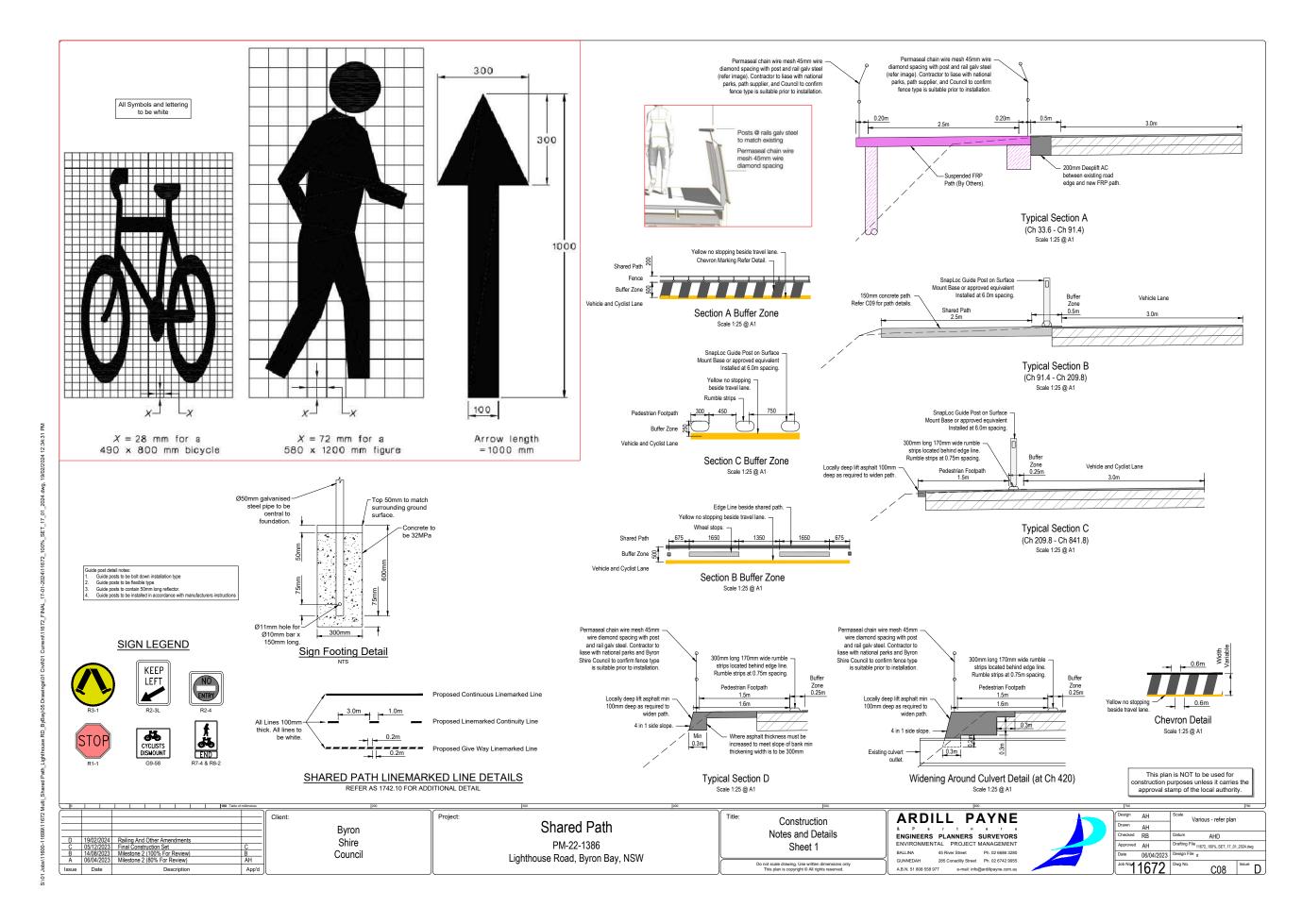


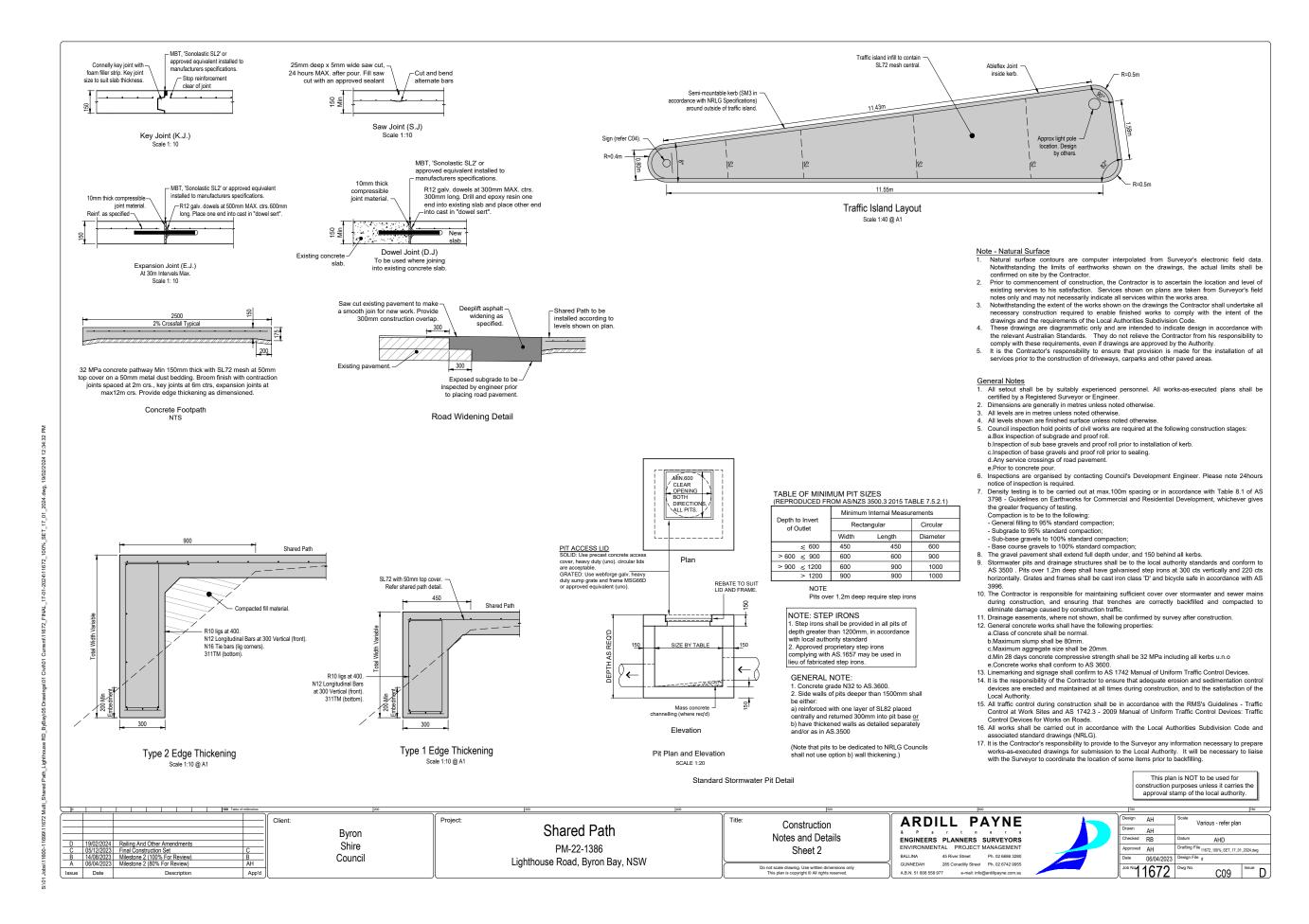


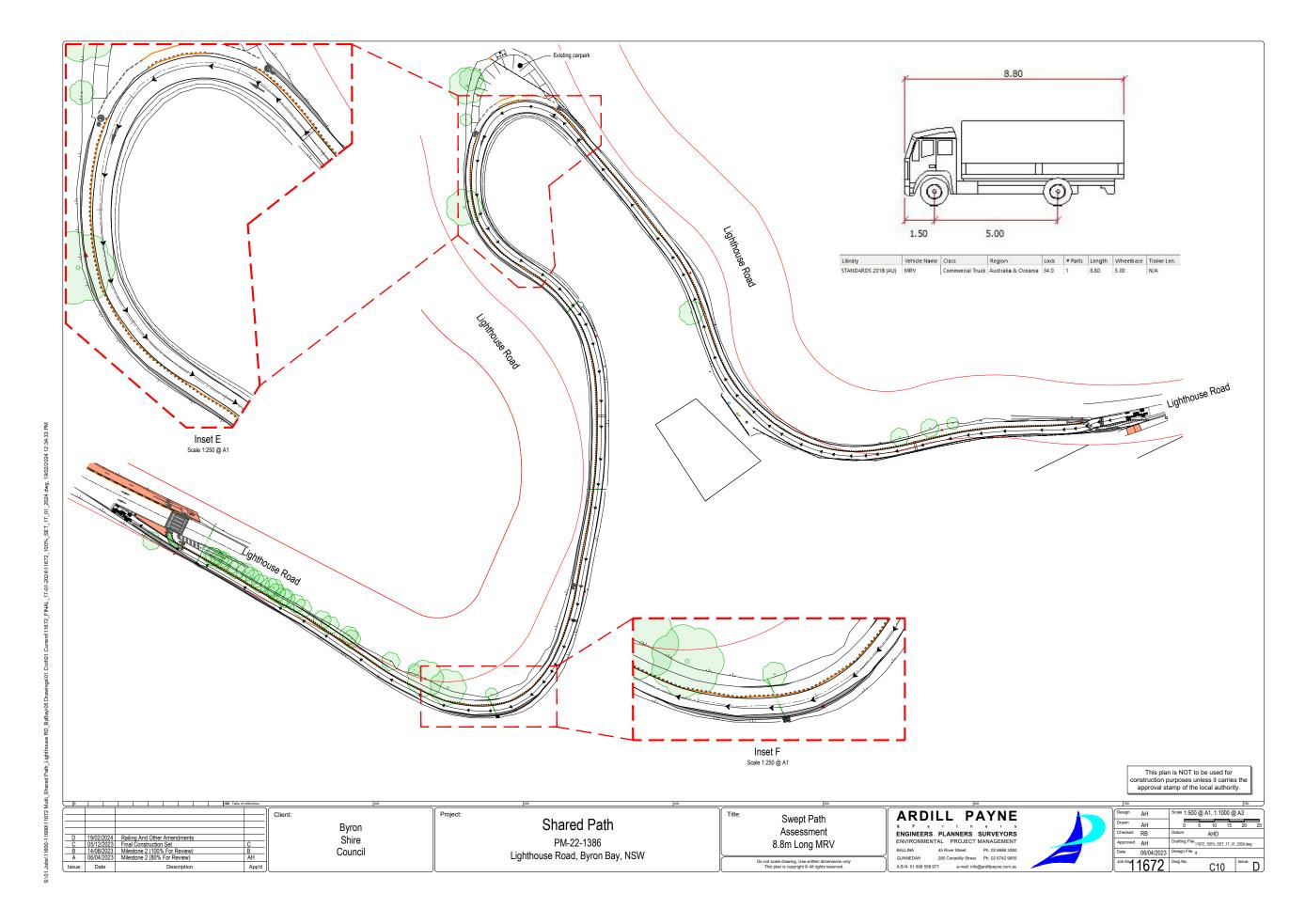


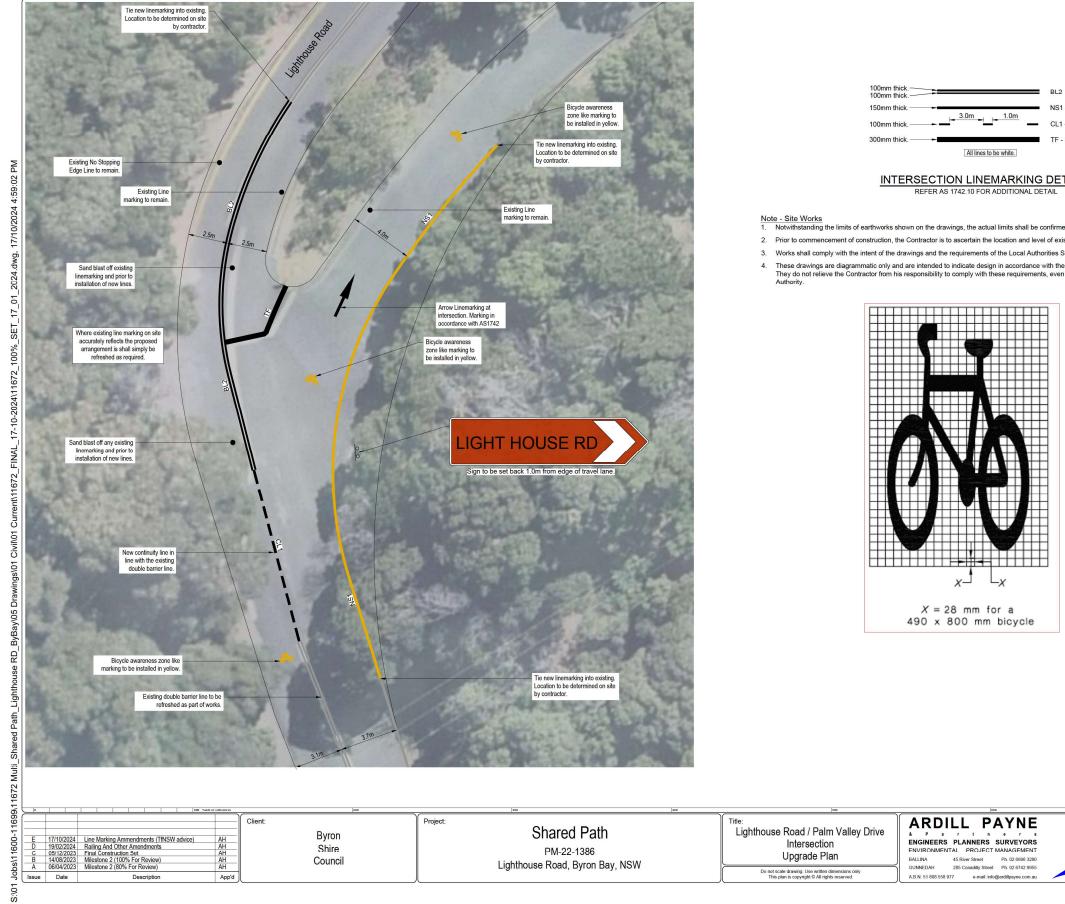
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C11 Issue E

LOCAL TRAFFIC COMMITTEE MEETING

## Report No. 6.5 Byron Street shared path – Bangalow - 'Get NSW Active' Grant Application

File No: 12024/1499

5 The purpose of this report is to gain Local Traffic Committee and Council support for the proposed changes to Byron Street, Bangalow.

Works will include the construction of a 2.5m wide concrete path with associated drainage, landscaping, crossings, signs, lines and retaining walls. The existing handrail on the pedestrian bridge over Byron Creek will be replaced with a bike safe full barrier fence, as per plan in attachment 1.

Staff is now working on the 'Get NSW Active' FY2025/26 Grant application to fund construction.

Staff has applied for the same Grant FY2024/25 which was unsuccessful. As a result of that, staff has worked to improve the application based on TfNSW feedback, attachment 2.

15 Review of Environmental Factors and a Risk Register Report have been completed for the proposed work.

## **RECOMMENDATION:**

20 That the Local Traffic Committee endorses the detail design of the Byron Street footpath extension project, as per drawings (E2024/129370) in attachment 1.

## Attachments:

1 IFC 3724-C200 Set - Byron Street shared Path - Bangalow, E2024/129370, page 78 🗓 🛣

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# Byron Street, Bangalow Shared Path - Stage 2

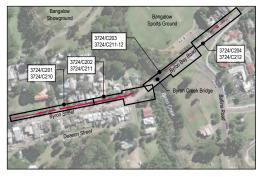
**Detailed Design** 

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	Drawing Register
3724/C200	Cover Sheet and Locality Plan
3724/C201	Existing Site Layout - Sheet 1 of 4
3724/C202	Existing Site Layout - Sheet 2 of 4
3724/C203	Existing Site Layout - Sheet 3 of 4
3724/C204	Existing Site Layout - Sheet 4 of 4
3724/C210	Design Layout Plan - Sheet 1 of 3
3724/C211	Design Layout Plan - Sheet 2 of 3
3724/C212	Design Layout Plan - Sheet 3 of 3
3724/C220	Shared Path Longitudinal Sections
3724/C221	Driveway Longitudinal Sections
3724/C230	Cross Sections: Station Ln. to Market Street
3724/C231	Cross Sections: Market Street to Bridge
3724/C240	Typical Details
3724/C241	Notes and Specifications
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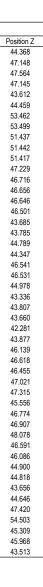


Key Plan - Byron Street and Byron Bay Road

Locality Plan - Bangalow

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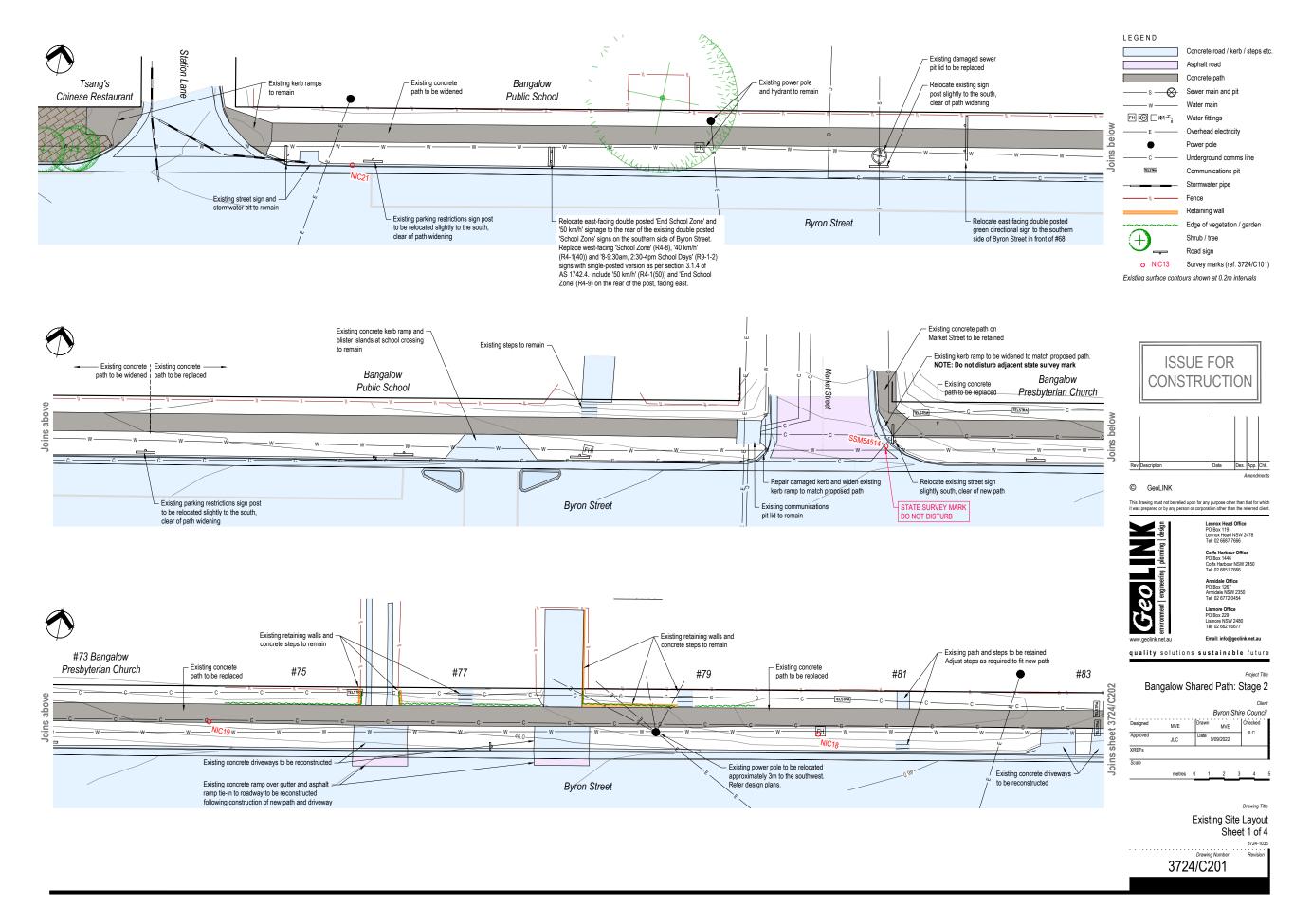
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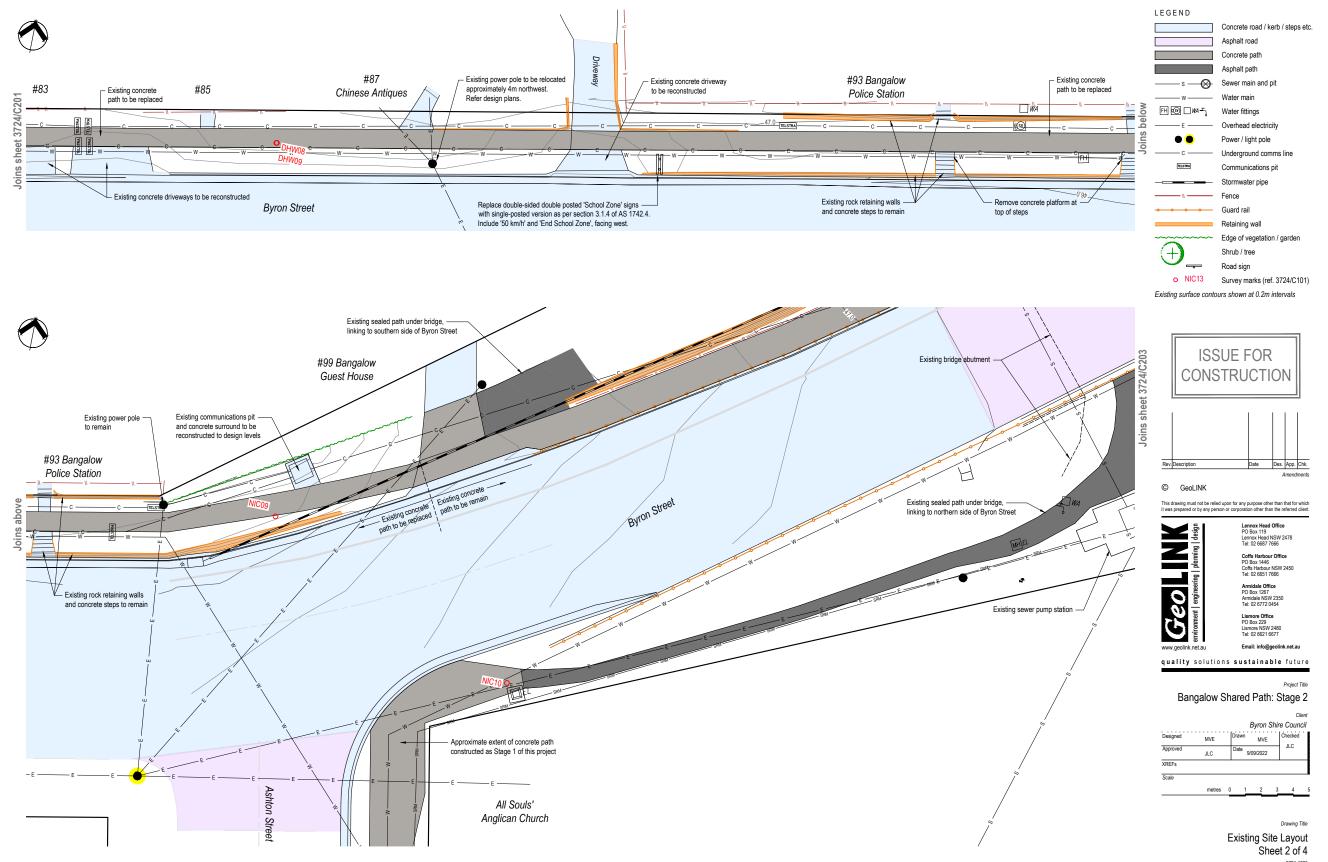
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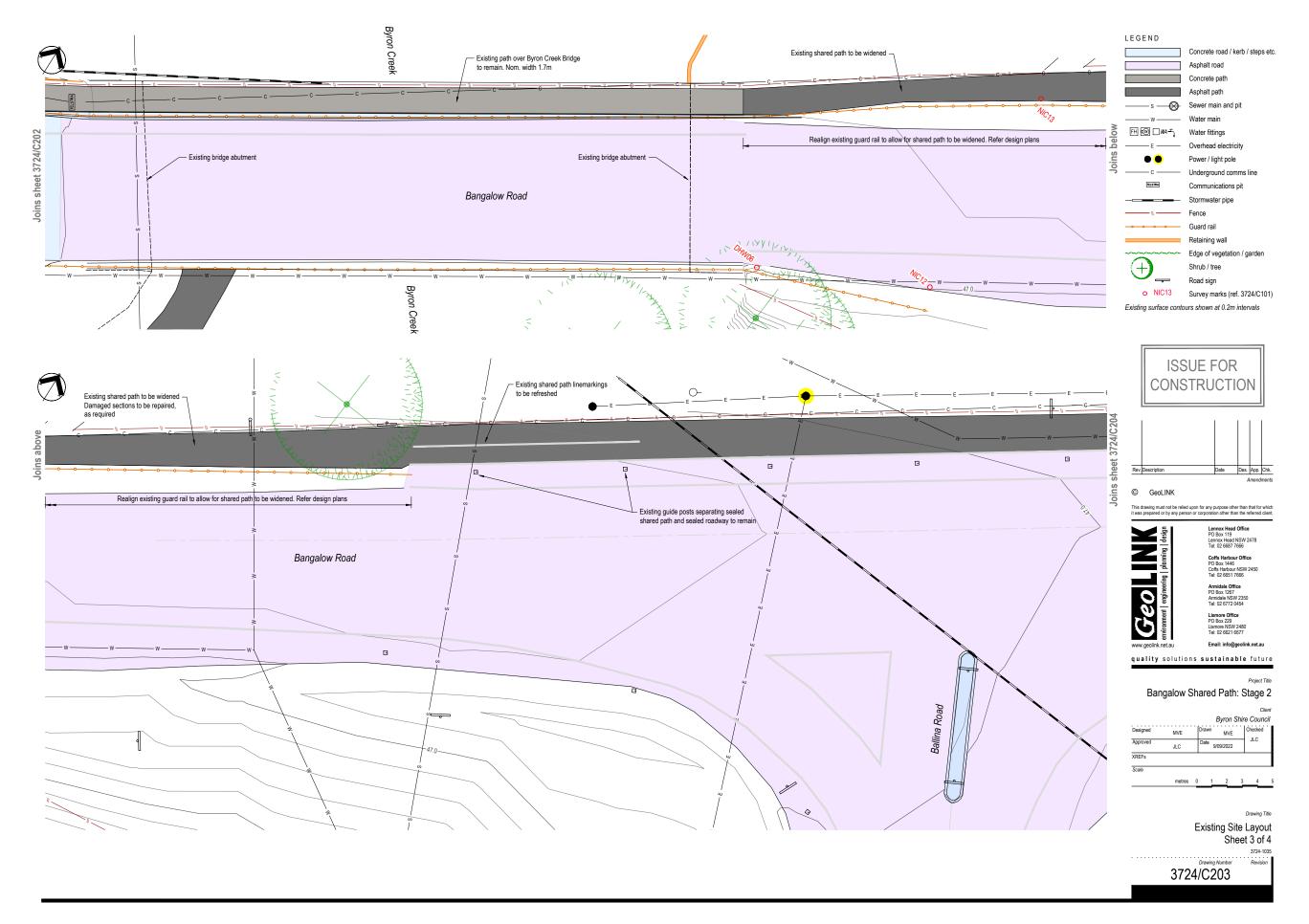


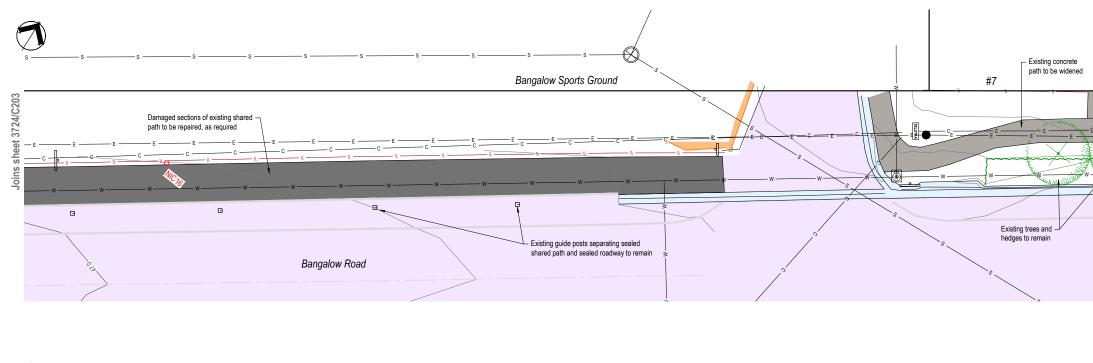


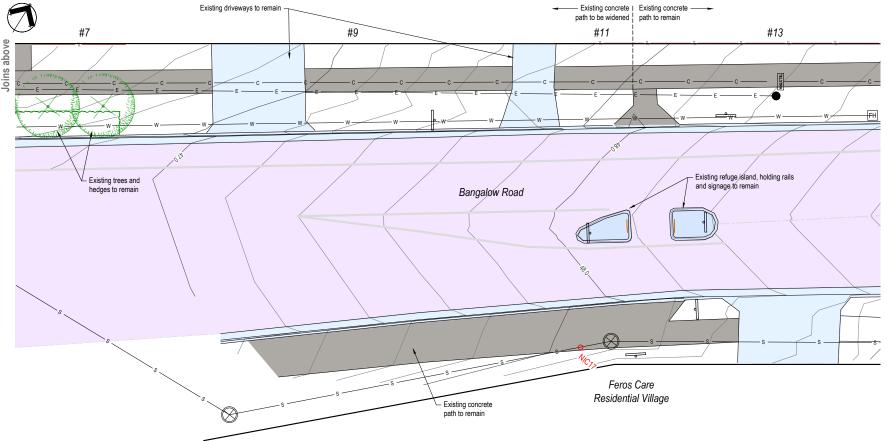
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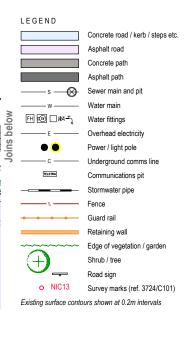
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#### REGULATORY MATTERS

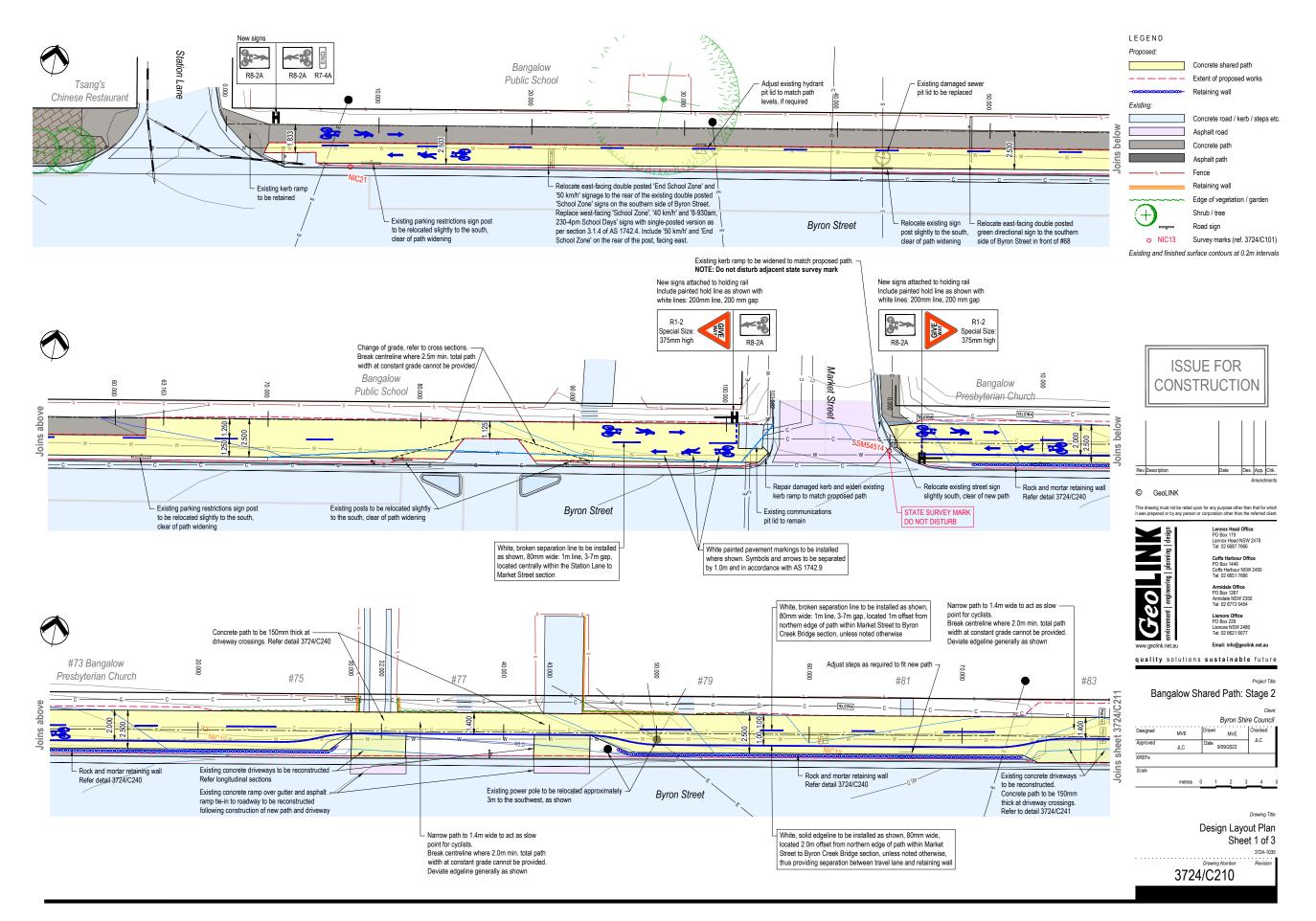




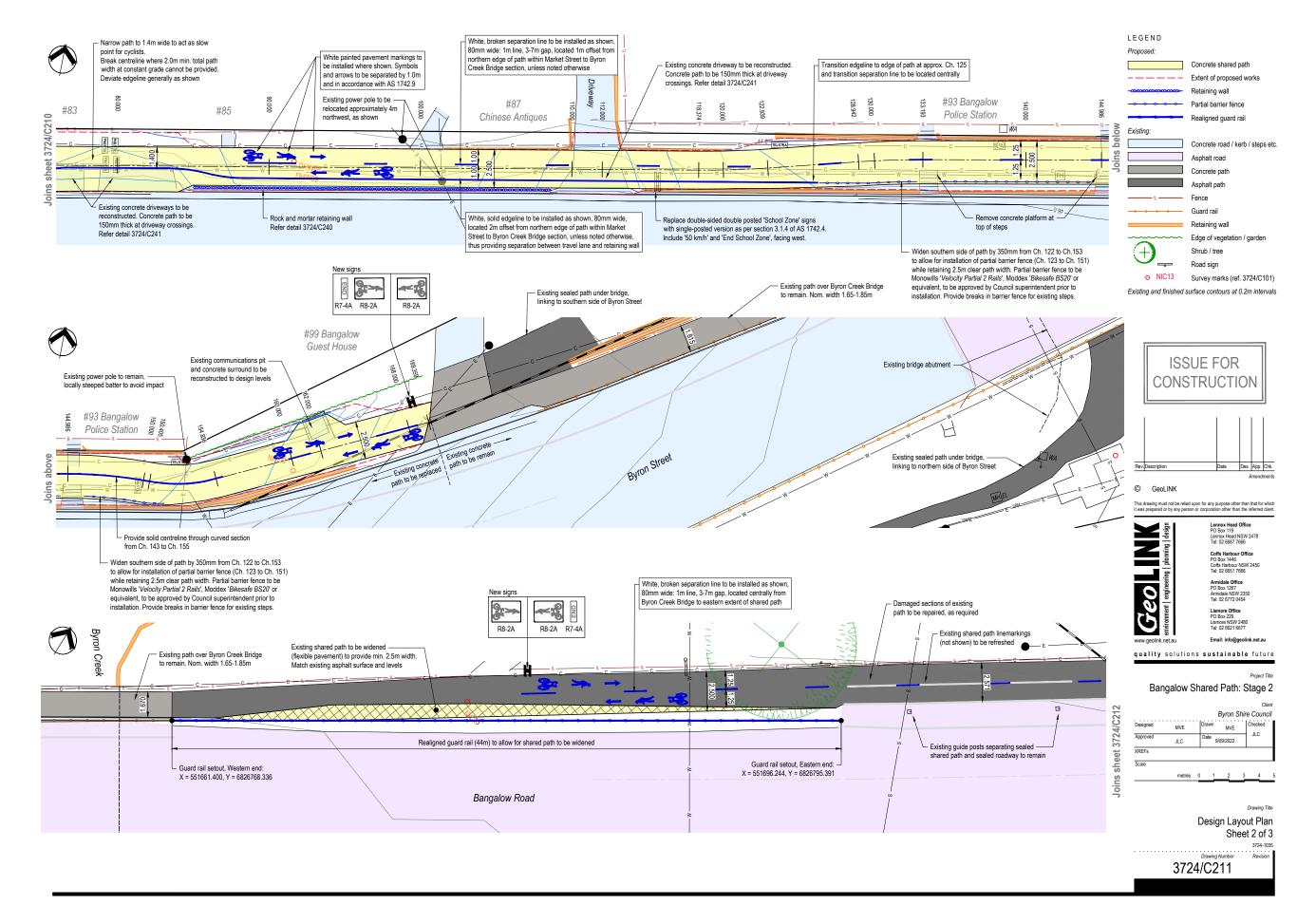


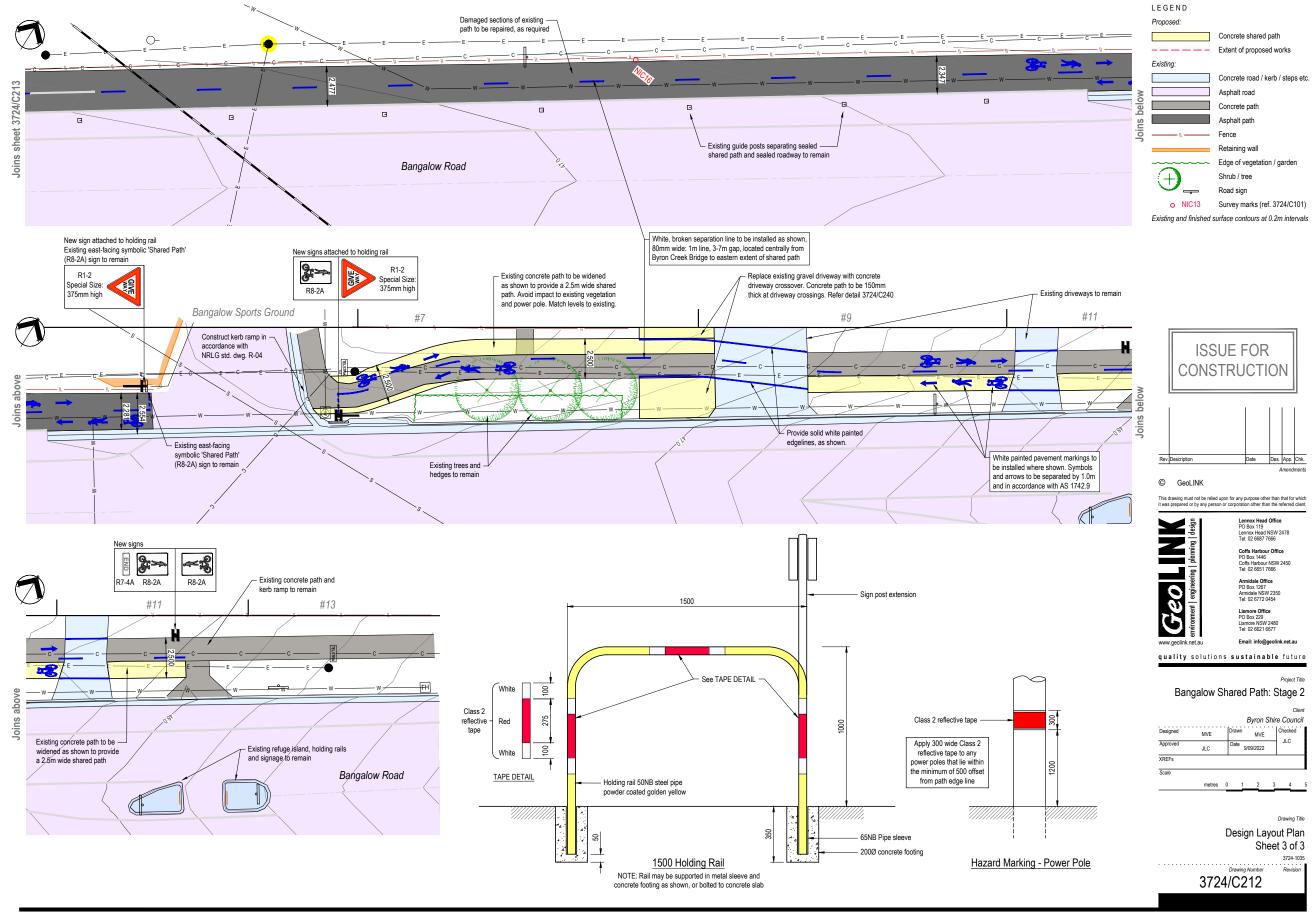


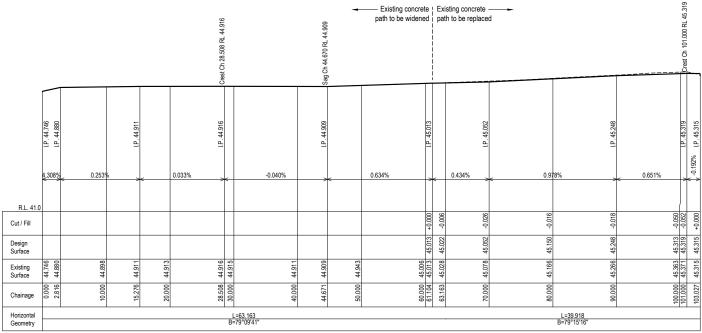


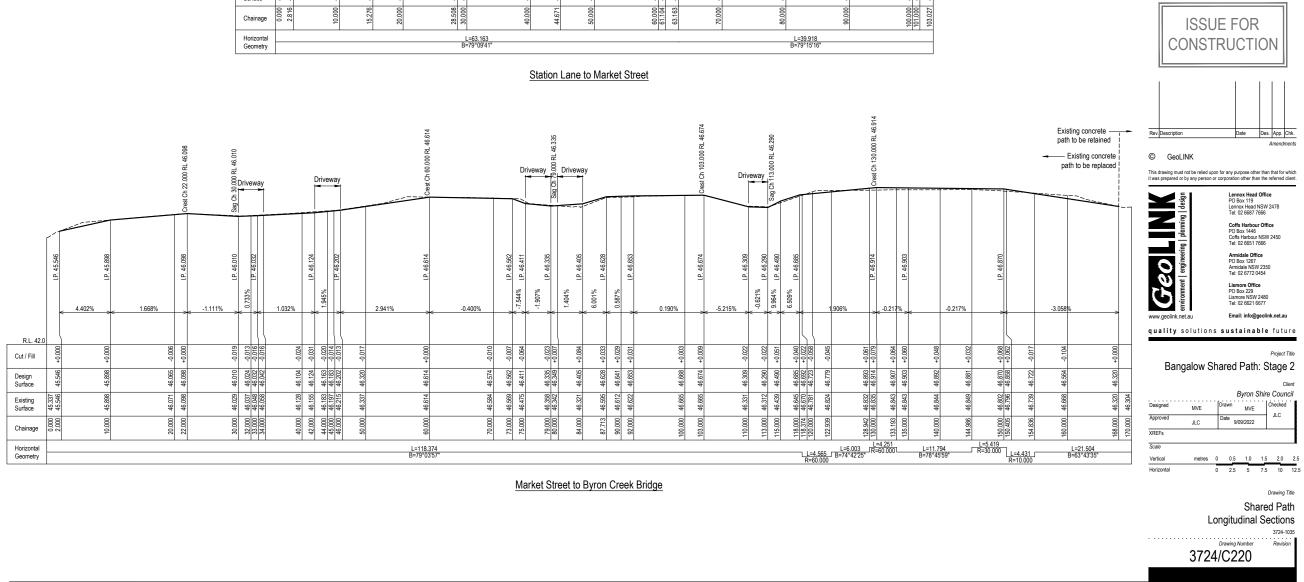


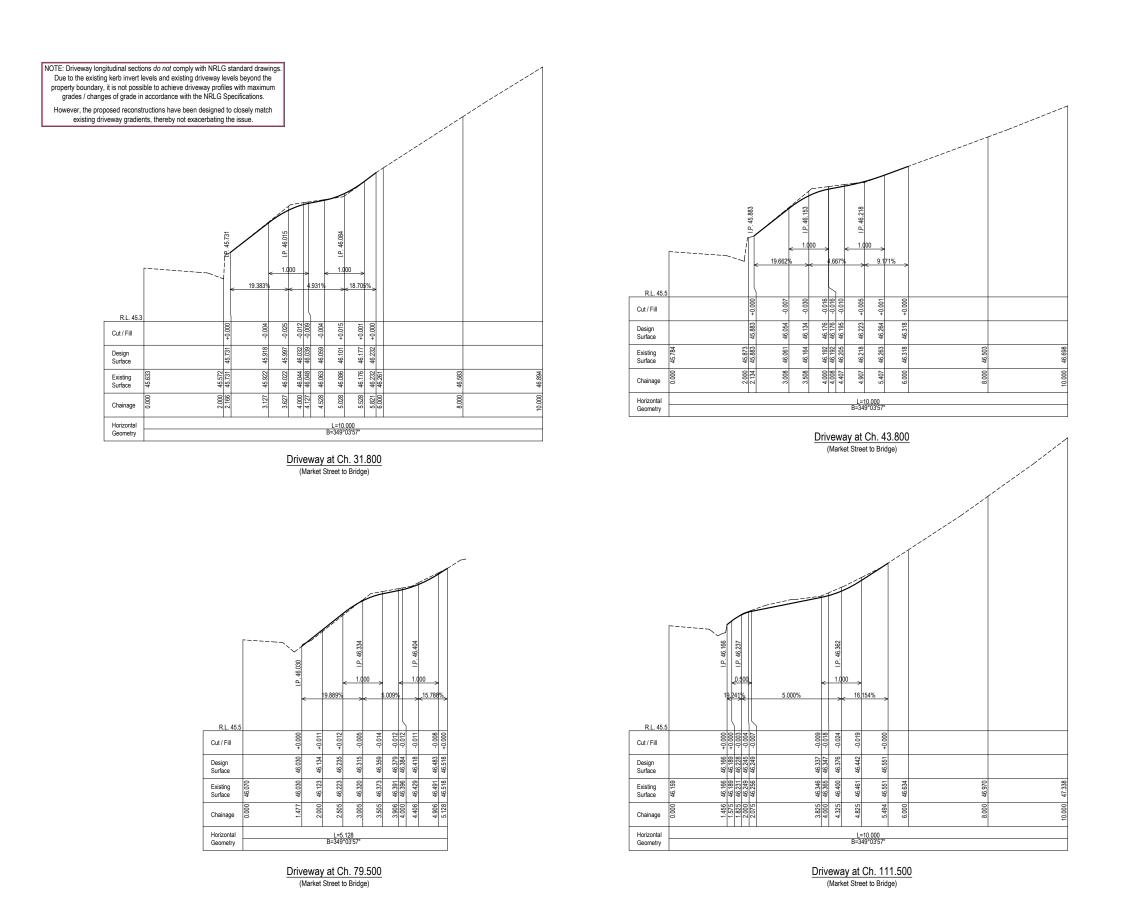
#### **REGULATORY MATTERS**





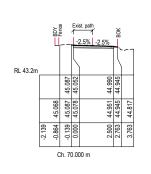


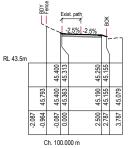


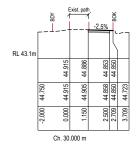


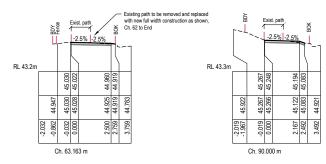
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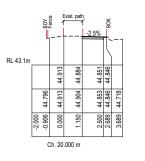
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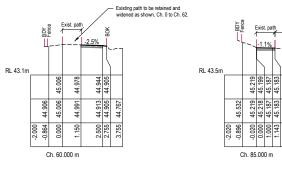


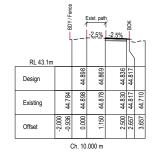


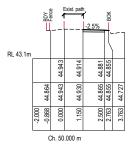


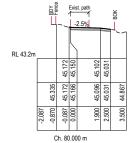






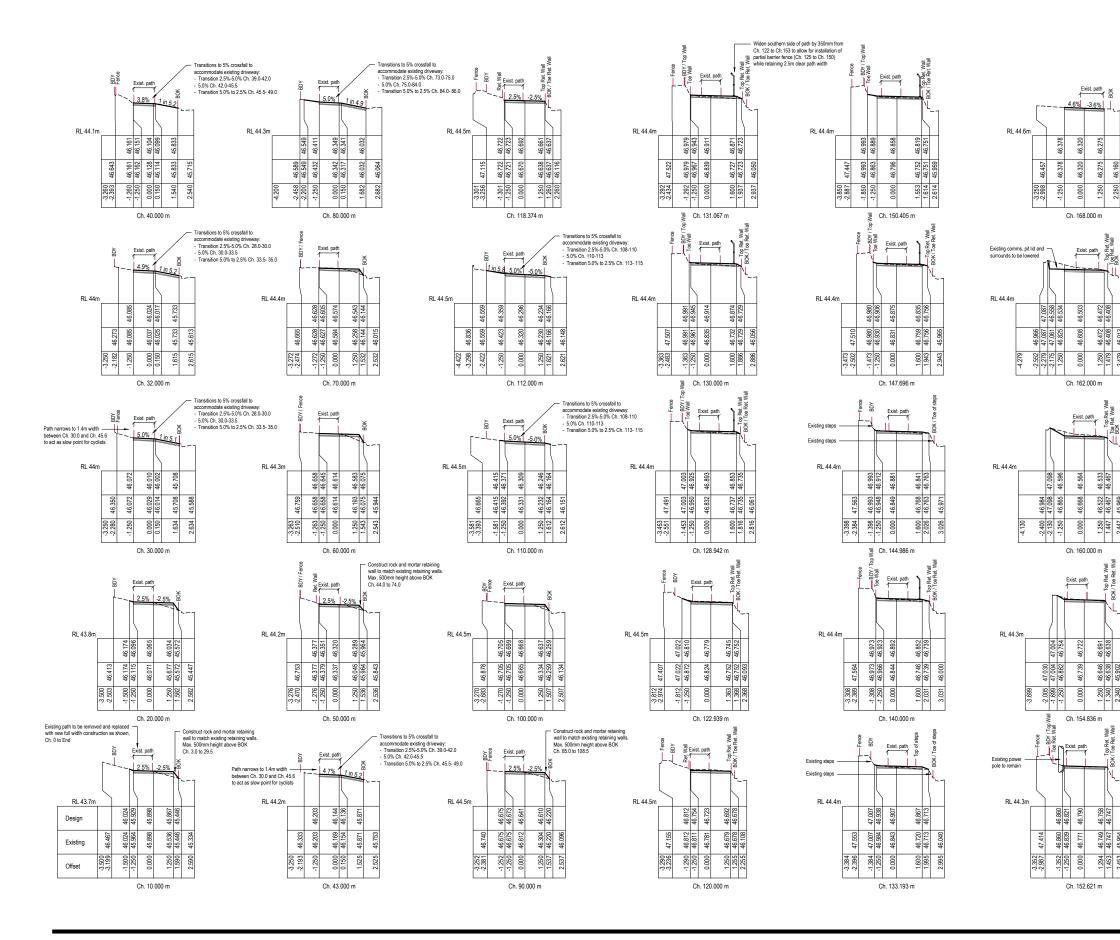




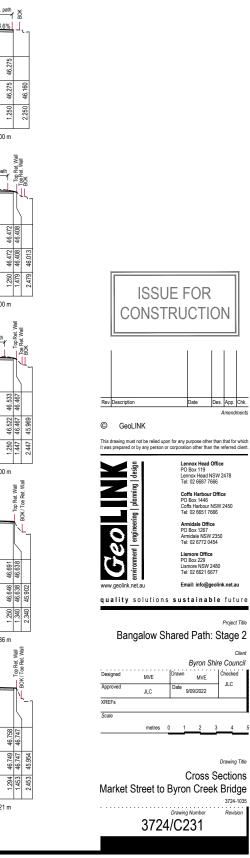


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#### 6.5 - ATTACHMENT 1



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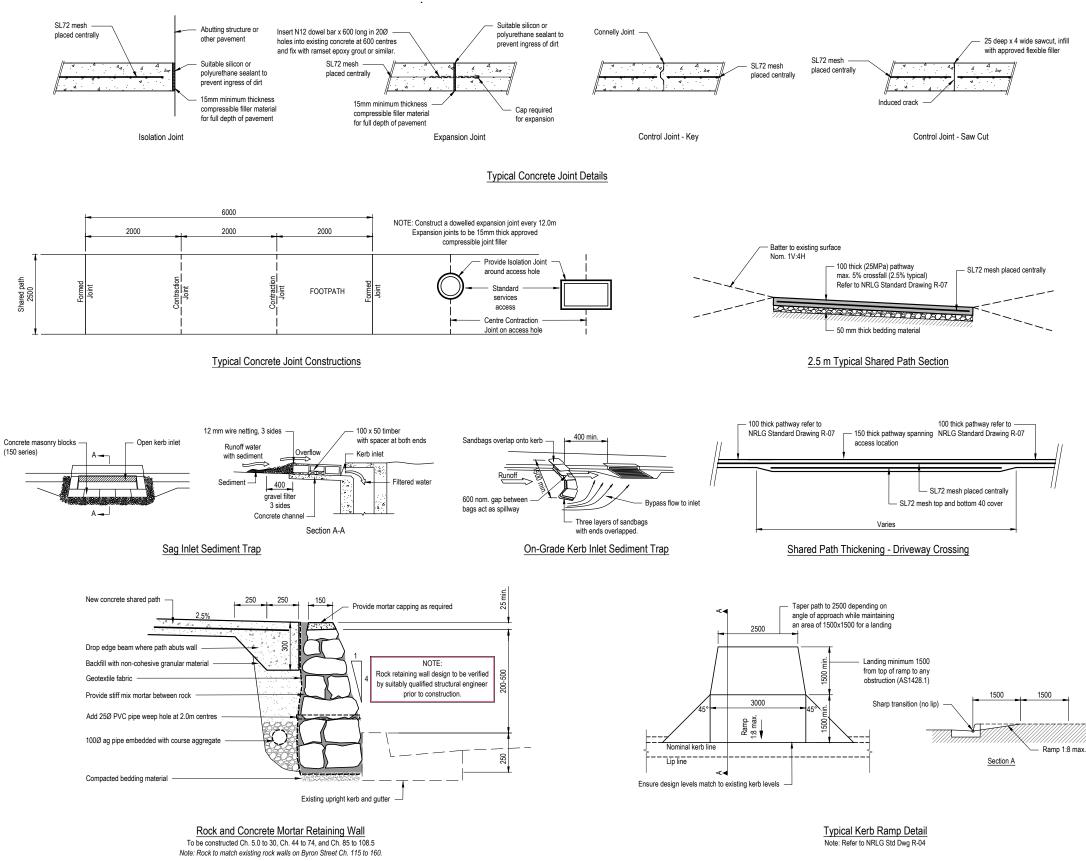
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#### **REGULATORY MATTERS**





These notes shall be read in conjunction with other specified documents. Where a discrepancy occurs between any documents such discrepancy shall be referred to the superintendent before proceeding with construction.

#### General

- 1. These drawings shall be read with full consideration of all the specifications, acts, regulations, standards, codes of practice and guidelines, both specifically referred to herein and implied and with other such written instructions as may be issued during the course of the contract. The contractor is responsible for ensuring they, and all site workers as necessary, have access to the relevant specifications, acts, regulations standards, codes of practice and guidelines, including the NRLG Specifications and are aware of the requirements and obligations set out therein. Any discrepancies shall be referred to the Superintendent before proceeding with the work.
- 2. All materials and workmanship shall be in accordance with the relevant and current Australian Standards, by-laws and ordinances of the relevant building authorities, and local ernment specifications
- 3. All dimensions shown shall be verified by the Contractor on site. The drawings shall not be scaled for dimensions. 4. Unless otherwise noted all levels are in metres, and dimensions
- shown in millimetres 5. The Contractor must ensure they are satisfied as to the site
- conditions and the requirements of the work. No claim whatsoever will be considered as a consequence of the failure of the Contractor to have made a full and comprehensiv investigation of the site and assessment of the conditions likely to affect the works. 6. The Contractor shall notify all relevant Local and Public
- Authorities of any works carried out that may affect those authorities. The Contractor must ensure they are fully aware of the requirements of all relevant authorities such as hold points, testing and analysis, approvals, and inspections of work in progress and meet the requirements of all relevant authorities. The contractor shall maintain a full set of the approved
- Drawings on site throughout the construction period, protect them from damage and make them vailable whenever required.
- 8. Existing underground facilities may not be shown on the drawings or may be in locations or levels different from those shown on the drawings. The Contractor shall be responsible for ascertaining the exact location of each underground facility prior to undertaking any work that may damage such facility.
- 9. When trade or propriety names, brands, catalogue or reference numbers are nominated, the Contractor shall use such equipment, services unless written approval is obtained from the Superintendent for the Contractor to supply a substitute material or equipment of identical characteristics of type, quality, appearance, finish, method of construction, and/or performance. 10. Explosives of any form are NOT permitted on or near this site
- 11. Plant and equipment stored on site shall be the sole responsibility of the Contractor. A compound for plant and equipment can be established on the property beyond the construction site area subject to the approval of the Superintendent having regard to environmental and operational issues. Upon completion of the works the Contractor shall restore the site compound to a condition acceptable to the Superintendent
- 12. Unless otherwise agreed or stated in the Specification or Contract (where applicable), the Contractor shall give one clear working day's notice to the Superintendent, in writing, of the construction operations which require examination, inspection or testing under the Contract. 13. Unless otherwise stated, all materials, plant, equipment, fixtures
- and other items salvaged from the Site of the works shall be the property of the Principal and shall not be removed from the Site without the prior approval of the Superintendent.
- 14. Other contractors may be working on the site during the works period. It shall be the contractor's responsibility to liaise with all contractors and ensure that any interference between adjacent works are kept to a minimum, and that all contractors are informed when works (that may effect other contracts) are proposed to be implemented

#### Glossary of Terms

Agenda

- 1. AS Australian Standard 2. Contractor - is defined to include or be synonymous with 'builder' or 'constructor'
- NRLG Northern Rivers Local Government Construction Manual 4. Superintendent - is defined to include or be synonymous with
- 'engineer', 'supervisor', or 'nominated overse

#### Safety Requirements

- The Contractor shall ensure that all the relevant requirements of the Work Health and Safety Act - 2011 (WHSA) and Work Health and Safety Regulation - 2011 (WHSR), and those codes of practice, statutory requirements and Australian Standards pertaining to workplace safety, are implemented and maintained throughout the works site and for the entire duration of the works.
- The Contractor shall appoint a Representative or Committee as requested in accordance with the WHSA and WHSR, and provide the name(s), address(es) and contact phone number(s) of the Representative or Committee to the Superintendent in

#### Local Amenity

- Construction Times 1. Construction work must not unreasonably interfere with the amenity of the neighbourhood. Hours of work involving
- construction noise audible on adjoining premises shall be estricted to:
- (i) Monday to Friday, from 7 am to 6 pm (ii) Saturday, from 8 am to 1 pm; and

writing upon their appointment.

- (iii) No construction work to take place on Sundays or Public
- Construction Noise
- Construction noise is to be limited as follows: a) For construction periods of four (4) weeks and under, the I 10 noise level measured over a period of not less than fifteen (15) minutes when the construction site is in operation must not exceed the background level by more than 20 dB(A).
- b) For construction periods greater than four (4) weeks and not exceeding twenty-six (26) weeks, the L10 noise level measured over a period of not less than fifteen (15) minutes when the construction site is in operation must not exceed the background level by more than 10 dB(A).

#### Survey Setout

- 1. The Contractor shall at their own expense set out the Works correctly in accordance with the Contract and the drawings and shall provide all instruments and materials necessary for that 2. If at any time during the progress of the work, any error is
- discovered in the position, level, dimensions or alignment of any part thereof, the Contractor shall immediately notify the Superintendent and shall, unless the Superintendent otherwise directs, rectify the error in accordance with the General Conditions of Contract
- The Contractor shall preserve and maintain in their true position all State Survey Marks (SSM) and Permanent Marks (PM) whether or not the marks are to be used for the purpose of setting out, checking or measuring the work under the Contract. Should any SSM or PM be disturbed or obliterated, the
- Contractor shall immediately notify the Superintendent and shall arrange for a Registered Surveyor to rectify such disturbance or obliteration. Unless the disturbance or obliteration has been caused by the principle, his employees or agents, the cost of rectification shall be borne by the Contractor. Such notice shall include the requirements of the Local Council for undertaking inspection of the work in progress. The Contractor shall be fully responsible for making himself aware of Council's requirements in this regard and for making all necessary provisions for such Council inspections.

#### Traffic Management

19 November 2024

- Where works occur in public areas, road reserves or private developments it shall be the Contractor's responsibility to ensure the safety of pedestrian and vehicular traffic within the works area.
- The Contractor shall ensure that the approved Traffic Management Plans prepared by an accredited traffic consultant, is strictly adhered to during the construction period

- Environmental Management
- 1 All work shall be undertaken so as to avoid damage to the environment, including erosion, contamination and sedimentation of the site, surrounding country, watercourse
- and streams. 2. The Contractor shall comply with the requirements of environmental legislation enacted by Federal, State and Local
- governments Necessary vegetation clearing and grubbing shall be in accordance relevant specifications, requirements of authorities
- and the direction of the Superintendent. Toxic Substances and Contaminants
- 4. Herbicides and other toxic chemicals shall NOT be used without prior written permission of the Superintendent.
- The Contractor shall give written notice to the Superintendent 14 days prior to any toxic substances entering the Site, comply with statutory requirements and take proper precautions to keep poison and other injurious substances in places secured against access by unauthorised persons, children and animals.
- 6. The Contractor shall dispose of all solid, liquid and gaseous contaminants in accordance with all statutory requirements and to locations off Site.
- The Contractor shall service and maintain vehicles and machinery at locations approved by the Superintendent. Sediment and Erosion Control
- 8. All erosion and sediment controls are to be in accordance with the Soils and Construction Volume 1 - Managing Urban Stormwater 4th Ed. (the 'Blue Book').
- 9. All sub contractors are to be informed of their responsibilities in relation to erosion and sedimentation
- 10. Accidentally disturbed areas would be stabilised and/or re-vegetated or secured with geofabric as soon as practical after the damage has occurred.
- Works to cease if pollution problems are suspected or detected.
   Safety issues must be considered at all times; incorporate
- traffic control devices to the satisfaction of the superintendent 13. The weather is to be monitored during the works. Works shall be scheduled outside of forecasted significant rain events and postponed during unforeseen rain events.
- 14. Pavement formations and any excavation works are to be scheduled outside of average high rainfall periods.
- Works shall cease and all sediment control measures checked and repaired/re-installed (if required) if heavy rainfall forecast. 16. Sediment control features would be checked as soon as practical (within 24 hours) after significant rainfall events.

#### Earthworks

- 1. Earthworks includes the following works: Removal of topsoil
- All activities and quality requirements associated with site regrading, excavation, haulage of material and filling Removal and replacement of any unsuitable material from
- below the work defined for general earthworks; and Any associated spoil or horrow activities The Contractor shall undertake all inspection and testing
- necessary to demonstrate that the quality requirements of this specification have been achieved. All earthworks are to be carried out in accordance with NRLG Specification C213. Nature of Earthworks Materials
- The Contractor shall be responsible for any assumptions made in relation to the nature and types of materials encountered in excavation and the bulking and compaction characteristics of the materials incorporated in filling operations.
- Protection of Farthworks 4. The Contractor's responsibility for care of the Works shall
- include the protection of earthworks. 5. Prior to commencement of earthworks operations, the Contractor shall install and maintain effective erosion and
- sedimentation control measures to the approval of the superintendent 6. Adequate drainage of all working areas shall be maintained throughout the period of construction to ensure run-off of water
- without ponding, except where ponding forms part of a planned erosion and sediment control system When rain is likely or when work is not proposed to continue in a working area on the following day, precautions shall be taken to minimise ingress of any excess water into earthworks
- material. Ripped material remaining in cuts and material placed as fill shall be sealed off with a smooth roller. Should insitu or stockpiled material become over-wet as a result of the Contractor not providing adequate protection of earthworks, the Contractor shall be responsible for replacing

and/or drving out the material and for any consequent delays

#### Path Construction

- Construct shared path and ancillary works to details shown. 2 All materials and workmanship shall be in accordance with the current Australian Standards, NRLG Guidelines and Standard Drawings and
- Austroads Specifications and Guidelines 3. Excavate natural surface where required, trim and compact subgrade
- and construct concrete footpath. Earthworks:
- a) Fill material shall be clean, well graded (ALD >50mm) with limited plasticity (PI<20), and free of organic matter.
- b) Fill shall be placed in lavers not exceeding 200mm loose thickness and compacted to 95% standard.
- c) Side batter slopes shall not be greater than 4H : 1V uno. d) The top 100mm of any fill area outside of the footpath shall be
- topsoiled and turfed. 5. All concrete footpaths to be constructed in accordance with NRLG std.
- dwg. R-07 (current version), unless noted otherwise. Concrete footpath to be 25MPa at 28 days.
- Concrete footpath to have lightly broomed finished perpendicular to
- pedestrian traffic. Crossfall of footpaths is to be consistent along the path as much as practicable. A maximum crossfall of 2.5% is not to be exceeded except
- where specifically indicated. 9. Footpath to be provided with contraction joints as per detail to induce controlled cracking. The joint shall be formed in the freshly placed
- concrete perpendicularly across the footpath at intervals shown. 10. Match new path to existing concrete driveways and levels unless otherwise stated. Dowel to existing as per detail provided and NRLG
- Standard Drawing R-07. 11. Trim all trees with overhanging limbs 2.4m above footpath to provide adequate headroom clearance and clear edge of footpath by min. 0.5m. 12. Construct path to height of adjacent pits and services where levels
- permit. Adjust Telstra Pits, Water Valve Covers and Boxes, Sewer Manholes, and Stormwater Pit Covers to match new path level as required. All services to be provided with isolation joints as per detail 13. All signage and pavement markings are to be in accordance with
- AS 1742.9. All permanent signs are to be supplied by Byron Shire Council and installed by contractor. 14. Once completed, the path is to be fully inspected to ensure no trip
- hazards are present, and all edges have been backfilled
- All distributed areas are to be reinstated to their original line and matching in to new construction
- b) The restored surface shall be free of all stones (exceeding 20mm) c) All distributed surfaces or areas of fill are to be sown with an even distribution of seed and fertilizer at the rates specified in NRLG Annexure C273A

#### Holding Rails and Signage

- Holding rails to be placed on the left hand side of the path, 600 from the kerb line and 300 from the path
- Installation of signs and line marking to comply with AS1742
- Signs which are to mounted adjacent to bicycle paths or footpaths separate from vehicular roadways should be located as follows: Lateral Placement - Signs should be at least 600mm clear of the
- outer edge of the facility Mounting height - Signs should be mounted a minimum of 2m
- above the ground surface Overhead signs - Signs may be placed above bicycle paths. provided the minimum vertical clearance is 2.5m and the supporting structure conforms with the Lateral Placement (above)

#### Concrete

1. All workmanship and materials shall be in accordance with

No admixtures shall be used in concrete unless approved in writing

surfaces in contact with sewerage or groundwater. . . 65

Construction joints where not shown shall be located to the approval of

the Superintendent. All concrete surfaces to be cured by an approved method for 7 days.

10. Except as otherwise required, exposed concrete corners and edges

SL denotes grade 500 SL hard drawn wire reinforcing fabric to AS 1304.

Clear concrete cover to all reinforcement shall be as follows unless

All concrete shall be compacted using mechanical vibrators.

- AS 3600 and AS 3610 current editions.
- 2. Concrete guality:
  - strength grade 25 MPa

shown otherwise (mm)

elsewhere

shall have 20 mm chamfers

nominal aggregate size 20mm slump 80mm 3. All cement shall be sulphate resistant

slabs, top cover to mesh.

#### 6.5 - ATTACHMENT 1

1. The Contractor shall arrange for an approved testing authority to undertake the testing of path subgrades, pavements and concrete slabs, and shall comply with the testing methods and

Acceptance Testing of Works

frequencies specified in NRLG Specification CQC.

General

Testing

Testing Authority

which conducts the tests.

original sent to the Contractor.

commencement of work under the Contract.

within seven days of the start of the comm

representative of the material or work.

accepted by the Superintenden

Completion of Works

the Defects Liability Period.

Superintendent

excavations;

or through the Site: and

results for inspection by the Superintenden

that existing before commencement of works.

as a result of the work is removed from roads.

be carried out at no cost to the Principal.

Maintenance / Defects Liability Period

the whole of the defects liability period. Maintenance shall include, but not be limited to

as the work proceeds.

the job datum.

Test Results

 All acceptance testing shall be carried out by a testing authority certified by the National Association of Testing Authorities (NATA) to conduct the specified tests.

3. Test results shall be endorsed by the NATA certified authority

4. The authority conducting the test shall submit a copy of the test result(s) and/or report direct to the Superintendent with the

5. The Contractor shall supply full details of the testing requirements in the Contract to the testing authority, prior to

6. The Contractor shall supply a copy of the Contractor's letter commissioning the testing authority to the Superintendent

7. The Contractor shall liaise with the testing authority and arrange

for all acceptance testing. 8. The Contractor shall, unless agreed or stated otherwise in the Specification give one clear working days written notification to the Superintendent before carrying out acceptance testing. 9. Tests and/or samples shall be representative of the work or material which is being tested for quality and/or acceptance. 10. The Contractor shall arrange for retesting the work when the Superintendent considers that the test and/or samples are not

11. The Contractor shall arrange to carry out testing progressi

12. The locations of all tests in plan and elevation shall be shown on test certificates and test reports and locate tests relative to

13. The Contractor shall submit copies of the test results and not proceed with the tested portion of the work until the results are

14. The Contractor shall keep on Site an up-to-date record of test

1. Prior to the issue of a Certificate of Practical Completion, all surplus material and rubbish shall be removed and the whole of the site left clean and neat in appearance. Similarly, the whole vork area shall be left in a neat and tidy condition at the end of

Any road pavement or footpath disturbed or destroyed during construction shall be reinstated to a condition at least equal to

The Contractor shall ensure that deleterious material deposited

4. Any damage to external areas of the site as a result of the works shall be reinstated by the Contractor as directed by the

Any clearing, cleaning or reinstatement by the Contractor shall

The Contractor shall maintain the work under the contract for

Reinstatement of any subsidence in fills, trenches and

Repair of any erosion or other damage caused to any item of the work by the flow or passage of water on, over

Reconstruction of any portion or item of the works which has deteriorated or no longer conforms to relevant specifications, the design, or the intent of the works.

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## LOCAL TRAFFIC COMMITTEE MEETING

#### FOR INFORMATION ONLY

	Report No. 7.1	Brunswick Heads Pay Parking - Notification of Completion
5	Directorate:	Infrastructure Services
	Report Author:	Shelley Currie, Road Safety Officer
	File No:	12024/1325

#### 10

## Purpose:

This report is in accordance with the Transport for NSW Traffic Management and Pedestrian Works Temporary Delegation to Councils No. 2 and the requirement to notify of works completed under this delegation.

15 Attached is a record of parking signage and line marking works completed in Brunswick Heads in relation to the introduction of paid parking.

## Communicate to:

- Transport for NSW
- NSW Police

## 20

## Attachments:

1 Memo to GM - Brunswick Heads Parking Signage approval, E2024/96072, page 93 🗓 🛣

25

Page 1 of 2

#### **Byron Shire Council - Memorandum**

МЕМО ТО:	General Manager
COPY TO:	Director Infrastructure Services
MEMO FROM:	Road Safety Officer
SUBJECT:	Approval to complete Brunswick Heads parking signage installation
DATE:	26 September 2024
RECORD NO:	E2024/96072

Dear Mark,

In accordance with the Transport for NSW Traffic Management and Pedestrian Works Temporary Delegation to Councils No.2, I am seeking your approval for the installation of new parking signage and line marking to support the implementation of pay parking in Brunswick Heads, as per Council resolution 24-131 shown below.

	Report No. 13.12 File No:	Brunswick Heads Pay Parking - Implementation I2024/373
24-131	Resolved that Cou	ncil:
	for the Brunsw (Resolution 23	inclusion in the draft 2024/25 Fees and Charges, a fee of \$5 per hour vick Heads Pay Parking scheme and reiterates its earlier commitment 3-652) to reducing the resident permit fee from \$55 to \$0 when the ads scheme becomes operational.
		nswick Heads Pay Parking scheme using a combination of existing and Pay by Plate cashless parking meters and other options such as
28	scheme b) advising	ner reports: ng how the funds generated from the Brunswick Heads Pay Parking could be distributed in Brunswick Heads and the Shire. of different software options to improve the usability and efficiency of hit application process
	community gro a) impacts b) impacts	rther report on discussions with relevant Brunswick Heads oups on: on of likely overflow from avoidance of parking charges on possible revenue from various options on permits and exemptions d parking areas (Hunter/Lyon)
		t to the vote and carried. I, and Coorey voted against the motion.

Page 2 of 2

The following documentation is attached for your review:

- 1. Bitzios Brunswick Heads Pay Parking Signage Install Plans
- 2. Byron Shire Council Proposed Linemarking and Signage, Brunswick Heads

Please note these drawings have been reviewed by Ben Taylor (Council's Traffic & Transport Engineer), Jason Brook and Aaron Stockwell (Bitzios Consulting).

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Shelley Currie Road Safety Officer

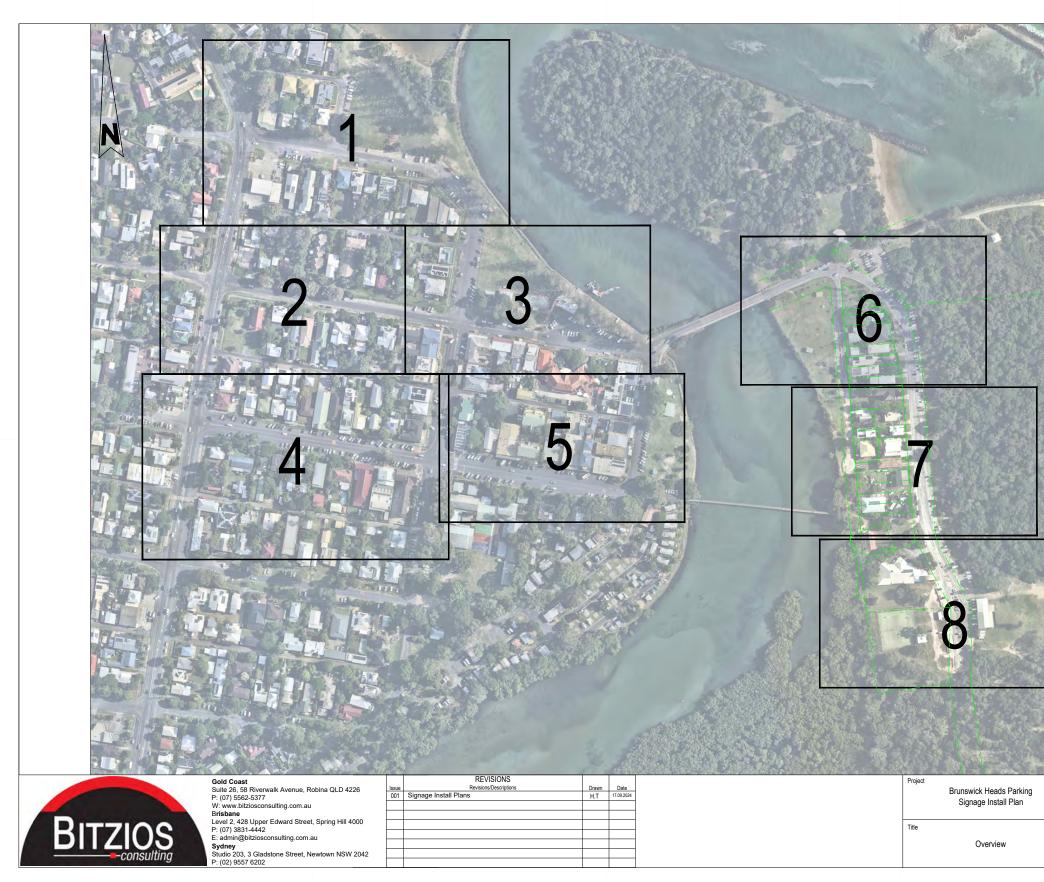
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A Director Infrastructure Services

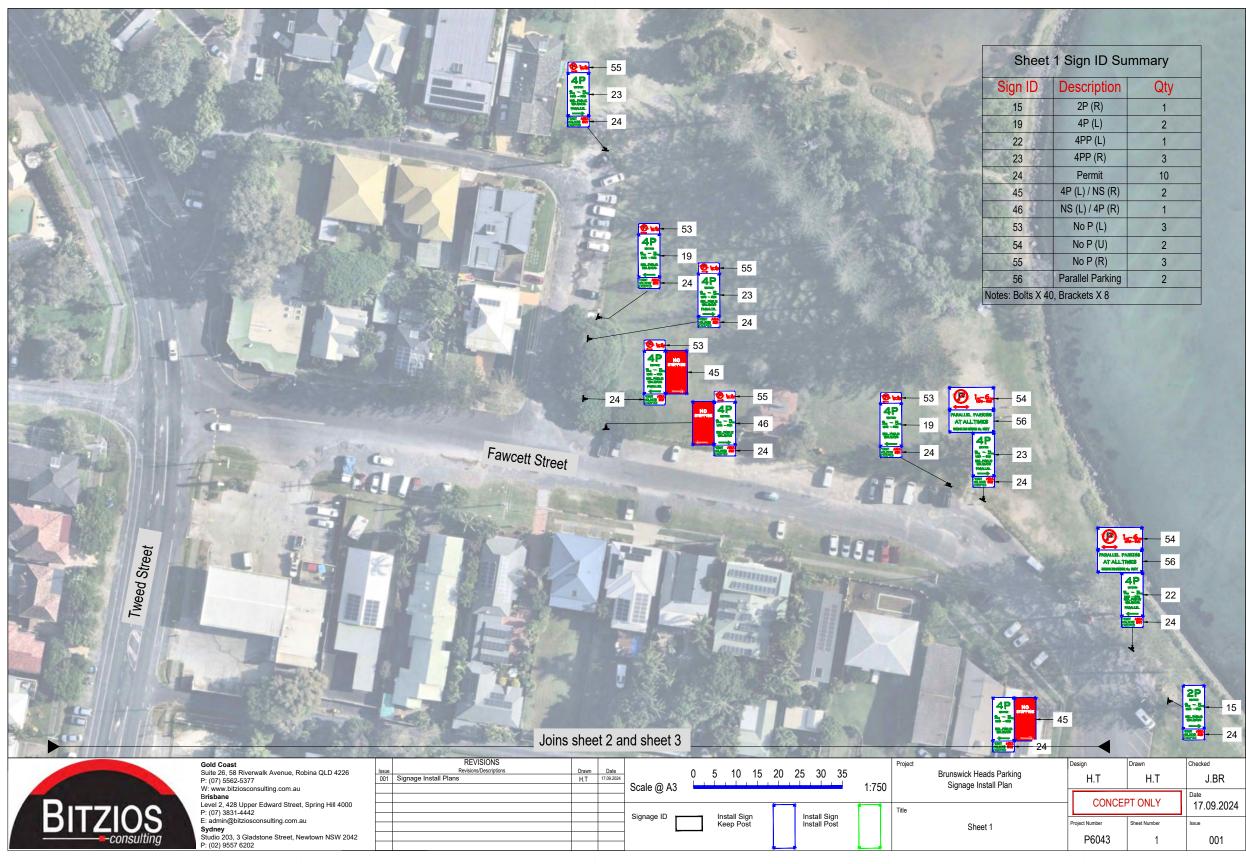
Approved / Not Approved

Mark Arnold General Manager

26/ 7/24

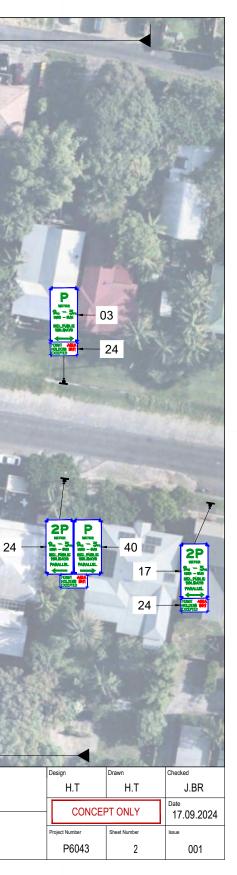


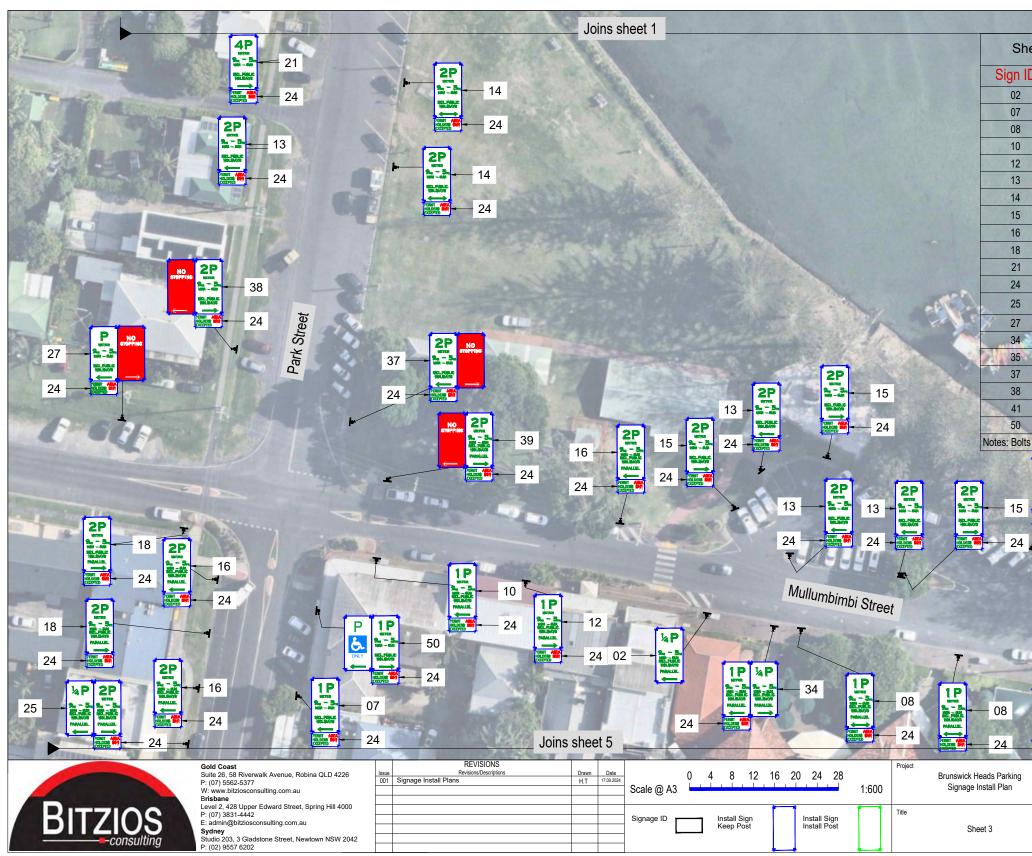




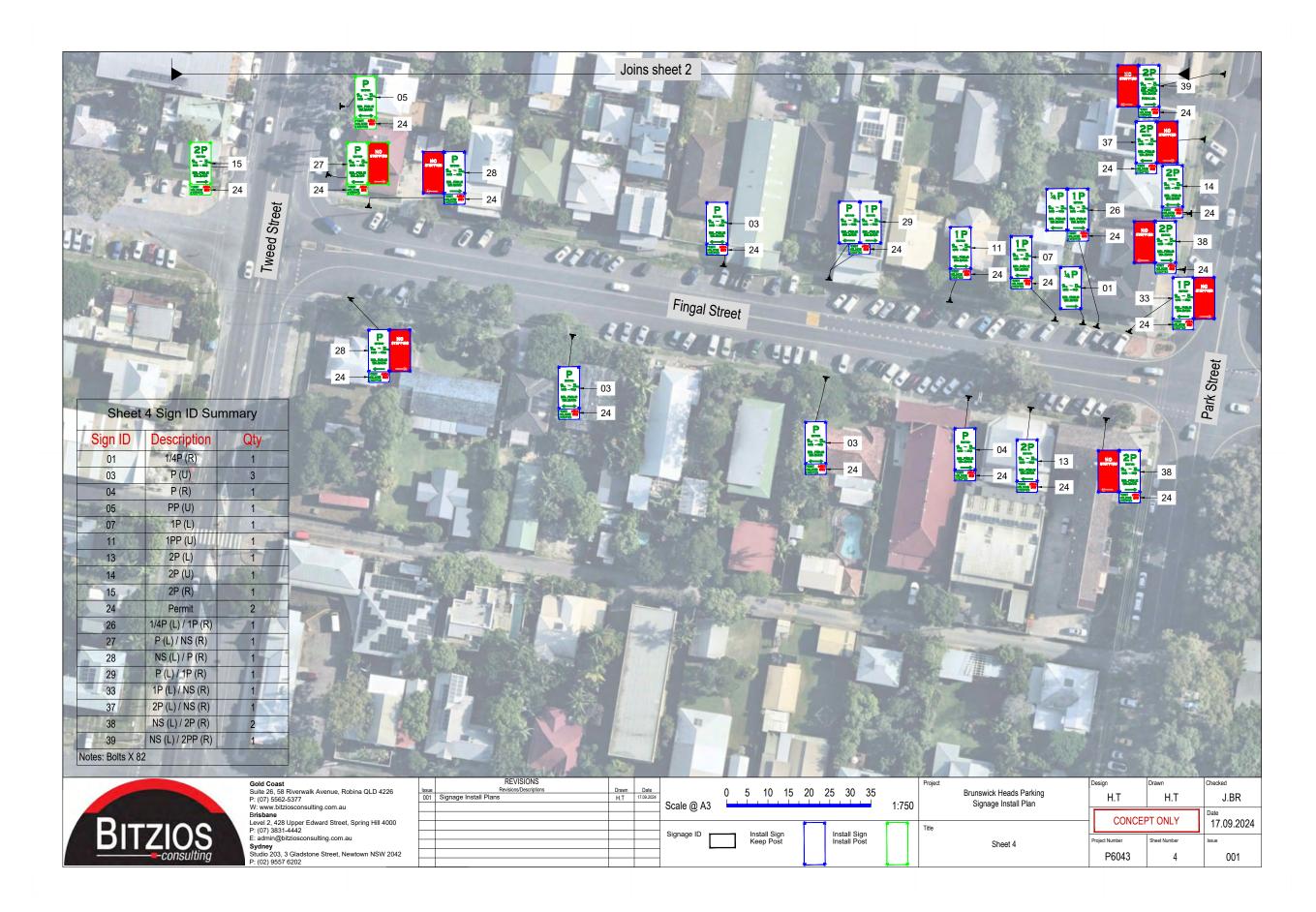
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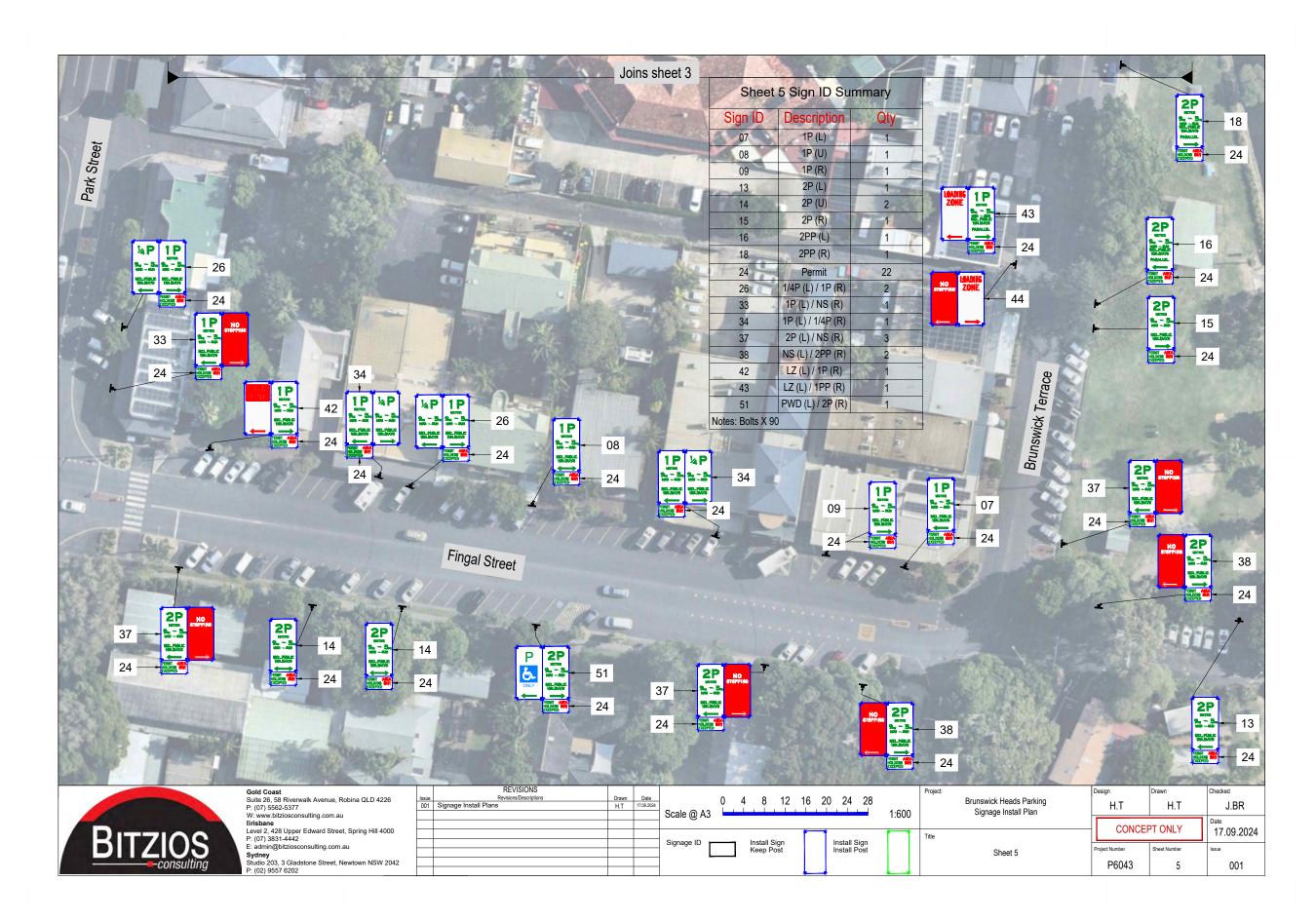
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## Proposed Line Marking and Signage, **Brunswick Heads**



Locality	sketch
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Index	
Description	
Index and Locality Sketch	
General Notes	
Sheet Layout Plan	
Signage Plan – Fawcett an	d Park Street
Line Marking Plan – Park S	Street
Line Marking and Signage P	lan – Balun Lane
Line Marking and Signage P	lan – Balun Lane
Signage Plan – Park Stree	t
Line Marking Plan - South	Beach Road
Quantities	
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Legend

Issue A, B, C, etc. = Preliminary approvals / tender drawings (NOT FOR CONSTRUCTION) Issue 1, 2, 3, etc. = Construction issue drawings

#### **ISSUED FOR CONSTRUCTION** DATE \_\_\_\_\_\_23/09/24

Project Pulse Number:	
Plan Register Number: <b>3091</b>	
Drawing number	Issue
3091-01	1

BYRON
SHIRE
COUNCIL

DWG No.

3091-01

3091-02

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Issue

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#### General

- 1. These drawings shall be read in conjunction with the relevant Northern Rivers Local Government development design and construction manuals and standard drawings.
- 2. This note and the following notes form an integral part of this drawing set.
- 3. All dimensions are in metres unless shown otherwise.
- 4 Dimensions shall not be scaled from the drawings
- 5. Materials and workmanship shall be in accordance with the specifications, together with the requirements of all applicable codes of practice, Australian standards and statutory authorities.
- 6. Survey data has been compiled from field pick-ups and office records. The project manager should ensure that sufficient data is shown to enable construction without disturbance to features that are not shown on the drawings
- 7. Services shown hereon have been located where visible on the site, from information received from relevant authorities and from historical records held by Byron shire council, and potholed and visually located where indicated.
- 8. Prior to any demolition, excavation or construction on site, the relevant authorities should be contacted for location of all existing and planned services (Before You Dig – www.byda.com.au) within the works area. Services that may be impacted must be potholed, visually located and protected from any damage.
- 9. The title boundaries shown hereon were not field investigated or marked at the time of survey and have been determined by plan dimensions only.
- 10. The origin of co-ordinates is MGA.
- 11. The datum for levels is AHD.

#### Site works

- 1. All soils containing organic matter (e.g. roots, grass etc.) must be stripped from the construction site prior to filling / building works and must not be used as fill material
- 2. All exposed surfaces shall be grassed or paved to prevent scour and erosion damage.
- 3. The constructor is responsible for implementing all necessary sedimentation and erosion control measures specified or deemed necessary to protect the works and adiacent areas.
- 4. The constructor is responsible for the maintenance and management of a temporary and / or permanent erosion and sedimentation controls during the construction and maintenance period.
- 5. All oversized material, which may impede compaction, must be removed from the fill platform.
- 6. Fill is to be uniformly compacted in up to 200-300mm horizontal layers and must achieve a minimum standard of compaction of greater than 95% standard compaction to AS 1289 for cohesive soils, or a density index of greater than 65% for cohesionless soils. Benching of the natural ground will be required on sloping ground prior to commencement of fill onerations
- 7. Clays of high plasticity or high in-situ moisture content are not to be used as fill.
- 8. An imported granular fill with a plasticity index preferably less than 15%, with no excessive oversized material, may be used.
- 9. Field density tests, or equivalent, should be carried out to verify that the standard of compaction is achieved. Field density tests are to be taken over the full depth of the layer or from the bottom of the layer.

Restoration of surfaces

- The constructor shall clean pavements, lawns and other improved areas and leave them in the same order as they were at the commencement of the works. The constructor shall restore any fencing removed during construction and shall restore lawns with turf cut and set aside from the original surface and with imported turf from a source approved by the construction engineer. (WSA 02 2002 Part 3, Section 25).
- Immediately after backfilling of a trench excavated through 2 a navement has been completed the constructor shall temporarily restore the pavement. Where the trench crosses bitumen or concrete pavement, the surface is to be protected from deterioration. A pre-mixed asphaltic material may be used for such temporary restoration. the constructor shall maintain the temporary restoration until final restoration is carried out. Final restoration of the pavement shall be carried out to restore the pavement and its sub-base to no less than the original condition. Final restoration may include, if required by the construction engineer, the removal of temporary restoration.
- In other than roadways, the constructor shall place the backfill sufficiently high to compensate for expected settlement and further backfilling shall be carried out or the original backfill trimmed at the end of the defects liability period in order that the surface of the completed trench may then conform with the adjacent surface. Surplus material shall be removed and disposed of to areas arranged by the constructor. Where dry weather conditions have persisted after the original backfilling, including during the defects liability period, the constructor shall take all necessary steps to consolidate the trench before removing surplus materials from the site.
- In locations where, in the opinion of the construction 1. engineer, surplus material left in the vicinity of the trench would not be objectionable, the surplus material may be disposed by spreading neatly in the vicinity of the trench to the satisfaction of the construction engineer in such a way as to avoid future erosion of the backfill and adjacent ground surfaces. The constructor shall maintain the backfill and adjacent ground until the expiry of the defects liability period.
- 5 Where, within public or private property, the reasonable convenience of persons will require such the construction engineer may order the constructor to level trenches at the time of backfilling. The constructor shall make good any subsequent settlement, as required by placing additional fill. The constructor shall immediately restore any damaged or
- disturbed private property and services. 7
- Should the constructor elect to tunnel under paving, kerb and gutter or other improved surfaces in lieu of trenching, hackfilling shall be so carried out as to restore full support to those surfaces. The constructor shall remain responsible for the repair of the improved surfaces, if subsequently damaged due to subsidence of the backfill, until the end of the defects liability period.
- 8. The constructor shall provide notice to affected property owners of any pending works.

#### Driveways

- All existing driveways affected by new works are to be cut back, removed & reconstructed using material to match existing
- The constructor shall liaise with the property owners 2. regarding any variation to the above.
- З Reconstruction of existing concrete driveway or pathway is to be in accordance with Northern Rivers Local Government D1.37 AND D1.38 "Handbook for driveway access to property" and relevant standard drawings
- 4 Reconstruction of existing bitumen sealed driveway shall be of similar construction to that of the existing with a compacted gravel base course

#### Existing services

- 1. The constructor shall be responsible for the location of existing services prior to commencing with the works. 2. The constructor shall be responsible for the replacement of
- any existing services damaged during construction with new services of equivalent type and specifications.
- 3. The constructor shall be responsible for liasing with telecommunications and electrical supply authorities with supply and fitment of replacement telecommunications and electricity pits and/or lids to suit his works program
- 4. When constructing or working near existing pressure mains it should be expected that there are concrete thrust blocks located at bends or other fittings on the existing main. It is very important not to disturb the bearing soil behind the thrust block to avoid failure of the existing pressure main. If excavation around existing thrust blocks can not be avoided then the existing pressure main shall be taken off line during the excavation works.

#### Concrete

All workmanship and materials shall be in accordance with

- A \$ 3600 current edition with amendments 1. Concrete quality (unless otherwise shown) shall be as follows
- course aggregate maximum size 20mm
- cement type "A" Portland cement.
- concrete shall have the following slump during placement - beams ,slabs and footings 80mm - columns and walls 80mm
- 3. Slab joints shall be placed as follows
- footpaths as per Northern Rivers Local Government standard drawing R-07
- Slabs and walls refer to slab jointing plan within this drawing set
- Slab sawn joints shall be cut within 24 hours of slab pouring in a neat and straight cut.
- 4. All splatter to surrounding surfaces shall be cleaned up immediatelv
- 5. Cover to reinforcement shall be obtained by the use of plastic bar chairs with maximum spacing of 800mm in any direction
- 6. All concrete shall be compacted using high frequency vibrators.
- 7. Curing of concrete surfaces shall commence immediately after surfaces are finished and shall continue to cure for a minimum of 7 days
- Slabs with specific rough finishes shall be kept free of bleed water and floated to prevent the formation of plastic shrinkade cracks.

# Proposed services

- has given approval. 1289571 4
- Section 21.1. 5
- materials
- Below 0.5m of the road surface

- Section 22.3). where:

6

Additives are not used.

Issue	Amendment details	Drawn	Check	Date	# Use figured dimensions only. Do not scale.	Website	www.byron.nsw.gov.au	COUNCIL	Vertical datum	1	AHD	-	
1	Construction Issue	C.A.	A.S.	19.09.24		Fax	02 66843018	SHIRE	Horizontal dat	um	MGA	7 0	General no
						Phone	02 66267000		Checked	A.S.	19.09.24		<b>.</b> .
					1	Mullumb	mby NSW 2482.	BYRON	Drawn	C.A.	19.09.24	Plan title:	
							tion Street,		Designed	C.A.	19.09.24		00
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After laying and jointing of a pipeline has been completed the constructor shall present the laid and jointed pipes for inspection by the construction engineer prior to commencement of trench backfilling. (WSA 02 2002, section

2. Backfill shall not be placed until the construction engineer

Material for the side support and overlay of the pipe shall be as for pipe bedding specified in clause C402.23. The material shall be compacted in layers of not more than 150mm to 95 per cent of the standard maximum dry density of the material used when determined in accordance with AS

The constructor shall backfill the remainder of the excavation and compact the backfill in layers of not more than 150mm thick in accordance with WSA 02-2002 Part 3,

Where the trench is within a roadway, proposed roadway, or footpath area, the remainder of the trench shall be: backfilled with a non-cohesive granular material, with a grading falling generally within the limits shown in Table C402.3, and compacted to density index of 70 when determined in accordance with AS 1289.5.4.1 for cohesionless

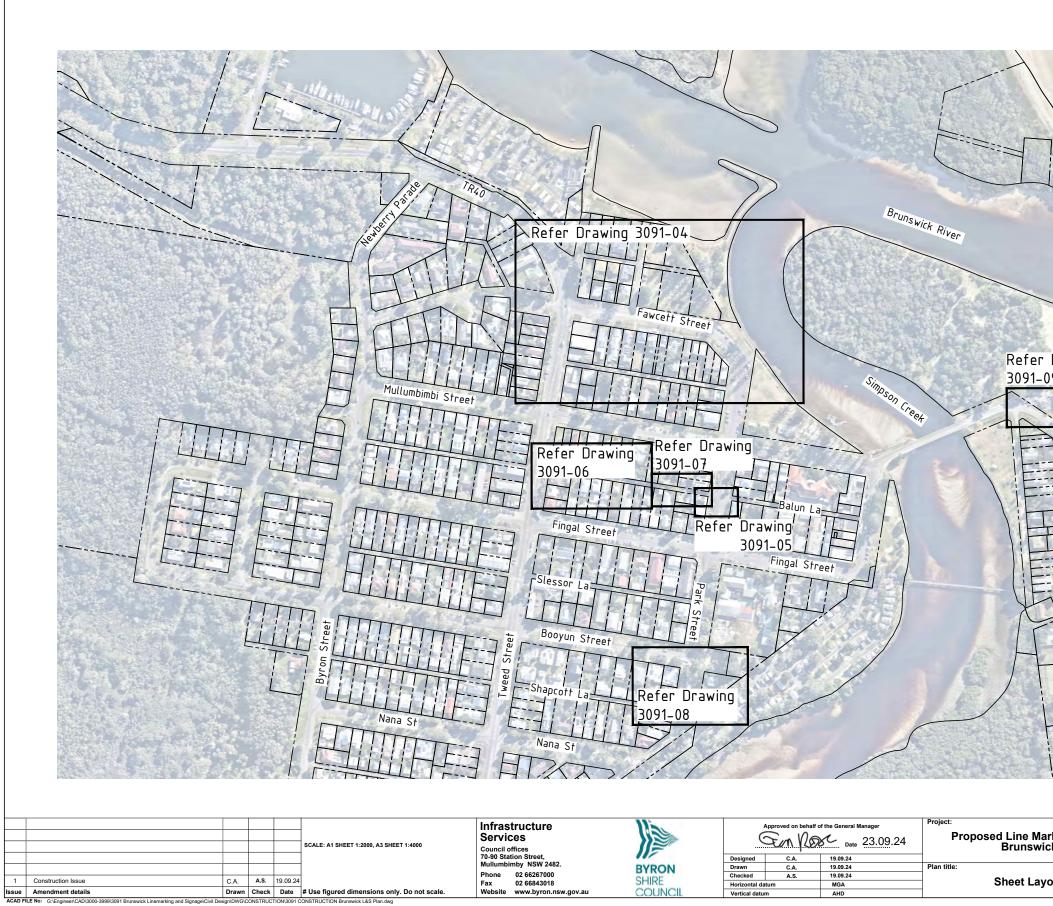
In the road reserve, but excluding the road pavement backfilled with excavated material, and compacted to 100 per cent of the standard maximum dry density of the material when determined in accordance with AS 1289.5.7.1, to within 0.5m of the road surface, but excluding the pavement layers. - Backfilled with road base and sub-base material as per existing or proposed pavement layers and compacted to 100 per cent of the standard maximum dry density of the material when determined in accordance with AS 1289.5.7.1 - Elsewhere, unless stated otherwise, the remainder of the trench shall be backfilled with ordinary excavated backfill material. Where suitable material is not available, granular material may be used for the full depth of backfilling. the material shall be compacted to a density index of 70 when determined in accordance with AS 1289.5.4.1 for cohesionless materials or 98 per cent of the standard maximum dry density of the material when determined in accordance with AS 1289.5.7.1 for cohesive materials.

The constructor shall carry out backfilling and compaction without damaging the pipe or its external coating or wrapping or producing any movement of the pipe.

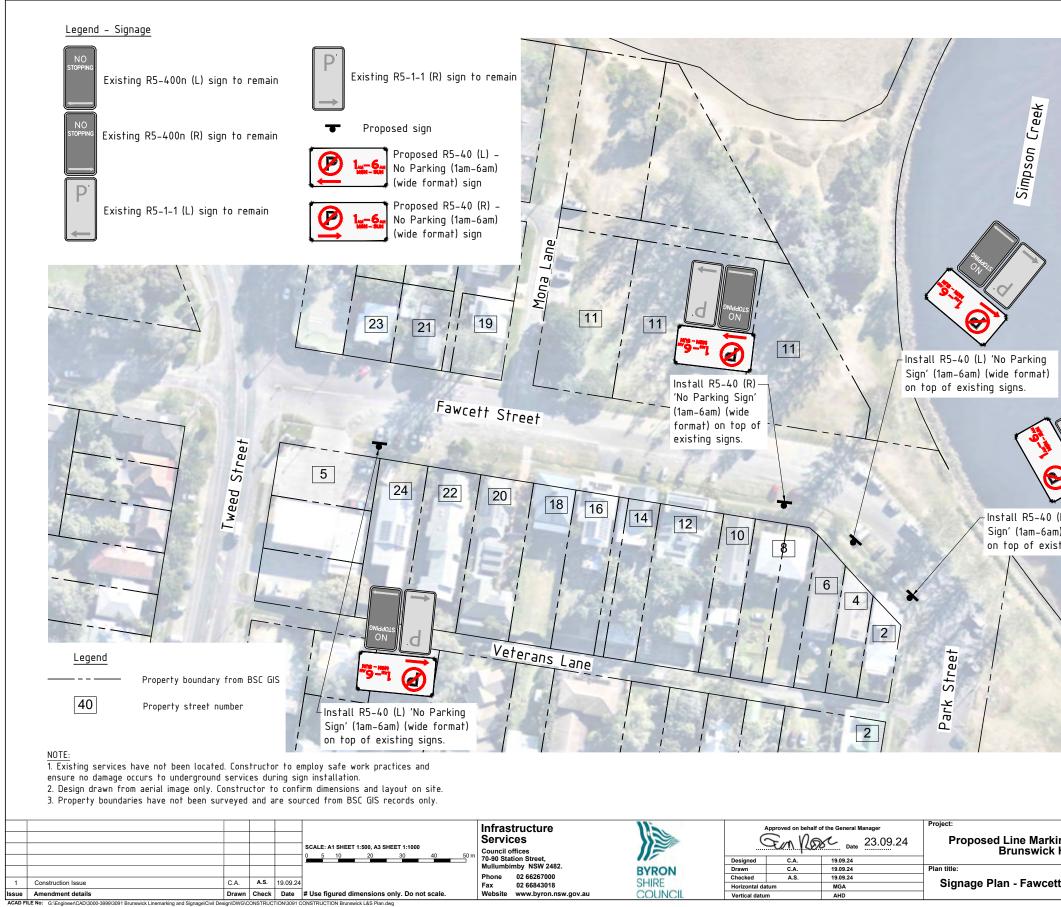
The constructor shall carry out compaction tests 75mm to 100mm below the level being tested (WSA 02-2002 Part 3,

The constructor may compact backfill by trench flooding only (A) The ground and backfill material is cohesionless and (B) Water for flooding has been sourced at the site. (C) The process will not create mud which would be moved off site by vehicles or construction plant. (D)

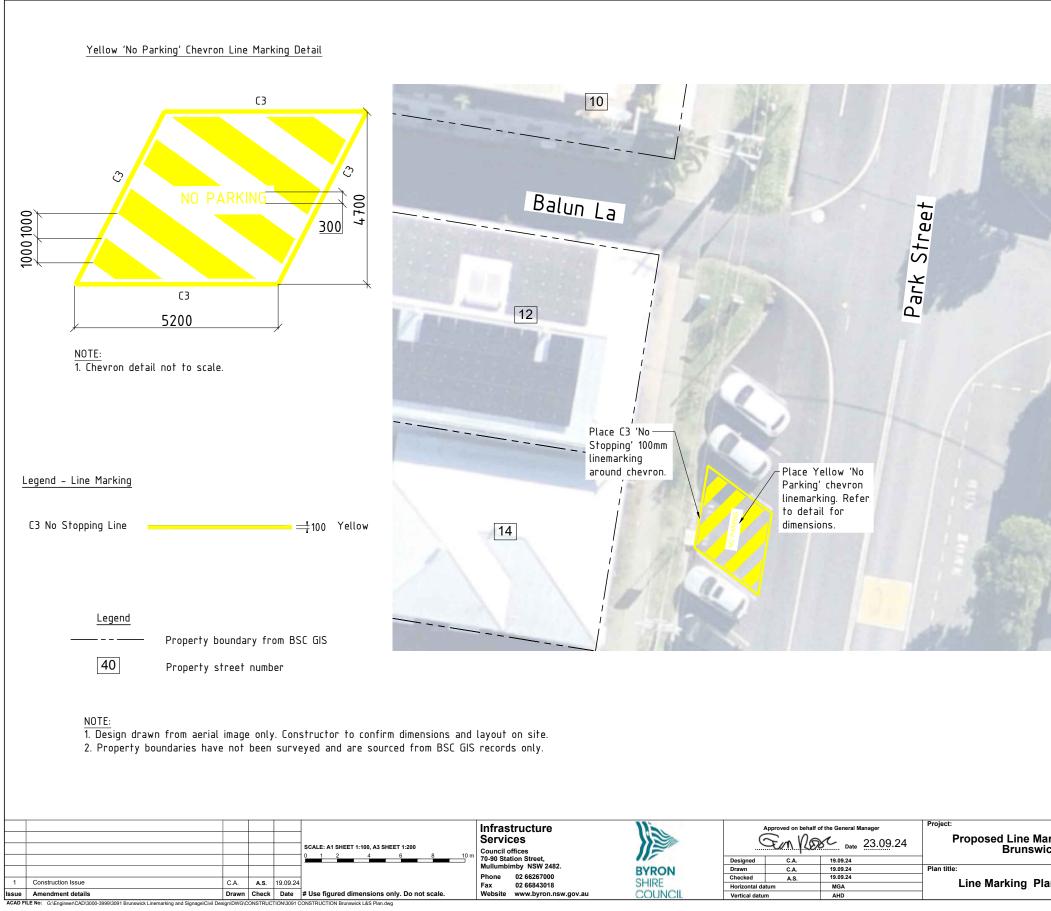
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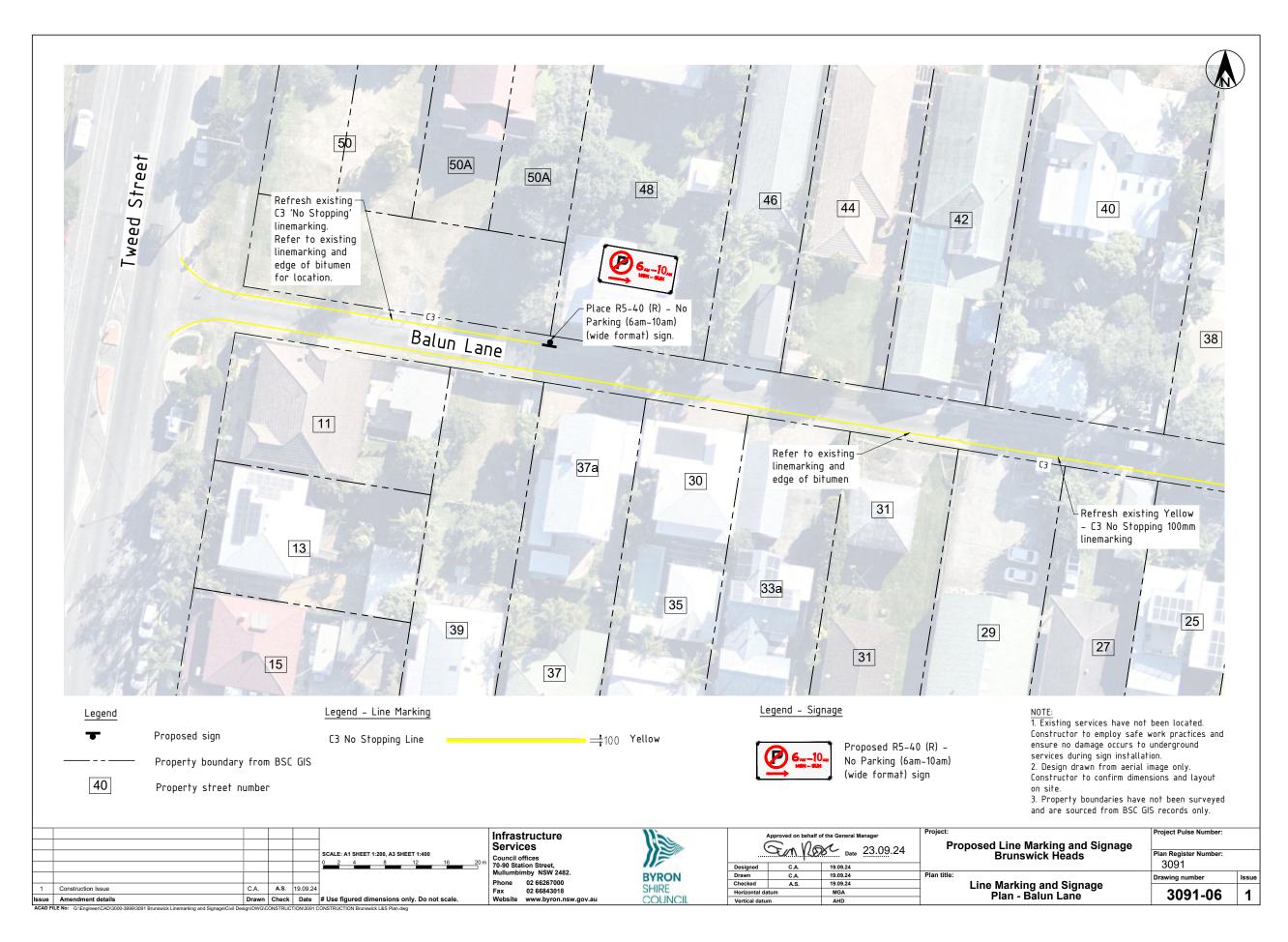
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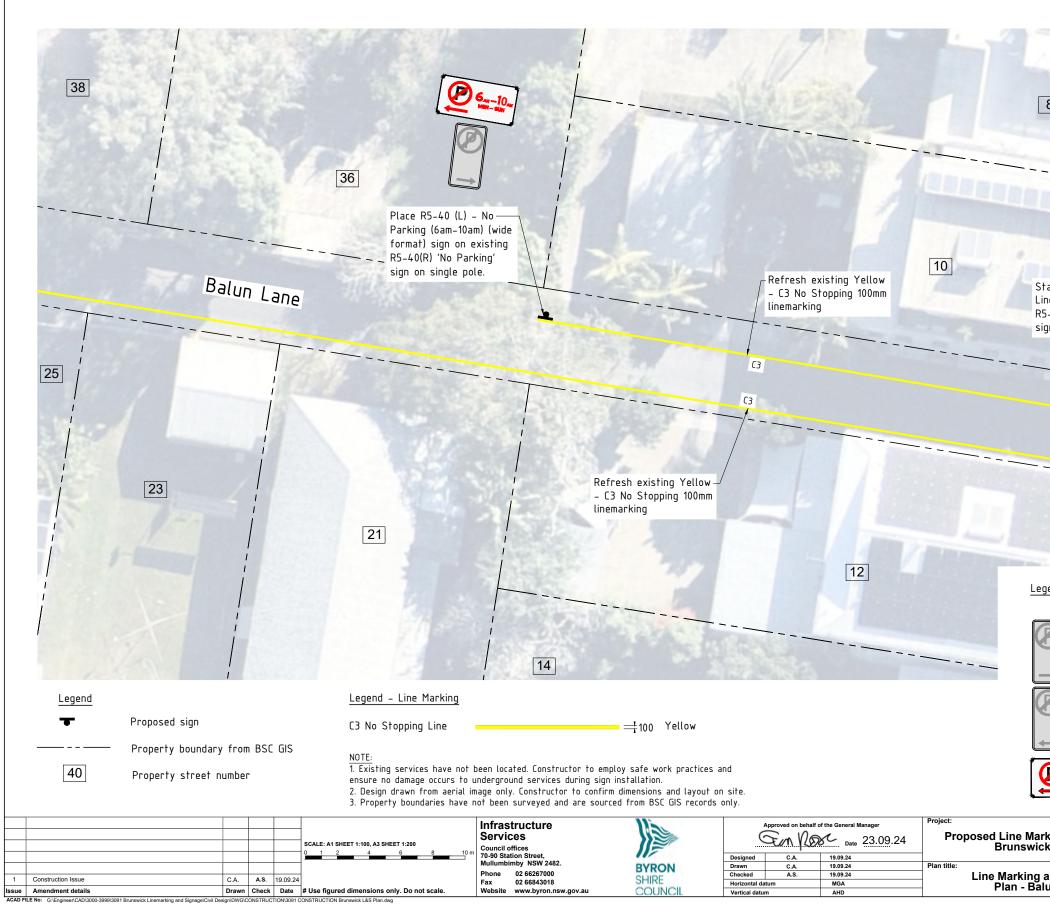


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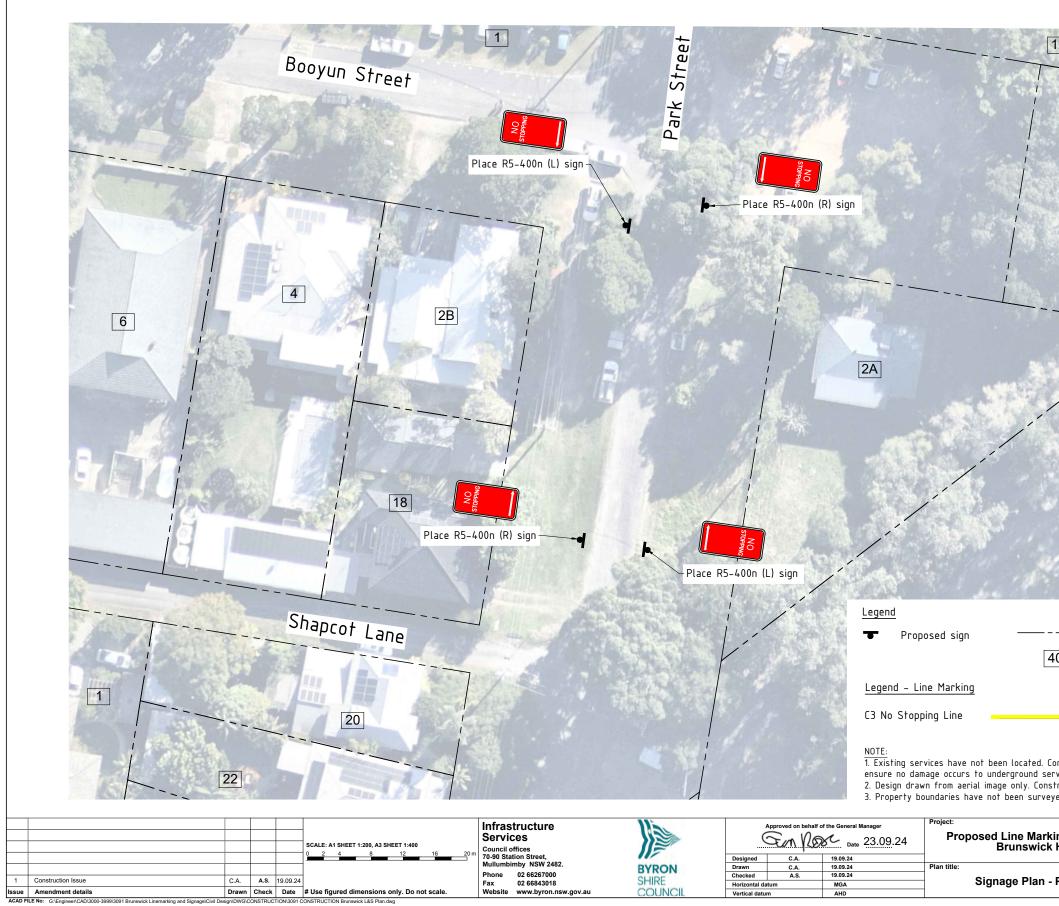


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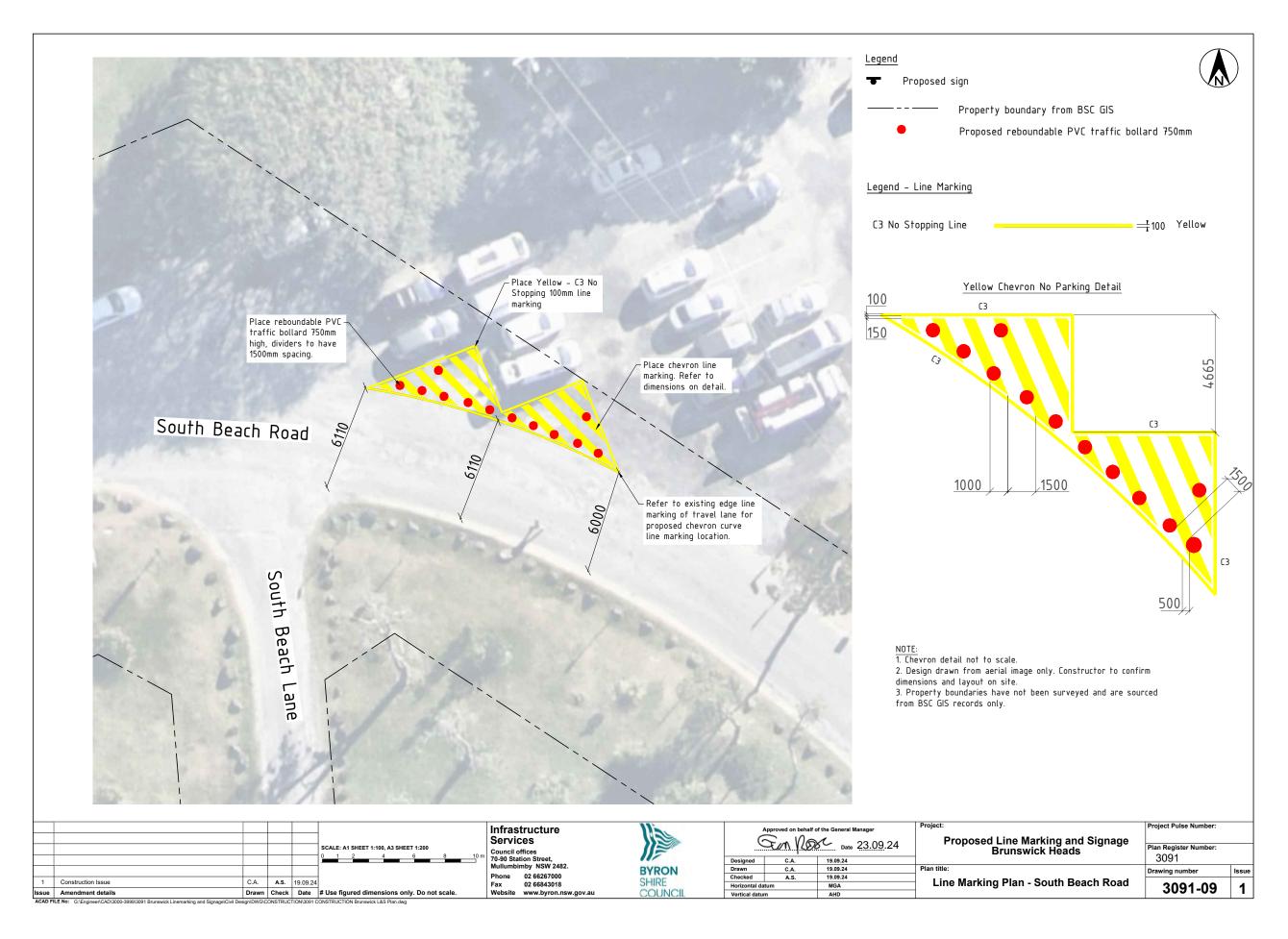




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Existing R5-40 (L) sign to remain	'No Parking'	
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	Park Street		



Quantities		
Item	Unit	Quantity
Linemarking		
C3- Yellow 100mm No Stopping Linemarking	LM	
Yellow Chevron Linemarking	M <sup>2</sup>	22
Signage		
R5-40 (L) No Parking Sign (1am-6am) (wide format)	ltem	2
R5-40 (R) No Parking Sign (1am-6am) (wide format)	ltem	2
R5-40 (R) No Parking Sign (6am-10am) (wide format)	ltem	1
R5-40 (L) No Parking Sign (6am-10am) (wide format)	ltem	1
R5-400n (L) 'No Stopping Sign'	ltem	2
R5-400n (R) 'No Stopping Sign'	ltem	2
Sign Poles	ltem	5
Reboundable PVC Traffic Bollards 750mm – Jaybro	ltem	12

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						Infras	tructure		A	proved on behalf o	of the General Manager	Project:
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lssu	Amendment details	Drawn	Check	Date	# Use figured dimensions only. Do not scale.	Website	www.byron.nsw.gov.au	COUNCIL	Vertical datum		AHD	1
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Agenda

<u> 7.1 - ATTACHMENT 1</u>

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