

BYRON HOTEL ROOM TAGS & TYPES

BASEMENT

- BCP BASEMENT CARPARK
- BS-1 BASEMENT STORE 1
- BS-2 BASEMENT STORE 2

GROUND

- FG-A / B FIRE ESCAPE GROUND A & B
- FGB FIRE ESCAPE GR/BASEMENT
- RH RESTAURANT / BAR / KITCHEN
- HR HOTEL RECEPTION
- HM HOTEL MANAGERS QUART.
- HS HOTEL STORE / LINEN
- WCA TOILET AMBULENT
- WCF TOILET FEMALE
- WCM TOILET MALE
- R1 RETAIL TENANCY 1
- R2 RETAIL TENANCY 2
- SSG LINEN CHUTE BASE
- SG SERVICE CORRIDOR G
- LS LUGGAGE STORE

LEVEL 1

- F1-A / B FIRE ESCAPE L1 A & B
- SC1-4 STORAGE CUBS 1-4
- H1-2 TYPICAL HOTEL ROOMS 1-2
- H4-5 TYPICAL HOTEL ROOMS 4-5
- H7-11 TYPICAL HOTEL ROOMS 7-11
- H3 DELUXE SUITE
- H6 DISABLED SUITE
- H12 DOUBLE ROOM
- SS1 LAUNDRY SHOOT
- S1 SERVICE CORRIDOR

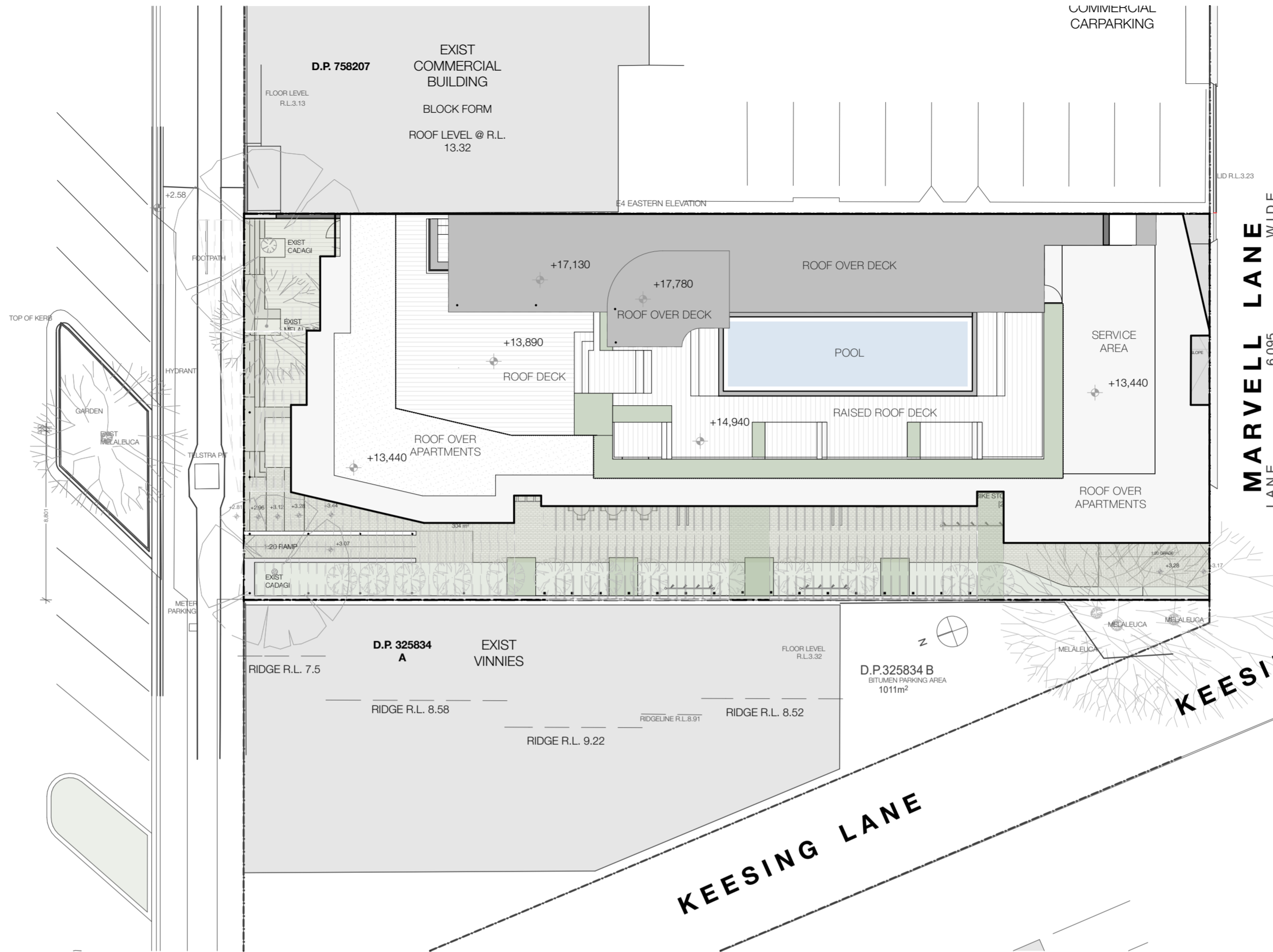
LEVEL 2

- F2-A / B FIRE ESCAPE L2 A & B
- SC5-9 STORAGE CUBS 5-9
- H13-14 TYPICAL HOTEL ROOMS 13-14
- H16-17 TYPICAL HOTEL ROOMS 16-17
- H19-23 TYPICAL HOTEL ROOMS 19-23
- H15 DELUXE SUITE
- H18 DISABLED SUITE
- H24 DOUBLE ROOM
- SS2 LAUNDRY SHOOT
- S2 SERVICE CORRIDOR

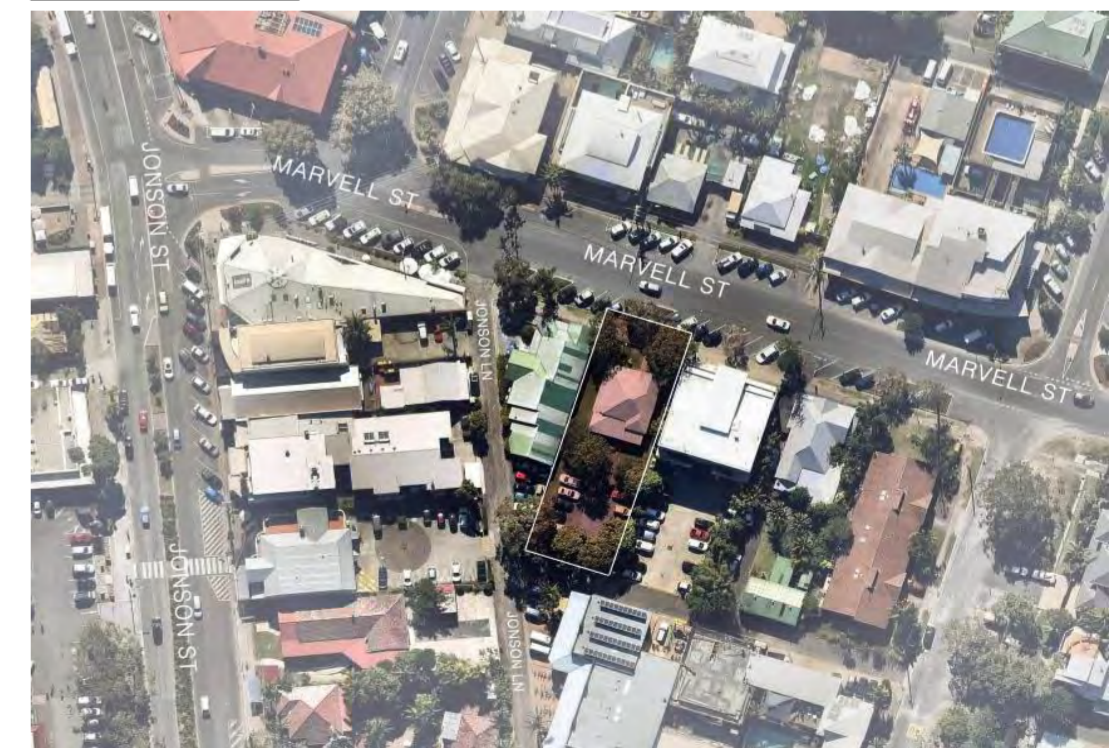
ROOF DECK

- F3 FIRE ESCAPE L3
- RD ROOF DECK
- PD POOL DECK
- P POOL
- CR CHANGE ROOMS
- PS POOL SERVICES
- WC-R UNISEX TOILETS

MARVELL STREET



LOCATION PLAN:



RATIONALE & URBAN DESIGN STATEMENT:

The Marvell Street Hotel has taken a unique dynamic form; the result of acutely responding to ideal urban outcomes for the wider community and prospective tenants of the building. These outcomes were outlined from the beginning of the project, balancing developer feasibility with the ideal outcomes for the Byron Bay local community.

Outcomes that have driven the project include:

1. **Staggered forms to reduce height perception from the street:** Despite being taller at points than the neighbouring commercial building, the perceived height of the building is considerably smaller due to intelligent stepping and sacrifices in building mass to key public areas.
2. **Shaped and situated to protect existing established trees:** Our vision is to retain the 3 major native trees towards the front end of the site. To achieve this we have staggered the basement and building form backward in order to protect their critical root zones. These trees will provide the shading for the Northern edge of the building. We have also setback the building from the South West, in order to retain the bulk of existing canopies of the 3 Maeleuca trees on the adjacent property. This is quite literally what gives the building its unique form - far from a whimsical gesture.
3. **Public amenity through introducing a laneway:** A big push throughout the design process was to incorporate a laneway into the project as to increase the amount of shaded areas to sit within the town, which we currently see in short supply. The success of arcades and laneways is becoming a clearly desirable outcome for improving urban density without retracting from the original character of the area.
4. **Lowered car parking, increased cycling:** We're pushing to take non-local traffic off the roads by providing for-hire cycling amenities on site, incorporating them into the design of the laneway in a way which promises to be exciting and engaging for the building's inhabitants.
5. **Operability, outdoor and indoors are strongly connected:** Screening protects from western sun and dapples light throughout the public areas to create sheltered transitions between inside and outside.
6. **Passive shading:** the planting is designed to spill over into the lane in conjunction with the lightweight pergola to the west, this will dapple the afternoon light sufficiently to reduce peak temperatures of the building throughout the year. This is the best way to enjoy Byron Bay's climate.
7. **Raw Aesthetic:** The fibro-shack has long been an icon in Byron, honest dwellings and places that expose their true material nature - this building will remain true to this simple and timeless principle.

We picture the Marvell Street Hotel to slot into the site, its presence in the urban environment will be considerate and enjoyable to inhabit for locals and tourists alike. The building typology strives to front the issue regarding dispersed AirBnb accommodation by efficiently creating densely compacted tourism amenity. Low profile yet efficient tourism solutions situated on sites like 4 Marvell Street in the near center of town can help repair some of the drastic issues the local community faces with rapidly expanding tourism and resultant housing affordability. We believe that the Marvell Street Hotel can set a new standard for this difficult balance.

HARLEY GRAHAM ARCHITECTS

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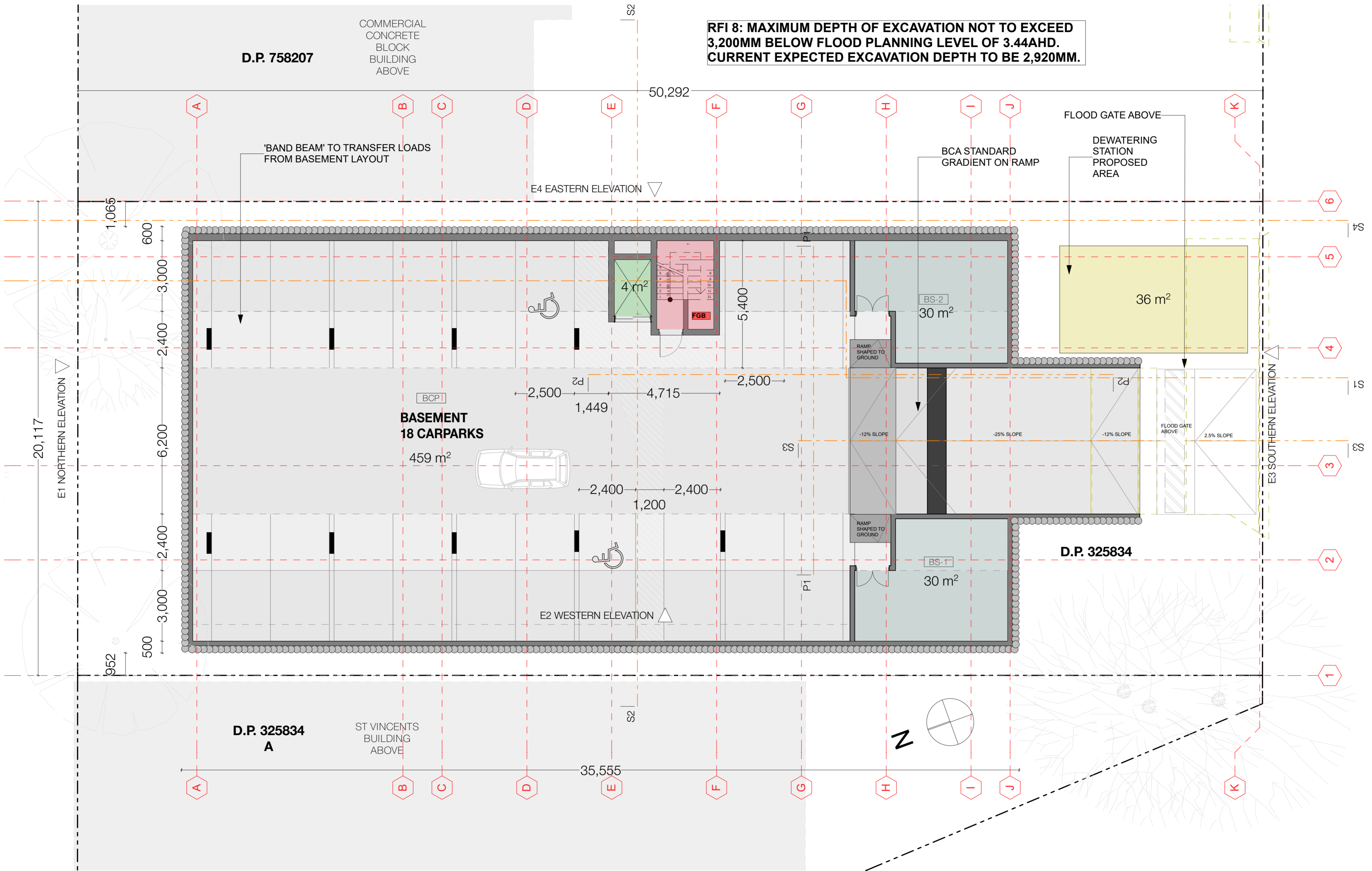
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G	DA-APPEAL 01.03.18

CLIENT	MAGNATE DEVELOPMENT	ADDRESS	4 MARVELL ST		APPROVED:	HG		JOB NO:	HGA180	
JOB NAME	4 MARVELL ST	LOT + DP	LOT B	DP 325834	SCALE	1:200	PAPER	A2	ISSUE	DA 1.0
DRAWING	SITE PLAN & DEVELOPMENT SUMMARY				SCALE	1:200	PAPER	A2	ISSUE	DA 1.0

D.P. 758207

COMMERCIAL
CONCRETE
BLOCK
BUILDING
ABOVE

**RFI 8: MAXIMUM DEPTH OF EXCAVATION NOT TO EXCEED
3,200MM BELOW FLOOD PLANNING LEVEL OF 3.44AHD.
CURRENT EXPECTED EXCAVATION DEPTH TO BE 2,920MM.**



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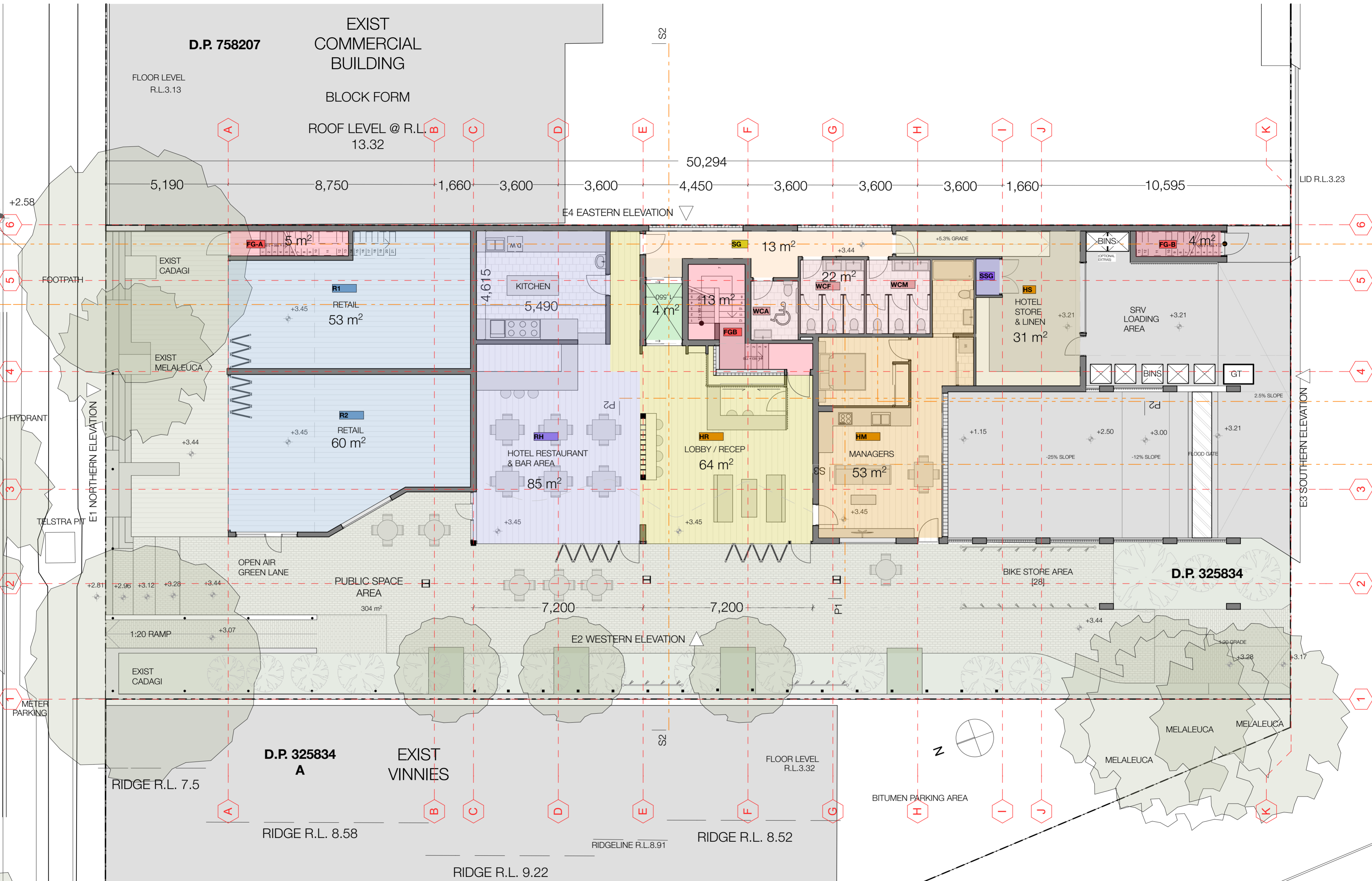
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DRAWING	BASEMENT				1:100	A2	DA	1.1	G



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DRAWING	GROUND FLOOR PLAN				1:100	A2	DA 1.2 G

D.P. 758207

LARGE COMMERCIAL BUILDING
BLOCK FORM

ROOF LEVEL @ R.L. 13.32

50,294

E4 EASTERN ELEVATION

2,370

20,117

E1 NORTHERN ELEVATION

E3 SOUTHERN ELEVATION

E2 WESTERN ELEVATION

D.P. 325834
A

VINNIES

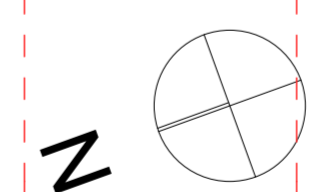
RIDGE R.L. 7.5

RIDGE R.L. 8.58

RIDGELINE R.L.8.91

RIDGE R.L. 8.52

RIDGE R.L. 9.22



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DRAWING	LEVEL 1			1:100	A2	DA	1.3	G

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LARGE COMMERCIAL BUILDING

BLOCK FORM

ROOF LEVEL @ R.L. 13.32

3,940

9,975

1,660

3,600

3,600

4,450

3,600

3,600

3,600

1,660

10,595

E4 EASTERN ELEVATION

F2-A

16 m²

84 m²

F2-B

16 m²

28 m²

H13

4 m²

28 m²

H14

8 m²

H15

45 m²

H16

28 m²

H17

28 m²

H18

31 m²

H19

28 m²

H20

28 m²

H21

28 m²

28 m²

H22

H23

28 m²

H24

53 m²

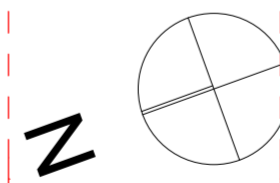
12 m²

E2 WESTERN ELEVATION

E3 SOUTHERN ELEVATION

20,117

E1 NORTHERN ELEVATION



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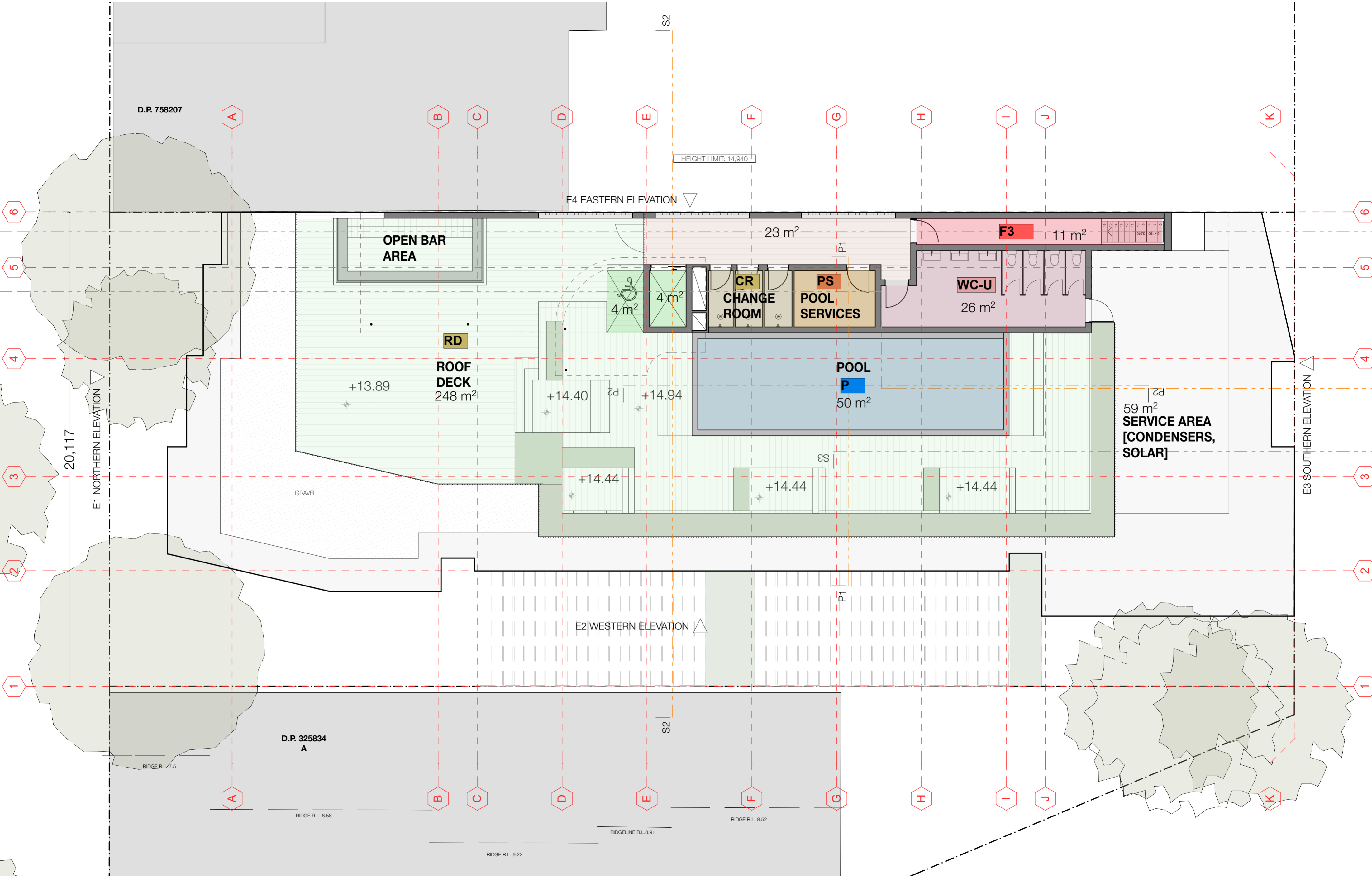
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DRAWING	LEVEL 2			1:100	A2 DA 1.4 G



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DRAWING	ROOF				1:100	A2	DA	1.5	G

LANDSCAPING PLAN

At the heart of Byron Bay's perceived culture is a respect and utilisation of the local climate and fauna. Yet currently within the town few large scale developments attempt to bolster this cultural prerogative. The Marvell Street Hotel design at its core, is centered around giving back a cool shaded oasis to the public realm in the form of it's laneway.

Despite the buildings scale we will strive to showcase the ability of landscaping to provide a public oasis amongst the typically dry landscape of the town - setting an example for future developments and an expectation for local residents to find a connection to local nature in development of this type.

Species to be nominated by Landscape Architect will be both native and low-water consumption, with the potential addition of non-native plants in the case of hanging plants if no suitable native species are available for this purpose.

PLANTING AREAS		
LEVEL	DEPTH (MIN-MAX)	AREA M ²
GROUND FLOOR	1000MM-SOIL	108M ²
LEVEL 1	900MM	44M ²
LEVEL 2	900MM	54M ²
ROOF	700MM-1400MM	60M ²
TOTAL M²		266M²

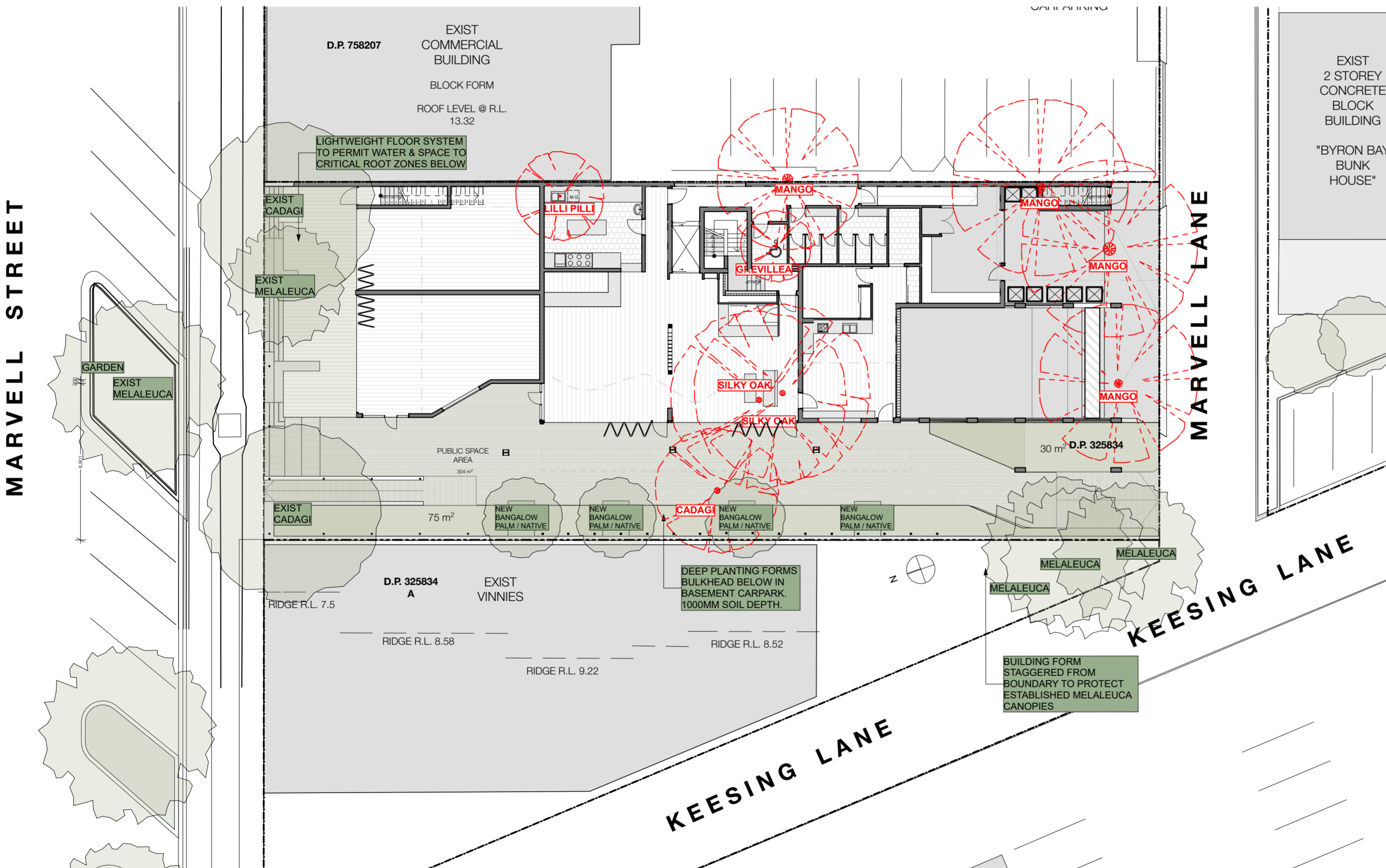
LANEWAY RESTAURANT FROM GROUND:



VIEW FROM 4 MARVEL: EXISTING TREES RETAINED



VIEW OF LANEWAY ELEVATED:



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		LANDSCAPING PLAN				APPROVED: HG		JOB NO: HGA180
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4 MARVELL STREET

ELEVATIONS

ELEVATION MATERIALS KEY:

- BR - BRICK, NATURAL RAW FINISH TO TIE WITH VINNIES
- BRW - BRICK, PAINTED WHITE FINISH
- BLW - BLOCKWORK, PAINTED WHITE FINISH

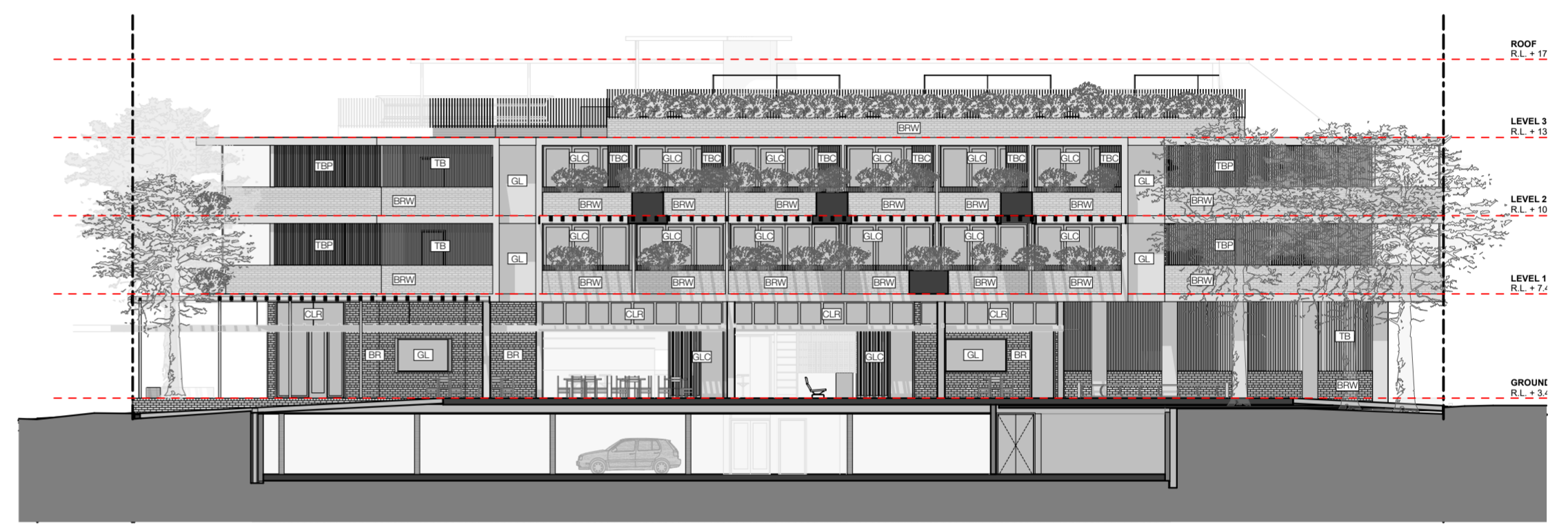
- GL - GLASS WINDOW, FIXED
- GLC - GLASS CASEMENT
- GB - GLASS BRICK, FIRE RATED
- CLR - GLASS CLERESTORY WINDOW / OPERATIONAL VENTILATION'
- GW - SMALL GREEN WALL-FACADE SYSTEM NORTH FACADE ONLY

- TB - TIMBER BATTENS FIXED, ACCOYA FINISH
- TBC - TIMBER BATTENS CASEMENT, ACCOYA
- TBP - TIMBER BATTENS FIXED ON GLASS WINDOW FRAME, PIVOTING



NORTHERN ELEVATION

1:100



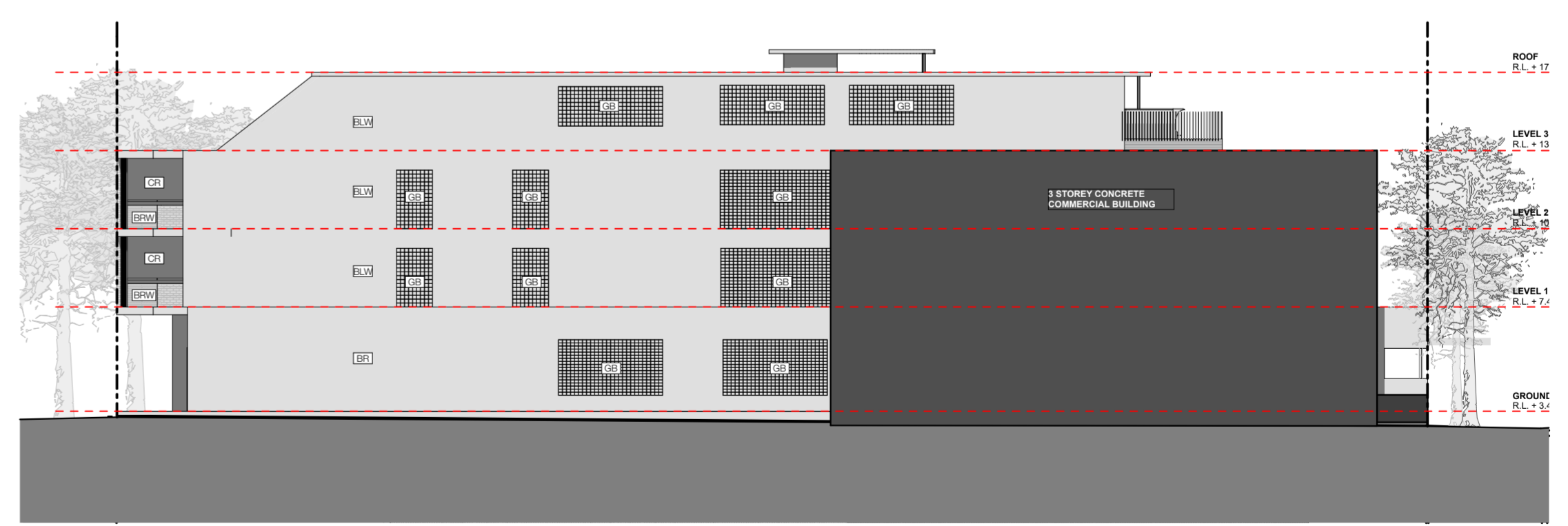
WESTERN ELEVATION

1:100



SOUTHERN ELEVATION

1:100



EAST ELEVATION

1:100

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DRAWING	ELEVATIONS			ISSUE	DA 2.1	DWG NO	G
			DP 325834				

4 MARVELL STREET

NORTH - SOUTH SECTION

UNDER HEIGHT LIMIT

OVER HEIGHT LIMIT



LONG SECTION 1

1:100

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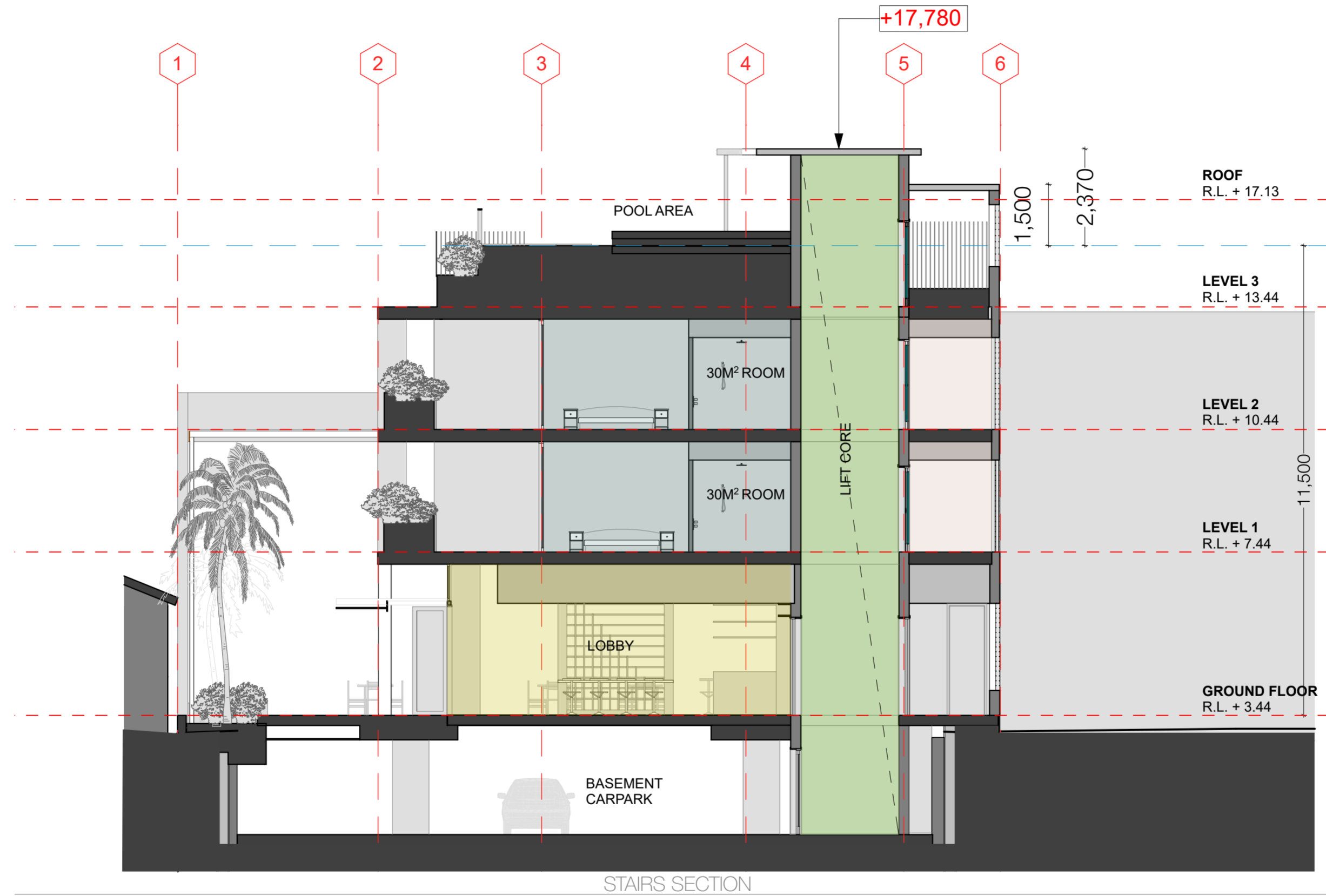
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DRAWING	SECTION 1 - NORTH - SOUTH				1:100	A2	DA 3.1	G	

4 MARVELL STREET

EAST - WEST SECTION



1:100

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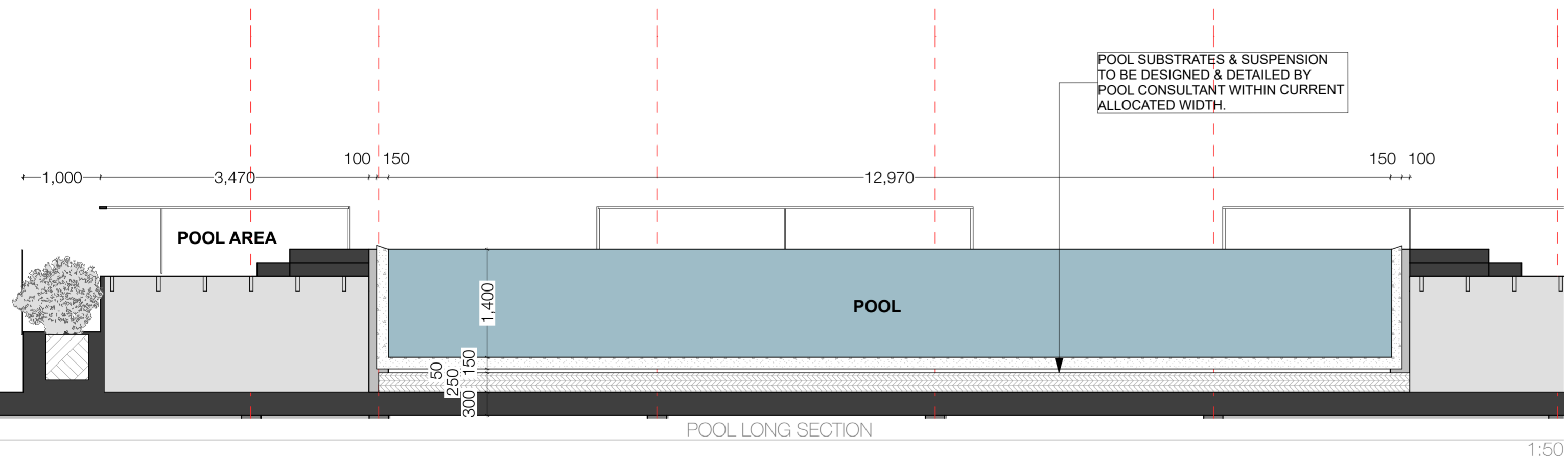
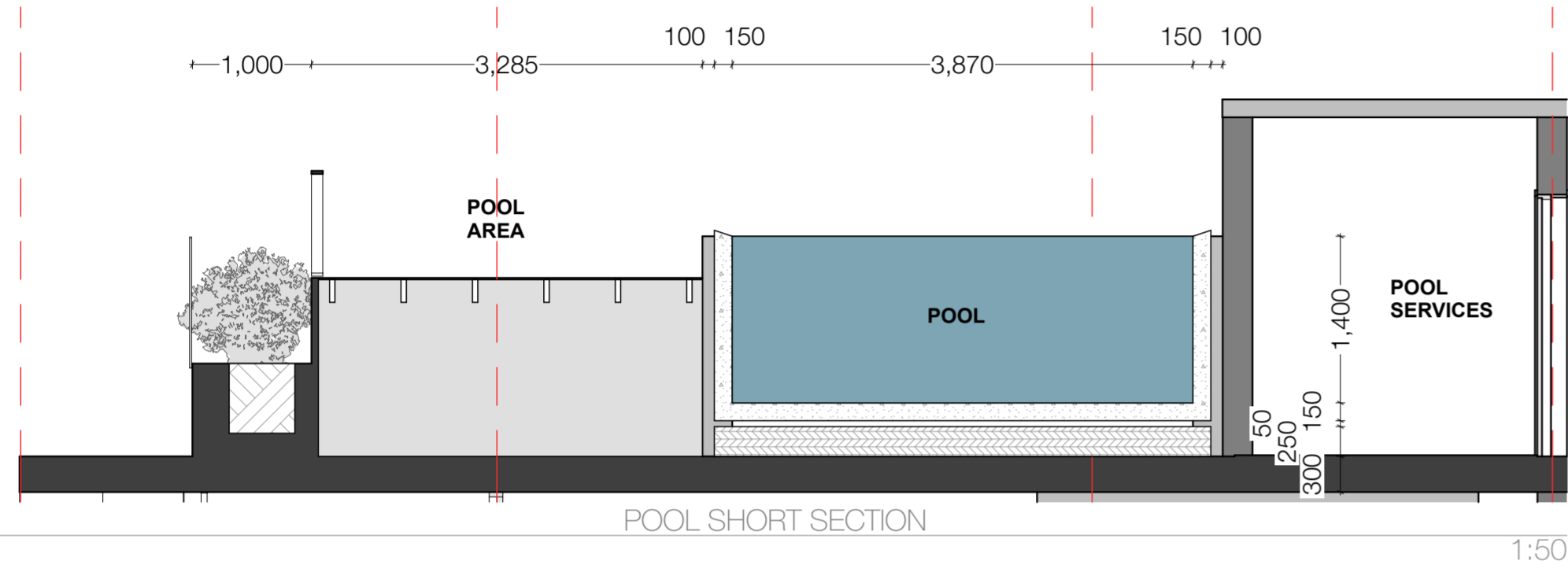
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DRAWING	SECTION 2 - EAST-WEST				1:100	A2	DA	3.2	G

4 MARVELL STREET

DA RFI - POOL SECTIONS



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DRAWING	POOL SECTIONS				1:100	A2	DA	4.1	G

4 MARVELL STREET

HEIGHT ANALYSIS PLAN

RATIONALE

The rooftop activation of Marvell Street Hotel results in a height breach as a direct result of fire engineering & lift overruns.

To compensate for this breach, we gave the building significant setbacks from the boundary along the prominent Marvell Street area in order to retain an existing canopy of established trees that green the area.

We also staggered back the building in order to form a laneway, and with these design decisions we also staggered the height of the structure to make it look substantially smaller than it is.

The sightlines of the design are analysed over the following pages - which successfully demonstrate that although the 4 Marvell Street hotel is slightly taller than the neighbouring commercial building over a small area - it appears to be smaller overall because of the intentionally designed staggering forms.

KEY

- 17.09% HEIGHT VARIATION FOR LIFT OVERRUN
- 10-14% HEIGHT VARIATION FOR LIFT OVERRUN
- 11.5M HEIGHT LIMIT
- ATLEAST 1M BELOW HEIGHT LIMIT.
- ATLEAST 1.5M BELOW HEIGHT LIMIT.
- AT GROUND LEVEL HEIGHT



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G	DA-APPEAL 01.03.18

CLIENT	MAGNATE DEVELOPMENT	ADDRESS	4 MARVELL ST	APPROVED:	HG	JOB NO:	HGA180
JOB NAME	4 MARVELL ST	LOT + DP	LOT B DP 325834	SCALE	1:100	PAPER	A2
DRAWING	HEIGHT PLANE PLAN			ISSUE	DA	DWG NO	5.1
				REV	G		

4 MARVELL STREET

HEIGHT PERCEPTION DIAGRAMS

KEY

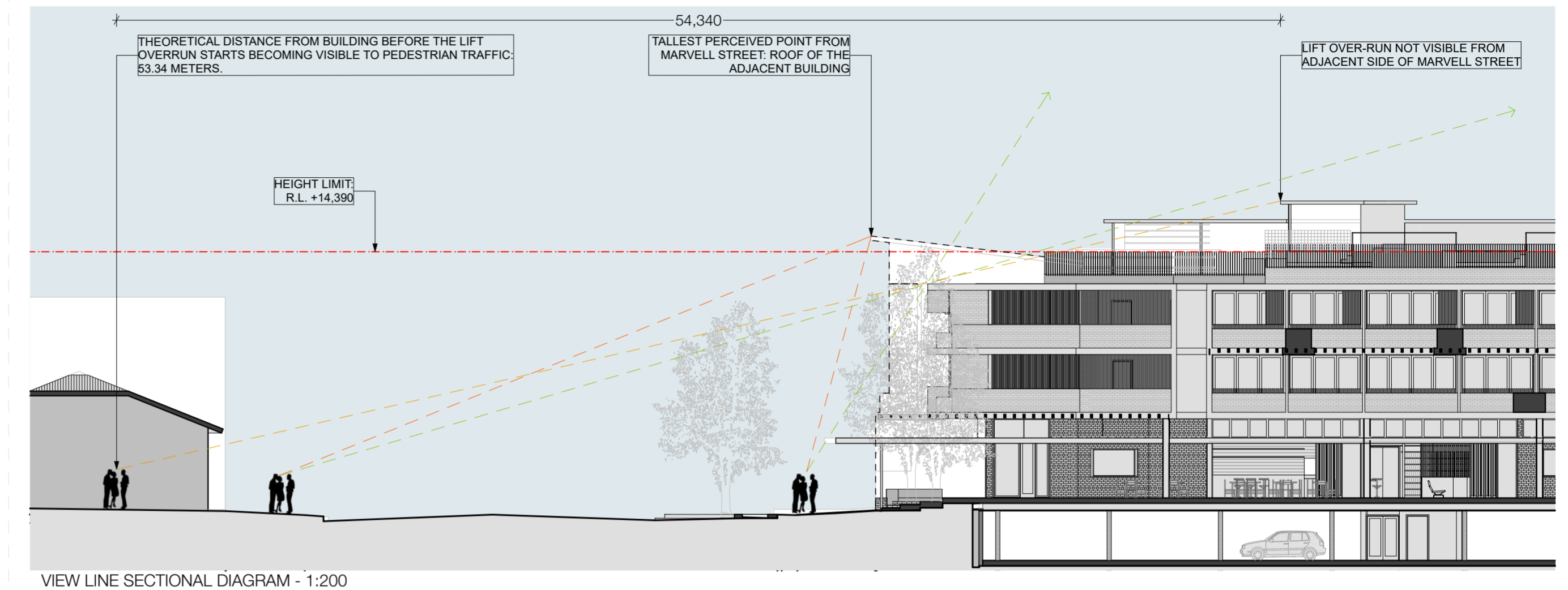
- > SIGHT LINE TO THE TALLEST VISIBLE POINT ON MARVELL STREET (ADJACENT BUILDING)
- > SIGHT LINES OF PUBLIC VIEW FROM MARVELL STREET, BOTH NORTH AND SOUTHERN SIDES.
- > SIGHT LINE REQUIRED TO PERCEIVE LIFT OVERRUN.

RATIONALE

As can be seen in the sectional diagram and perspectives below - the impact of the building on the street is heavily reduced by the staggered design and retained planting. It is argued that because of the "roof feature" on the adjacent commercial block, that this will remain to appear as the biggest building on the street front even after the construction of the Boutique Hotel.

The lift overrun theoretically cannot be seen from the opposite side of the street, as the angle required to see it cannot be reached without travelling into a building on this side.

Even from further down the street adjacent to Bayleaf cafe the lift overrun is barely visible and the buildings profile is not overly imposing as it staggers effectively with Vinnies on the adjacent corner block.



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DRAWING	HEIGHT PLANE DIAGRAMS				1:200	A2	DA 5.2	G	

4 MARVELL STREET

CONTEXT PERSPECTIVES

CONTEXT ANALYSIS

As can be seen in the following photomontages 4 Marvell Street, despite breaching height planes in very small regions, is perceivably an in-scale building due to its staggering form; which not only respects its context but strives to retain trees that would typically be lost in developments of this type. These solutions are a direct result of our analysis of the Byron Town Centre Masterplan, which encourages this balance. As of yet there are no buildings that exemplify these ideals in the area, so the solutions are completely new and inspired by sub-tropical designs seen in Queensland, but balanced with our local approach.

Even in these montages, it is worth noting that some virtual 'trimming' has taken place to further pronounce the building. Realistically this will happen, and the canopy shown in the montages will be reduced slightly to allow a considered balance between the building and immediate existing Cadagi & Maelaluca trees. Ultimately the goal is to have the facade of the building to the North balance with the tree canopies already on the subject site.

The conceptual perspective below also shows how the Maelucas retain a relationship to the corner of the laneway; which is then followed by an inviting 'slip-lane' which leads through from 4 Marvell Laneway and onto Marvell Street. Although the building is backed up against the boundary, the articulation, planting and balance with its surroundings makes an excellent compromise to the setbacks on the Northern face, where the building retains a stealthier profile on the street than its neighbouring commercial counterpart.

Understanding the constraints of creating a healthy development leads to these sorts of design compromises being made - so that the building can be both economically effective as well as improve the urban environment in key areas, as opposed to simply 'boxing out' the height plane, bulldozing trees and placing something that fails to react to its context.

NORTH WEST PHOTOMONTAGE: The most visible 4 Marvell Street will be - from adjacent side of the road, NW corner.



CONCEPTUAL RENDER ON MARVELL LN: Although not a photomontage, this illustration shows the neighbouring maelaluca trees dwarfing the height of the proposed building. The design staggers back on the site to allow this canopy to remain substantial and provides space for the critical root zones.



NORTH EAST PHOTOMONTAGE: The form is shrouded by established trees, with only a corner protruding from the canopy, this was the intended design.



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DRAWING	MONTAGE PERSPECTIVES				1:100	A2	DA	5.3	G

4 MARVELL STREET

MARVELL LANE USE SURVEY

Currently there is 0% public use or activation on the Northern side of Marvell Lane when assessing the total town block of the subject site. The new development will modify this to 8.17%.

Towards the southern side, 10.74% of the total of Marvell Lane is publicly accessible and provides thoroughfare to the Language School & YHA.

Councils position that additional amenity is to be provided for public use and laneway activation, as well as a reduction in services, is not supported by the street typology.

Even if we disregard this fact; the design for 4 Marvell St Hotel provides higher quality amenity than currently exists anywhere along this laneway:

1. The project provides thoroughfare from Marvel St to Marvel Ln - and permits a link to the YHA & Language School.
2. The design provides a shaded green oasis for the public to inhabit, the largest of its kind anywhere on Marvell St.
3. Promotes a relationship between larger buildings and heavily planted environments as opposed to 'dry sites' which are often common in developments of this type.
4. The site frontage of 20M would usually provide between 15-18 linear meters of retail frontage and street activation. By introducing the additional public space within the laneway, we are going to provide a total of 38-40M meters of activated retail frontage.
5. The laneway is over 50 meters in length, and at its widest - 8.8 meters, with its narrowest point at 6.7M. Within the site alone there is over 410 square meters of activated public area (not including the potentially active street areas in-front of the site).

We appeal that there is no way to improve this design solution considering the servicing requirements of the building, this is the best outcome we can achieve for this property. We are confident in this solution being a superior outcome to prior developments in this area that council has approved - both in terms of how the building is serviced and the public design impact to the overall lane.



WASTE DISPOSAL:

YHA - Bins on street for collection.



VIEW OF MARVELL LN FACING EAST FROM SUBJECT SITE:

The street is completely dominated by either bins, fire escapes or car parking, without exception.



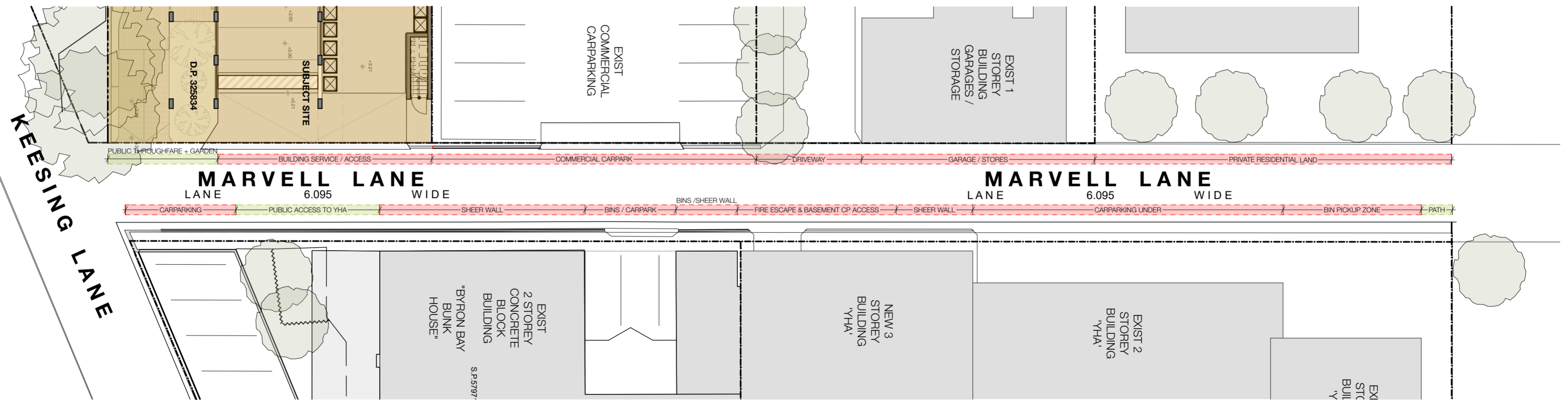
EASTERN END OF MARVELL LN

The YHA has a car-parking area beneath the structure, and to the left is storage garages.



ADJACENT TO SUBJECT SITE:

The tenancy directly to the East of Marvell lane has a paved carpark - no trees, no access.



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DRAWING	MARVELL LANE - STREET USE SURVEY			ISSUE	DA	DWG NO	5.4
				REV	G		